

Revised 04-25-2013

Disclaimer

The purpose of this Trucking Guide is to provide the information and forms a motor carrier, residing in Utah, needs to get started in their quest to become compliant with the Motor Carrier Safety Regulations, Utah Code and Administrative Rule. This booklet is not intended to take the place of published Federal, regulations. It only paraphrases the Federal Motor Carrier Safety Regulations published in Title 49 of the U.S. Code of Federal Regulations. It also only paraphrases Utah Administrative Code and Rule. The contents of this booklet may not be relied upon as a substitute for the official text. The regulations issued by the U.S. Department of Transportation and its Operating Administrations are published in the Federal Register and compiled in the U.S. Code of Federal Regulations (CFR). Copies of appropriate volumes of the CFR in book format may be purchased from the Superintendent of Documents, U.S. Government Printing Office or examined at many libraries.

This booklet is not intended to take the place of published State or agency regulations. It only paraphrases the Utah Criminal and Traffic Code. This booklet may not be relied on as a substitute for official text.

Many sources were consulted in preparing this handbook, and every effort has been made to present the information accurately. It is intended only as a helpful resource to the applicable laws and rules, not as a substitute for them. For specific questions, please refer to the law and rules themselves.

This handbook has been prepared and published by the Utah Department of Transportation's Motor Carrier Division. Any comments or corrections to its content should be forwarded to the MCD at (801) 965-4892, or by visiting mccustomerservice@utah.gov.



UTAH TRUCKING GUIDE

Motor Carrier Division

2013 Edition

Motor carrier regulations can be complex. Your responsibilities extend beyond your business operations to the condition of your vehicles and the actions of your drivers and employees. That's why we have developed this guide. It explains the basic requirements to operate as a motor carrier in Utah, as well as additional resources to assist you. If you need clarification, please call 801-965-4892 or toll free 866-215-5399.

Utah Department of Transportation

Our mission is to protect and preserve Utah's highway infrastructure, enhance safety, and facilitate commerce. The Motor Carrier Division (MCD) is committed to the advancement of highway safety by working with the commercial motor carrier industry to improve and enhance the safety of their operations.

We accomplish this by focusing our resources primarily on at-risk carriers and shippers who pose the greatest threat to highway safety. We partner with other local, state and federal agencies and industry groups to meet our safety performance based goal and objectives. We make safety performance the main criteria in issuing operating and bypass credentials to motor carriers; and we work to streamline policies, procedures, and regulations to make our business standards more efficient.

The Motor Carrier Division works closely with the United States Department of Transportation's (USDOT) Federal Motor Carrier Safety Administration (FMCSA) and the Utah Highway Patrol (UHP) in administering and enforcing the Federal Motor Carrier Safety Regulations (FMCSRs) and the Hazardous Materials Regulations (HMRs). In conjunction with FMCSA, Motor Carrier Division Transportation Safety Investigators conduct on-site investigations, inspections and reviews of interstate/intrastate carrier and shipper records to ensure that the carrier or shipper has adequate safety controls in place. This guide will assist the motor carrier or shipper from being considered unfit, which may lead to severe penalties including a possible cease and desist order. In cooperation with the Utah Highway Patrol's Commercial Vehicle Enforcement Section, and **UCA-72-9-501**, the Motor Carrier Division conducts vehicle safety inspections on commercial motor vehicles traveling within the state. These efforts ensure carriers comply with applicable vehicle safety standards.

In addition, the Motor Carrier Division is responsible for implementing, administering and enforcing vehicle size, weight and load limitation regulations and policies. The Motor Carrier Division oversees the Port of Entry facilities that are located throughout the state.

Utah Trucking Guide

2013 EDITION

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SECTION 1

Company Operating Requirements

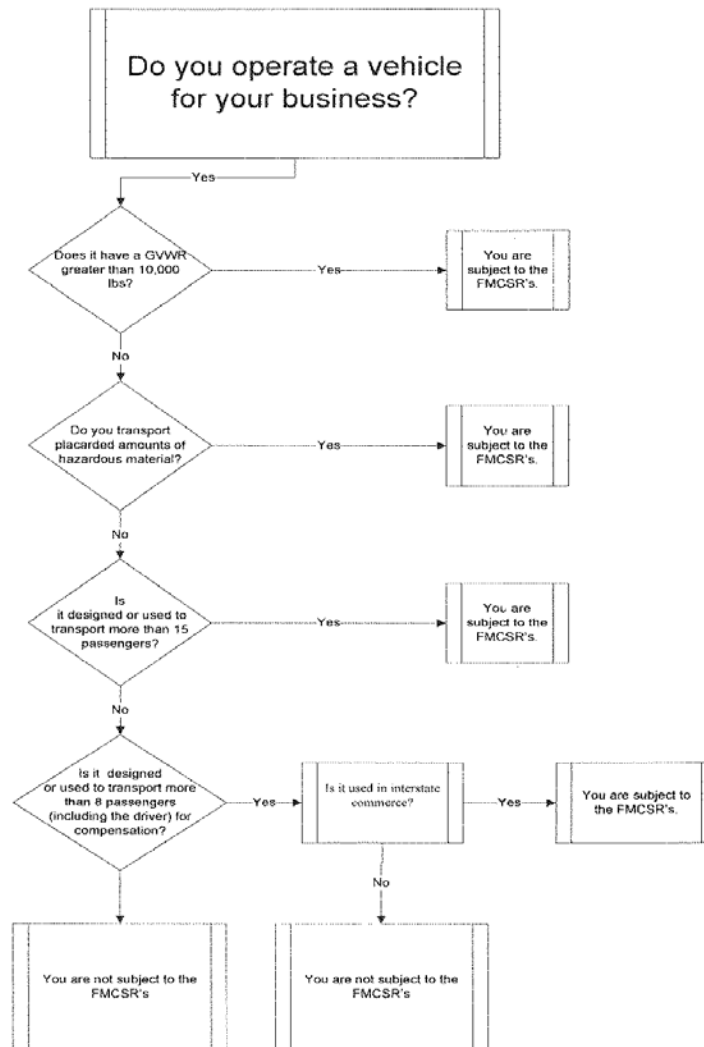
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Chapter 1

Are You a Motor Carrier?

Are YOU subject to the Federal Motor Carrier Safety Regulations?

If you operate a vehicle for your business, which falls into one of the following categories you could be considered a motor carrier and subject to the DOT regulations.



* There are various exceptions to the regulations, i.e., operating authority and vehicles being operated by government entities.

Chapter 2

Obtaining USDOT Numbers, Operating Authority and MCS-150 Updates

WHO NEEDS TO REGISTER WITH FMCSA?

Companies that operate commercial vehicles transporting passengers or hauling cargo in intrastate/interstate commerce must be registered with the Federal Motor Carrier Safety Administration to receive a USDOT number and operating authority.

The USDOT number serves as a unique identifier for monitoring a company's safety information. Audits, compliance reviews, crash investigations and inspections all use a USDOT number to identify a carrier, so carriers must have their USDOT number on their vehicles. The Compliance Safety and Accountability (CSA) program will also use USDOT numbers and other means of identification to track both commercial motor vehicles' and drivers' adherence to safety rules.

MCS-150 FORM

To obtain a USDOT number, a transport company must complete the MCS-150 form. The form also must be updated every other year (biennially), and also when a company goes out of business or has had its USDOT number revoked and is re-applying.

The MCS-150 form is also used by a company to declare the type of business they will conduct, so they might gain operating authority if required such as: Motor property or passenger carrier, shipper, freight forwarder or cargo tank facility.

A USDOT Number is required if you have:

- Vehicles that are over 10,000 pounds
- If you transport between 9 and 15 passengers (including the driver) for compensation
- If you transport 16 or more passengers,
- or haul hazardous materials in interstate commerce

A USDOT Number update is required:

- Every two years as required by 390.19
- When a company goes out of business
- When Company has its USDOT number revoked
- Anytime company information changes such as phone number, address, etc.

FMCSA ONLINE REGISTRATION AND LICENSING SYSTEM

There is no application fee ever associated with the USDOT number. You must complete the MCS-150 (Motor Carrier Identification Report) to obtain a USDOT number. The form can be found at <http://safer.fmcsa.dot.gov>

If you do not have access to the internet you can call the FMCSA toll free number 1-800-832-5660 to have a form mailed to you. Interstate operating authority is required to transport property (regulated commodities) or passenger's for-hire in interstate commerce. Please refer to Chapter 3.

To register on line you can access the at <http://www.fmcsa.dot.gov/online-registration>



Make steps 1-4 fit on one page

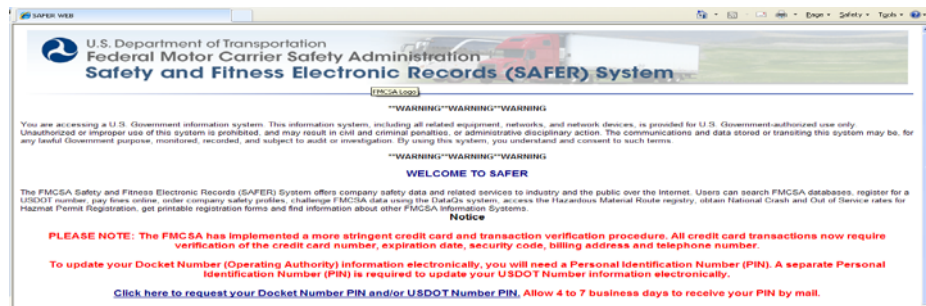
When do I update my MCS-150 Form?

All motor carriers operating commercial motor vehicles in interstate or intrastate commerce are **required** to file an updated form MCS-150 (Motor Carrier Identification Report) **every two years** or, if the company information changes.

[Update your MCS-150 Form in Four Easy Steps...](#)

Step 1: Log into the website by typing the following URL into your browser: <http://safer.fmcsa.dot.gov/>

Step 2: You have now arrived at the Safety and Fitness electronic Records (SAFER) System. Click on "Click here to request your Docket Number Pin and/or USDOT Number PIN."



Step 3: Motor carriers MUST have a USDOT Personal Identifier (PIN) in order to update the Form MCS-150 electronically. To obtain a PIN click "Yes" in the box that appears like the one below:

Verification of signature

Applicants who file electronically must submit a valid credit card number as their electronic signature. Please note that **NO CHARGE** will be made to the credit card.

Click "Yes" if you are not an existing Mexican motor carrier or Broker Only company and one of the following applies to you:	
<ul style="list-style-type: none">• You need a U.S. DOT Number or Docket Number Personal Identification Number (PIN)• Your U.S. DOT Number or Docket Number PIN has been compromised• You forgot your U.S. DOT Number or Docket Number PIN	<input type="button" value="Yes"/>

[FMCSA Registration Home](#)

Step 4: You must enter your credit card information to obtain a PIN number. Your credit card information is used for verification only.

U.S. Department of Transportation
Federal Motor Carrier Safety Administration
FMCSA Registration

Please enter your credit card information below.

This credit card is for verification only.
YOUR CREDIT CARD WILL NOT BE CHARGED.
The name and address must match the name and address on file for the credit card.
This address will be used as part of the verification process.

Name on Credit Card *

Address *

City *

State / Country *
PICK A VALUE

Postal (ZIP) Code *

Credit Card Type *
PICK A VALUE

Credit Card Number (XXXXXXXXXXXX) *

Credit Card Expiration Date

Month *
PICK A VALUE

Year *
PICK A VALUE

Three or Four Digit CCID Number*

Billing Telephone Number*

Step 4: Allow 4-7 business days to receive your PIN by mail. Once you have obtained a PIN number, your company will be able to file changes to your USDOT number electronically at <http://safer.fmcsa.dot.gov/>

The FMCSA Online Registration and Licensing System are web-based and designed to process registration and licensing requests quickly and accurately. It also helps ensure Commercial Motor Vehicle (CMV) operators complete and submit all the correct forms needed for a particular task. Additionally, the system allows for online payment with a major credit card.

FMCSA encourages all applicants with Internet access to use online registration: <http://www.fmcsa.dot.gov/online-registration>. A registration tracking number with a Personal Identification Number (PIN) can be issued allowing for a return to the system to complete, update or add information.

49 CFR Part 390.21

Interstate carriers are required to mark their vehicles with the carrier's USDOT number in addition to their business name. The letters "USDOT" must precede the number. This number is issued by the United States

Department of Transportation. Carriers can apply for this number by filing the Form MCS-150, Motor Carrier Identification Report and MCS 150A, Safety Certification Application with the USDOT.

For more information or to request a USDOT number online at the FMCSA website (www.fmcsa.dot.gov). If you need assistance with the online system contact the Motor Carrier Division at (801) 965-4892 or (866) 215-5399

Chapter 3

Operating Authority

Interstate commerce is trade, traffic, or transportation **involving** the crossing of a State boundary. Either the vehicle, its passengers, or cargo must cross a State boundary, or there must be the intent to cross a State boundary to be considered an interstate carrier.

If your operations include **interstate** commerce, you must comply with the applicable Federal safety regulations **and** Operating Authority rules (commonly referred to as a MC number), in addition to State and local requirements. You must notify the State in which you plan to register your vehicle(s) of your intentions to operate in interstate commerce to ensure that the vehicle is properly registered for purposes of the International Registration Plan (IRP), and International Fuel Tax Agreement (IFTA). The base State will help you by collecting the appropriate fees and distributing a portion of those fees to the other States in which you operate commercial motor vehicles.

In order to operate in Interstate Commerce (*your vehicle or your cargo cross State lines*) you will need to register with the Federal Motor Carrier Safety Administration (FMCSA). The following applications must be registered with the FMCSA:

- **Private** and **Exempt For-Hire** motor carriers with vehicles with a GVWR of 10,001 pounds or more must submit:
 - The Motor Carrier Identification Report (MCS-150)
- **Authorized For-Hire** motor carriers must submit:
 - The Motor Carrier Identification Report (MCS-150)
 - Proper application for Motor Carrier Operating Authority, (OP-1 series): \$300.00

In addition to the OP-1 Application, you must also submit the following to complete the application process **before Operating Authority will be granted**:

- BOC-3 (Process Agents)
- Proof of Liability and/or Cargo Insurance
- Certain **Hazardous Material** motor carriers must submit:
 - The Combined Motor Carrier Identification Report and HM Permit Application (MCS-150B)

More information can be found at <http://www.fmcsa.dot.gov/registration-licensing/online-registration/onlineregdescription.htm>

For “Frequently Asked Questions” regarding Registration and Licensing, access FAQ’s at <http://www.fmcsa.dot.gov/about/other/fag/faqs.aspx>

Chapter 4

Vehicle Identification Requirements

All Commercial Motor Vehicle must be marked as follows [49 CFR 390.21](#)

All commercial motor vehicle must be marked as specified below:

Markings must display:

- The legal name or a single trade name of the motor carrier.
- The identification number issued by the FMCSA, preceded by the letters "USDOT."
- If the name of any person other than the operating carrier appears on the CMV, the name of the operating carrier must be followed by the information required above and preceded by the words "operated by."
- Vehicles with a Utah base plate must also display the gross registered weight if registered by gross laden weight and exceeding 12,000 pounds (UCA 41-1a-229)

Markings must:

- Appear on both sides of the vehicle;
- Be in letters that contrast sharply in color;
- Be readily legible, during daylight, from a distance of 50 feet; and
- Be maintained to ensure readability.
- Lettering shall be free from obstruction.

INTRASTATE OPERATIONS

[UCA 72-9-105](#) and [UCA 41-1a-229](#)

- An intrastate motor carrier shall have lettered on both sides of any vehicle used for transportation of persons or property:
 - the name of the motor carrier company; and
 - the location of domicile by city and state for an intrastate commercial **vehicle**
- The lettering shall be free from obstruction and legible from a distance of at least 50 feet.
- In addition to the lettering required under Subsection (1), the department may require an identification number assigned by the department to be displayed in accordance with this section.
- The number may be used to assist the department in conjunction with the U.S. Department of Transportation to develop a program to improve motor carrier safety enforcement.
- An intrastate commercial vehicle primarily used by a farmer for the production of agricultural products is exempt from the provisions of this section.
- Farm Operation Exemption: A commercial vehicle primarily used by a farmer for the production of agricultural product is exempt from the provisions of this section. [UCA 72-9-105\(4\)](#). However, they are not required to have the gross registered weight displayed if they are based in Utah.

(Refer to 72-9-105)

Chapter 5

Unified Carrier Registration (UCR)

The Unified Carrier Registration (UCR) Act of 2005 was passed by Congress as part of the SAFTEA-LU bill. It requires the following entities: Motor Carriers, Private Motor Carriers, Leasing Companies, Freight Forwarders, and Brokers that have registered with Federal Motor Carrier Safety Administration (FMCSA) and have indicated, or should have indicated, that they are going to operate in “Interstate Commerce” are required to register with the UCR and pay the appropriate fee.

How often does a company have to register with the UCR and what determines the amount of the fees?

The UCR is an annual carrier registration fee that runs from January 1 thru December 31st. The UCR fee is based upon the number of vehicles, (power unit only), that are rated for 10,001 lbs GVWR or more, 10 or more passengers, or transport hazardous materials in an amount requiring placards and are operated in interstate commerce as defined in CFR 390.5.

How much are the fees and how does a company register with the UCR?

The UCR fees are set by a Board of Directors that is made up of industry and government representatives. Fees may be subject to change. The fees are set up in the following brackets:

UCR Fee Bracket		Credit Card Payment			E-Check Payment		
Fleet Size	UCR Fee	Credit Card Fee	Service Fee	Total Cost	E-Check Fee	Service Fee	Total cost
0-2 vehicles	\$76.00	\$2.60	\$3.00	\$81.60	\$1.00	\$3.00	\$80.00
3 - 5	\$227.00	\$5.62	\$3.00	\$235.62	\$1.00	\$3.00	\$231.00
6 - 20	\$452.00	\$10.12	\$3.00	\$465.12	\$1.00	\$3.00	\$456.00
21 - 100	\$1,576.00	\$32.60	\$3.00	\$1,611.60	\$1.00	\$3.00	\$1,580.00
101 - 1,000	\$7,511.00	\$151.30	\$3.00	\$7,665.30	\$1.00	\$3.00	\$7,515.00
1,001 - 200,000	\$73,346.00	\$1,468.00	\$3.00	\$74,817.00	\$1.00	\$3.00	\$73,350.00

The State of Utah uses the national registration system, which is maintained by the State of Indiana at www.ucr.in.gov. Please be aware when going to this website do not Google, Bing, or Yahoo this website. Please enter the exact web address in your address bar at the top of your internet browser.

Note: If it is a “.com” website instead of a “.gov” website you may be charged double or more than required.

What is the step by step process to pay for UCR?

1. To process and pay for your Unified Carrier Registration go to www.ucr.in.gov
2. Select Start UCR Registration.
3. Select the "New Applicant or Renew Your Unified Carrier Registration"
4. Make sure the "USDOT" option is selected.
5. Enter the Motor Carrier USDOT number.
6. Select the appropriate year from the drop down box.
7. Verify the Legal Name is correct.
8. Select the next button.
9. Select the appropriate carrier classification.
10. Select the next button.
11. Select the option indicating the number of vehicles was taken from the MCS-150 form or the actual number of vehicles operated in the prior year.

Note: Carriers have two options on indicating the number of vehicles they operate.

Option 1: A company can use the number of vehicles as noted in box 26 of the MCS-150 form or:

Option 2: The actual number of vehicles operated during a 12 month period of time ending June 30th of the prior registration year. Then they would determine the number of vehicles that are used in "Interstate Transportation" see the UCRFAQ section L for further clarification.

12. Verify the number of vehicles they are paying for is correct. Any adjustments can be made in options 1 if have any vehicles that operate solely in intrastate operations.

Note: Carriers may subtract the number of vehicles used exclusively in intrastate transportation.

13. Select the next button.
14. Select the box on the left of the certification statement. To verify that the information provided on the form is accurate.
15. Enter the "Company Representatives" name and title.
16. Select the pay button.
17. Select okay, if the amount is correct.
18. Select the Credit Card or E-Check option for method of payment.
19. Select the next button.
 - a. System will indicate the total amount of the fees to be paid.
20. Select the next button
21. Select the submit button after the required credit card or checking account information has been input.

How do states verify payment?

States verify payment electronically at www.saferysys.org. There is no receipt, permit or stickers provided to place in or on the vehicle.

Chapter 6

Vehicle Registration, Licensing and Fuel Tax Credentials

This information is provided as a courtesy of the [Utah State Tax Commission](#). It is intended only as a helpful reference to the applicable laws and rules.

INTRASTATE REGISTRATION REQUIREMENTS

UCA 41-1a201

A registration is a document issued by a jurisdiction that allows operation of a vehicle on the highways of this state, for the time period for which the registration is valid and that is evidence of compliance with the registration requirement of the jurisdiction. The power unit shall be registered for the combined gross weight of the truck/tractor and all trailers when loaded.

Note: Tow trucks are not required to have gross weight registration sufficient to cover vehicles being towed or carried as a result of their specialized operations. Not exempted if you exceed 80,000 Pounds you will be required to have? Over dimensional permits This may be deleted as the Tax Commission has indicated they have nothing in their regulations that gives tow trucks an exemption. State code only gives exemptions to STE & SME's. AA

INTERNATIONAL REGISTRATION PLAN (IRP) CODE 41-1a-301

Qualified registrants based in Utah who travels in another IRP state or Canadian province must display IRP license plates or obtain valid trip permits for the state or Canadian province in which they wish to operate. Registrants based in another IRP jurisdiction must have Utah on their IRP registration cab card for the correct weight, or have a valid 96-hour Utah trip permit. The program allows the display of the base state apportioned license plate. The base state collects the appropriate registration taxes for each state and distributes the tax to the respective state. Each IRP registrant is required to file an annual application with their base jurisdiction. The application lists the vehicles to be apportioned, the fleet mileage for each jurisdiction, and the declared gross weight for each jurisdiction. Mileage reporting is done on a fiscal year starting July 1st through June 30th. When all fees are paid, the registrant is issued a cab card, validation stickers, and registration plate for each vehicle. The cab card lists all IRP jurisdictions for which the registrant has registered and the gross weight for each jurisdiction.

IRP PARTICIPATING STATES AND PROVINCES (Non- Apportioned)

- All U.S. states except Alaska and Hawaii.
- All Canadian provinces except Yukon, Northwest Territories, and Nunavut.
- For vehicle not apportioned for Utah must have a temporary registration and fuel permit

Tax Commission for additional information over 26,000 pounds and statement

41-1a-301(12)(b) A

QUALIFIED VEHICLES

A qualified motor vehicle means a motor vehicle used, designed or maintained for transportation of persons or property and: **IRP manual qualifications are different than the ones listed below. IRP Manual Qualified Vehicle**

- Two-axle power unit over 26,000 pounds
- Vehicles used in combination when weight exceeds 26,000 pounds.
- Power unit with three (3) or more axles regardless of weight.

However, 41-1a-301(12)(b) gives an exception to vehicles under 26,000 pounds regardless of axles or registered weight. The code reads:

41-1a-301(12) (b) A state temporary permit or registration fee is not required from nonresident owners or operators of vehicles or combination of vehicles having a gross laden weight of 26,000 pounds or less for each single unit or combination.

Single Vehicle when the weight exceeds 26,000 pounds gross vehicle weight

- Is used in combination, when the weight of such combination exceeds 26,000 pounds gross ~~vehicle~~ combination weight
- Qualified motor vehicle does not include recreational vehicles

EXEMPT VEHICLES

The following vehicles are exempt from registering under the IRP:

- Government-owned vehicles.
- Recreational vehicles.
- A state temporary permit or registration fee is not required from nonresident owners or operators of vehicles or combination of vehicles having a gross laden weight of 26,000 pounds or less for each single unit or combination.

(What about SME and vehicles under 26,000 pounds?) Tax Commission?

RECIPROCITY AGREEMENT

Utah has reciprocity agreements with Idaho regarding farm-plated vehicles hauling their own farm equipment or product. These agreements allow Utah-plated vehicles to travel in Idaho, and Idaho-plated vehicles to travel into Utah without an IRP registration meeting the following conditions:

- Two-axle power unit over 26,000 pounds
- Vehicles used in combination when weight **exceeds** 26,000 pounds.
- Power unit with three (3) or more axles regardless of weight.

Reciprocity is not extended to intrastate operations (pick up and delivery within state).

UTAH BASE STATE REQUIREMENTS

HYPER LINK TO IRP WITH TAX COMMISSION (Re publication 3?)

DISPLAY OF CREDENTIALS

A valid annual registration cab card, temporary credential, or trip permit must be carried in the vehicle at all times. Utah vehicles registered under the IRP are valid for one year and expire at 12:01 a.m. on the last day of the appropriate calendar quarter. An IRP registered vehicle may operate with a faxed temporary for 60 days without displaying a registration plate.

96 HOUR REGISTRATION AND FUEL TAX PERMITS

[UCA 41-1a-301\(12\)](#) & [UCA 59-13-303](#)

A Single Unit is a power unit only. A Combo Unit is issued for a power unit and trailer(s) combination

LINK TO TAX COMMISSION

INTERNATIONAL FUEL TAX AGREEMENT (IFTA)

The IFTA is a base jurisdiction fuel tax program. The program allows carriers to file one fuel report per quarter for all participating IFTA jurisdictions. The base jurisdiction collects the appropriate fuel tax and distributes the tax to the applicable IFTA states or Canadian provinces.

UTAH SPECIAL FUEL USER PERMITS UCA 59-13-303

Intrastate only carriers may purchase a Utah Special Fuel User Permit through the Utah State Tax Commission. This permit is the equivalent of an IFTA license but is only valid for the State of Utah. **The Permit is void if a motor carrier leaves the state of Utah. Verification is needed does all permits expire upon leaving the state of Utah?**

FUEL TAX PERMITS 59-13-303(1)(b)

In place of the special fuel user permit or IFTA, a user may purchase a special fuel user trip permit. The permit is valid for 96 hours or until the qualified vehicle leaves the state.

The cost of the permit is \$25.00. **Tax Commission to verify**

Note: Intrastate only vehicles using gasoline engines are not required to have fuel permits in the State of Utah.

QUALIFIED VEHICLES

A qualified motor vehicle means any vehicle used or intended for use in two (2) or more member jurisdictions and which meets any of the following criteria:

- A power unit with a gross weight or registered weight over 26,000 pounds.
- A power unit with three or more axles regardless of weight.
- A power unit used in combination when the weight of the combination **exceeds** 26,000 pounds.

Recreational vehicles are exempt from IFTA fuel tax licensing and reporting.

DISPLAY OF CREDENTIALS

A legible photocopy of the IFTA license must be carried in the cab of each qualified vehicle in the fleet. Two decals are required per vehicle. Place one decal on each side of the exterior portion of the cab. The original license should be kept with the business records. The IFTA license and decal is valid for the current calendar year. New decals may be applied to all vehicles between December 1st of the previous year and March 1st of the current year.

ADDITIONAL INFORMATION

For further information regarding IFTA or IRP Vehicle Registration and Licensing Requirements contact the Utah Tax Commission at (801) 297-6800 or (888) 251-9555 or visit the following websites:

Utah Tax Commission motorcarrier.utah.gov

Utah DMV dmv.utah.gov

IFTA www.iftach.org

IRP www.irponline.org

Make page for section with picture/from Trucking Guide

SECTION 2

Qualifications for Operating Commercial Motor Vehicles

Chapter 7

Insurance Requirements and MCS 90

Title 49 CFR, Part 387

Financial responsibility means having insurance policies or surety bonds sufficient to satisfy the minimum public liability requirements. Public liability means liability for bodily injury, property damage and environmental restoration. Environmental restoration means restitution for the loss, damage or destruction of natural resources arising out of an accidental discharge of toxic or other environmentally harmful materials or liquids.

Requirements for Financial Responsibility

Motor Carriers of property operating commercial motor vehicles in interstate commerce and for hire carriers of passengers operating in interstate/intrastate or foreign commerce must have at least the minimum amount of insurance required by law.

(See Schedule of Limits in this folder for Minimum levels of Financial Responsibility.)

The motor carrier must have proof of the minimum level of insurance at the Company's principal place of business.

Proof

Proof may be shown by any of the following:

MCS-90

Endorsements for Motor Carriers of Property policies of insurance for public liability under Sections 39 and 30 of the Motor Carrier Act of 1980 (Form MCS-90) issued by an insurer.

- Endorsements for Motor Carriers of Passengers policies of insurance for public liability under Sections 18 of the Bus Regulatory Reform Act of 1982 (Form MCS-90B)

MCS-82 (Surety Bond)

- A motor carrier surety bond for public liability under Section 30 of the Motor Carrier Act of 1980 (Form MCS-82) issued by a surety.
- A motor carrier of Passengers Surety Bond for public liability under Section 18 of the Bud Regulatory Act of 1982 (Form MCS-82B*)
- A written decision, order or authorization of the interstate Commerce Commission authorizing the motor carrier to self-insure under 49 CFR 1043.5

Utah Administrative Rule

Adoption of Federal Regulations.

Licensed child care providers operating a passenger vehicle with a seating capacity of not more than 30 passengers, and wholly in intrastate commerce, are exempt from 49 CFR Part 387 Subpart B but are subject to the minimum coverage requirements in Section 72-9-103

(Refer to R909-1-2)

Insurance for Private Intrastate/Interstate Motor Carriers.

- Private motor carrier means a person who provides transportation of property or passengers by commercial motor vehicle, and is not a for-hire carrier.
- All private motor carriers shall have a minimum amount of **\$750,000 liability**.
- All interstate for-hire and private motor carriers transporting any quantities of oil listed in 49 CFR 172.101; hazardous waste, hazardous material and hazardous substances defined in 49 CFR 171.101, shall have \$1,000,000 minimum level of financial responsibility and a MCS-90 endorsement maintained at the principal place of business

(Refer to R909-1-3)



U.S. Department
of Transportation
Federal Motor Carrier
Safety Administration

**ENDORSEMENT FOR
MOTOR CARRIER POLICIES OF INSURANCE FOR PUBLIC LIABILITY
UNDER SECTIONS 29 AND 30 OF THE MOTOR CARRIER ACT OF 1980**

Form Approved:
OMB No.: 2126-0008

Issued to _____ of _____
Dated at _____ this _____ day of _____, 20____
Amending Policy No. _____ Effective Date _____
Name of Insurance Company _____
Countersigned by _____
Authorized Company Representative

The policy to which this endorsement is attached provides primary or excess insurance, as indicated by "[X]," for the limits shown:

- [] This insurance is primary and the company shall not be liable for amounts in excess of \$_____ for each accident.
[] This insurance is excess and the company shall not be liable for amounts in excess of \$_____ for each accident in excess of the underlying limit of \$_____ for each accident.

Whenever required by the Federal Motor Carrier Safety Administration (FMCSA), the company agrees to furnish the FMCSA a duplicate of said policy and all its endorsements. The company also agrees, upon telephone request by an authorized representative of the FMCSA, to verify that the policy is in force as of a particular date. The telephone number to call is: _____.

Cancellation of this endorsement may be effected by the company of the insured by giving (1) thirty-five (35) days notice in writing to the other party (said 35 days notice to commence from the date the notice is mailed, proof of mailing shall be sufficient proof of notice), and (2) if the insured is subject to the FMCSA's registration requirements under 49 U.S.C. 13901, by providing thirty (30) days notice to the FMCSA (said 30 days notice to commence from the date the notice is received by the FMCSA at its office in Washington, D.C.).

DEFINITIONS AS USED IN THIS ENDORSEMENT

Accident includes continuous or repeated exposure to conditions or which results in bodily injury, property damage, or environmental damage which the insured neither expected nor intended.

Motor Vehicle means a land vehicle, machine, truck, tractor, trailer, or semitrailer propelled or drawn by mechanical power and used on a highway for transporting property, or any combination thereof.

Bodily Injury means injury to the body, sickness, or disease to any person, including death resulting from any of these.

Property Damage means damage to or loss of use of tangible property.

Environmental Restoration means restitution for the loss, damage, or destruction of natural resources arising out of the accidental discharge, dispersal, release or escape into or upon the land, atmosphere, watercourse, or body of water, of any commodity transported by a motor carrier. This shall include the cost of removal and the cost of necessary measures taken to minimize or mitigate damage to human health, the natural environment, fish, shellfish, and wildlife.

Public Liability means liability for bodily injury, property damage, and environmental restoration.

The insurance policy to which this endorsement is attached provides automobile liability insurance and is amended to assure compliance by the insured, within the limits stated herein, as a motor carrier of property, with Sections 29 and 30 of the Motor Carrier Act of 1980 and the rules and regulations of the Federal Motor Carrier Safety Administration (FMCSA).

In consideration of the premium stated in the policy to which this endorsement is attached, the insurer (the company) agrees to pay, within the limits of liability described herein, any final judgment recovered against the insured for public liability resulting from negligence in the operation, maintenance or use of motor vehicles subject to the financial responsibility requirements of Sections 29 and 30 of the Motor Carrier Act of 1980 regardless of whether or not each motor vehicle is specifically described in the policy and whether or not such negligence occurs on any route or in any territory authorized to be served by the insured or elsewhere. Such insurance as is afforded, for public liability, does not apply to injury to or death of the insured's employees while engaged in the course of their employment, or property transported by the insured, designated as cargo. It is understood and agreed that no condition, provision, stipulation, or limitation contained in the policy, this endorsement, or any other endorsement thereon, or violation thereof, shall relieve the company from liability or from the payment of any final judgment, within the

limits of liability herein described, irrespective of the financial condition, insolvency or bankruptcy of the insured. However, all terms, conditions, and limitations in the policy to which the endorsement is attached shall remain in full force and effect as binding between the insured and the company. The insured agrees to reimburse the company for any payment made by the company on account of any accident, claim, or suit involving a breach of the terms of the policy, and for any payment that the company would not have been obligated to make under the provisions of the policy except for the agreement contained in this endorsement.

It is further understood and agreed that, upon failure of the company to pay any final judgment recovered against the insured as provided herein, the judgment creditor may maintain an action in any court of competent jurisdiction against the company to compel such payment.

The limits of the company's liability for the amounts prescribed in this endorsement apply separately to each accident and any payment under the policy because of any one accident shall not operate to reduce the liability of the company for the payment of final judgments resulting from any other accident.

THE SCHEDULE OF LIMITS SHOWN ON THE REVERSE SIDE DOES NOT PROVIDE COVERAGE. The limits shown in the schedule are for information purposes only.

Form MCS-80 (4/2000)

SCHEDULE OF LIMITS--PUBLIC LIABILITY

Type of carriage	Commodity transported	Jan. 1, 1985
(1) For-hire (In interstate or foreign commerce, with a gross vehicle weight rating of 10,000 or more pounds).	Property (nonhazardous)	\$ 750,000
(2) For-hire and Private (In interstate, foreign or intrastate commerce, with a gross vehicle weight rating of 10,000 or more pounds).	Hazardous substances, as defined in 49 CFR 171.8, transported in cargo tanks, portable tanks, or hopper-type vehicles with capacities in excess of 3,500 water gallons; or in bulk Division 1.1., 1.2, and 1.3 materials, Division 2.3, Hazard Zone A, or Division 6.1, Packing Group 1, Hazard Zone A material; in bulk Division 2.1 or 2.2; or highway route controlled quantities of a Class 7 material, as defined in 49 CFR 173.403.	\$5,000,000
(3) For-hire and Private (In interstate or foreign commerce, in any quantity, or in intrastate commerce, in bulk only; with a gross vehicle weight rating of 10,000 or more pounds).	Oil listed in 49 CFR 172.101; hazardous waste, hazardous materials, and hazardous substances defined in 49 CFR 171.8 and listed in 49 CFR 172.101, but not mentioned in (2) above or (4) below.	\$1,000,000
(4) For-hire and Private (In interstate or foreign commerce, with a gross vehicle weight rating of less than 10,000 pounds).	Any quantity of Division 1.1, 1.2, or 1.3 material; any quantity of a Division 2.3, Hazard Zone A, or Division 6.1, Packing Group 1, Hazard Zone A material; or highway route controlled quantities of a Class 7 material as defined in 49 CFR 173.403.	\$5,000,000

Federal Agency may not conduct or sponsor, and a person is not required to respond to, nor shall a person be subject to a penalty for failing to comply with a collection of information if it does not display a current valid OMB Control Number. For this information collection is 2126-0008. Public reporting burden for this collection of information is estimated to be approximately 2 minutes per response, including the time for reviewing instructions, gathering the data needed, reviewing the existing data sources, gathering the data needed, and completing and reviewing the collection of information. Send comments regarding this burden estimate or any other aspect of this collection of information, including suggestions for reducing this burden to: Information Collection Clearance Officer, Federal Motor Carrier Safety Administration, MC-800A, Washington, D.C. 20595.



U.S. Department of Transportation
Federal Motor Carrier
Safety Administration

ENDORSEMENT FOR
MOTOR CARRIER POLICIES OF INSURANCE FOR PUBLIC LIABILITY
UNDER SECTION 18 OF THE BUS REGULATORY REFORM ACT OF 1982

Issued to _____ of _____

Dated at _____ this _____ day of _____, 20____

Amending Policy No _____ Effective Date _____

Name of Insurance Company _____

Countersigned by _____
Authorized Company Representative

The policy to which this endorsement is attached provides primary or excess insurance, as indicated by "[X]," for the limits shown:

[] This insurance is primary and the company shall not be liable for amounts in excess of \$_____ for each accident.

[] This insurance is excess and the company shall not be liable for amounts in excess of \$_____ for each accident in excess of the underlying limit of \$_____ for each accident.

Whenever required by the Federal Motor Carrier Safety Administration (FMCSA), the company agrees to furnish the FMCSA a duplicate of said policy and all its endorsements. The company also agrees, upon telephone request by an authorized representative of the FMCSA, to verify that the policy is in force as of a particular date. The telephone number to call is: _____

Cancellation of this endorsement may be effected by the company or the insured by giving (1) thirty-five (35) days notice in writing to the other party (said 35 days notice to commence from the date the notice is mailed, proof of mailing shall be sufficient proof of notice), and (2) if the insured is subject to the FMCSA's registration requirements, by providing thirty (30) days notice to the FMCSA (said 30 days notice to commence from the date the notice is received by the FMCSA at its office in Washington, D.C.).

DEFINITIONS AS USED IN THIS ENDORSEMENT

Accident includes continuous or repeated exposure to conditions which results in Public Liability which the insured neither expected nor intended.

Bodily Injury means injury to the body, sickness, or disease to any person, including death resulting from any of these.

Motor Vehicle means a for-hire carrier of passengers by motor vehicle.

Property Damage means damage to or loss of use of tangible property.

Public Liability means liability for bodily injury, property damage.

The insurance policy to which this endorsement is attached provides automobile liability insurance and is amended to assure compliance by the insured, within the limits stated herein, as a for-hire motor carrier of passengers with Section 18 of the Bus Regulatory Reform Act of 1982 and the rules and regulations of the Federal Motor Carrier Safety Administration.

In consideration of the premium stated in the policy to which this endorsement is attached, the insurer (the company) agrees to pay, within the limits of liability described herein, any final judgment received against the insured for public liability resulting from negligence in the operation, maintenance or use of motor vehicles subject to the financial responsibility requirements of Sections 18 of the Bus Regulatory Reform Act of 1982 regardless of whether or not each motor vehicle is specifically described in the policy and whether or not such negligence occurs on any route or in any territory authorized to be served by the insured or elsewhere. Such insurance as is afforded, for public liability, does not apply to injury to or death of the insured's employees while engaged in the course of their employment, or property transported by the insured, designated as cargo. It is understood and agreed that no condition, provision, stipulation, or limitation contained in the policy, this endorsement, or any other endorsement thereon, or violation thereof, shall relieve the company from liability or from the payment of any final judgment, within the limits of liability herein described, irrespective of the financial condition, insolvency or bankruptcy of the insured.

However, all terms, conditions, and limitations in the policy to which the endorsement is attached shall remain in full force and effect as binding between the insured and the company. The insured agrees to reimburse the company for any payment made by the company on account of any accident, claim, or suit involving a breach of the terms of the policy, and for any payment that the company would not have been obligated to make under the provisions of the policy except for the agreement contained in this endorsement.

It is further understood and agreed that, upon failure of the company to pay any final judgment recovered against the insured as provided herein, the judgment creditor may maintain an action in any court of competent jurisdiction against the company to compel such payment.

The limits of the company's liability for the amounts prescribed in this endorsement apply separately to each accident and any payment under the policy because of any one accident shall not operate to reduce the liability of the company for the payment of final judgments resulting from any other accident.

The Bus Regulatory Reform Act 1982 requires limits of financial responsibility according to vehicle seating capacity, it is the MOTOR CARRIER'S obligation to obtain the required limits of financial responsibility. THE SCHEDULE OF LIMITS SHOWN ON THE REVERSE SIDE DOES NOT PROVIDE COVERAGE. The limits shown in the schedule are for information purposes only.

SCHEDULE OF LIMITS--PUBLIC LIABILITY
For-hire motor carriers of passengers operating in interstate or foreign commerce

Vehicle Seating Capacity	Effective Dates	
	Nov. 19, 1983	Nov. 19, 1985
(1) Any vehicle with a seating capacity of 16 passengers or more.	\$2,500,000	\$5,000,000
(2) Any vehicle with a seating capacity of 15 passengers or less.	\$ 750,000	\$1,500,000

Chapter 8

Federal Motor Carrier Safety Regulations- General Part 390

Title 49 CFR, Part 390

Federal Motor Carrier Safety Regulations; General

General Applicability

The Federal Motor Carrier Safety Regulations (FMCSR) applies to all employers, employees and commercial motor vehicles transporting property or passengers in interstate/intrastate commerce.

Commercial Motor Vehicle

Any self propelled or towed vehicle used on public highways in interstate/intrastate commerce to transport passengers or property under one of the following conditions:

- Vehicles has gross vehicle weight rating (GVWR) or gross combination weight rating of 10,001 or more pounds;
- Vehicle is designed to transport more than 15 passengers, including the driver;
- Designed or used to transport more than 8 passengers (including the driver) for compensation (interstate); or
- Vehicle is used in the transportation of hazardous materials in a quantity requiring placarding under the Hazardous Materials Transportation Act (HMTA) Regulations.

Exceptions to General Applicability

Transportation provided by Federal, State, Local or congressionally approved interstate agencies is exempt from the FMCSR's. Occasional non-commercial transport, school buses, ambulance services, a hearse and fire and rescue vehicle operations are also exempt.

What is an accident?

An occurrence involving a commercial motor vehicle operating on a public road those results in at least one of the following:

- A fatality;
- Bodily injury to a person who, as a result of the injury, immediately receives medical treatment away from the scene of the accident (crash);
- Disabling damage to one or more motor vehicles, requiring the vehicle(s) to be towed or otherwise transported from the scene by a tow truck or other vehicle.

Accident Register

For a period of three years after an accident (crash) occurs, motor carriers are required to maintain an accident (crash) register containing the following information;

- Time and place of accident (crash);
- Driver's name
- Number of injuries and fatalities
- Hazardous materials released if any, (other than fuel)

Motor Carriers are also required to maintain copies of all accident (crash) reports required by state or other governmental entities or insurers for a period of one year after an accident (crash) occurs.

Every commercial motor vehicle operated by a motor carrier in interstate/intrastate commerce must be marked on both sides of the vehicle with the following:

Vehicle Identification

- Name of motor carrier, or trade name
- The motor carrier's identification number preceded by USDOT for interstate carriers and followed by UT for intrastate carriers.

How can I update my USDOT record?

You can update your USDOT Number over the Internet at <http://safer.fmcsa.dot.gov> by filing a MCS-150 (Motor Carrier Identification Report). To update online you will need a Personal Identification Number (PIN) but the good news is that you can also apply for your PIN online at https://li-public.fmcsa.dot.gov/LIVIEW/PKG_PIN_START.PRC_INTRO You may also update your USDOT record by filing a MCS-150 by mail. You may mail the forms to FMCSA, MCMIS Team, 1200 New Jersey Avenue SE, Washington, DC 20590. Please put "update" on the MCS-150 form and make all necessary changes. You can also call our toll free number at 1-800-832-5660 to have an MCS-150 form mailed to you.

Where can I get the MCS-150 form?

Electronic filing is the preferred filing method, however, you may get the form from the FMCSA's website at:

<http://www.fmcsa.dot.gov/forms/print/r-l-forms.htm>

This does not automatically update your USDOT number, it must be faxed to (801) 965-4847. Please call customer service at (801) 965-4892 for additional information.

I want to inactivate or reactivate my USDOT Number. Can I do this online?

You are allowed to inactivate your USDOT Number online, using your Personal Identification Number (PIN). However, you cannot reactivate your USDOT Number online.

To reactivate a USDOT Number, you will need to send a signed letter on your company's letterhead, listing your USDOT Number, the name, address and phone number of the company, and a request asking us to reactivate your USDOT Number to:

Federal Motor Carrier Safety Administration
1200 New Jersey Avenue SE,
Washington, DC 20590
or fax it to 703-280-4003

ACCIDENT REGISTER**COMPANY NAME:**

Number	Date	Location (City, State)	Driver Name	Number Injuries	Number Fatalities	Vehicles Towed	Hazmat Incident

Note: This form is not mandatory, however the information contained herein is illustrative of information required in Part 390.15

Chapter 9

Driver Qualification File

Title 49 CFR, Part 391

Driver Requirements

A driver operating in interstate/intrastate commerce must:

- Be physically qualified under 49 CFR 391.41;
- Be 18 years old for intrastate transportation,
- Be 21 years old for interstate transportation, or when hauling hazardous materials;
- Read and speak the English language sufficiently to converse with the general public, to understand highway traffic signs and signals in the English language, to respond to official inquiries, and to make entries on reports and records
- Have a driver's license that is valid for the type of vehicle driven;
- Be able to drive the vehicle safely
- Know how to properly load and secure cargo;
- Not be disqualified from driving a commercial motor vehicle.

Medical Requirements

A driver is required to have in possession, a current and valid medical certificate showing that he or she is physically qualified to drive a commercial motor vehicle. Details for medical examinations are found in 49 CFR, 391.43. U.S.DOT medical certificate forms are available from a physician or private sources.

Certificate Renewal

A medical certificate must be renewed every two years **or as required by medical examiner**. Some medical conditions may require more frequent recertification.

CDL drivers must submit a copy of their current medical certificate in order to maintain a valid license. Medical self Certification form is available from Drivers License Division at:

<http://publicsafety.utah.gov/dld/documents/CDL42MedCertSelf2-13-12.pdf>

Skills Performance Evaluation (SPE)

Under certain circumstances, a driver may be granted a Skills Performance Evaluation (SPE) from the following physical qualification requirements; vision, insulin-dependent diabetes, deaf and hard of hearing, and limb impairment. An application requesting an SPE for intrastate drivers must be submitted to the Utah Driver's License Division. A request for an SPE for interstate drivers must be submitted to the Division Administrator, FMCSA. **The medical examiner's certificate and the SPE document must be carried in the commercial vehicle at all times while being operated.**

Driver Qualification File (DQ)

Part 391.51 - A carrier must maintain a driver qualification file for each of its drivers. The file is to be kept at the carrier's principal place of business for as long as the driver is employed by the carrier and for three years after the driver leaves the carrier's employment.

The Pre-Employment Screening Program (PSP)

NOTICE: 49 CFR Part 391.23 requires prospective employers to, at a minimum, investigate a driver's employment information, crash record, and alcohol and controlled substances history from all employers the driver worked for within the previous 3 years.

The PSP is a screening tool that assists motor carriers in investigating crash history and roadside safety performance of prospective drivers. The PSP allows motor carriers to purchase 5 years of crash data and 3 years of roadside inspection data from the Federal Motor Carrier Safety Administration's (FMCSA) Motor Carrier Management Information System (MCMIS). Records are available 24 hours a day via Web request. Motor carriers should visit the following website for more information:

<http://www.psp.fmcsa.dot.gov/Pages/default.aspx> **(This cannot be accessed on a network system)**

DRIVER QUALIFICATION FILE CHECKLIST

Every motor carrier must have a driver qualification (DQ) file for each regularly employed driver. The file must include the following:

- A. ____ Driver's Application for Employment (49 CFR 391.21). A person will not be allowed to drive a commercial motor vehicle unless he/she has completed and signed an application for employment.
- B. ____ Inquiry to Previous Employers -3- year (49 CFR 391.23(a)(2) & (c), and 391.53). This investigation must be made within 30 days of the date that his/her employment begins. Investigations shall include information concerning out-of-service violations, misuse of controlled substance or alcohol and accident history.
- C. ____ Inquiry to State Agencies – 3 years (49 CFR 391.23(a) (1) & (b) – (new hire MVR). The drivers driving record (MVR) for the preceding three years.
- D. ____ Driver's Road Examination and Certificate (49 CFR 391.31). A copy of the license or certificate which the motor carrier accepted as equivalent to the driver's road test pursuant to Section 391.33.
- E. ____ Medical Examiner's Certificate (49CFR 391.43). Driver must be issued a Medical Examiner's Certificate, which must be carried at all times and be renewed every two years or as required by medical examiner.
- F. ____ Annual MVR and Review of Driving Record (49 CFR 391.25). At least once every 12 months a motor carrier must obtain and review the driver MVR.
- G. ____ Annual Driver's List of Violations and Certification (49 CFR 391.27). At least once every 12 months, a motor carrier must require each driver that it employs to prepare and furnish it with a list of all violations of motor vehicle traffic laws and ordinances during the previous 12 months.

Additional required documentation, which **may** be maintained in the DQ file:

- H. ____ Alcohol and Controlled Substance Test Results (49 CFR 382.401). (These records must be maintained in a confidential and secure location with controlled access.
- I. ____ Inquiries for Alcohol and Controlled Substances information from previous CDL employers for 3 years (49 CFR 382.413). This investigation must be made within 30 days of the date that his/her employment begins. Provide previous employers with a written consent for release of information. Investigations shall include verified positive tests, refusals to be tested, and successful completion of DOT return-to-duty requirements. (Investigations shall include requirements of CFR Part 40.25)

Employment Application

Company Name

Address

City, State, ZIP

Phone number

Position Applying for: _____ Date of Application: _____

Name: _____ Social Security No.: _____

Last

First

Middle

Current Address: _____
City Street State Zip Code

Phone: _____ Birth Date: _____

Previous

Addresses: _____ Dates: From _____

(3 Years) *Street City State & Zip Code To* _____

Dates: From _____

Street City State & Zip Code To _____

Dates: From _____

Street City State & Zip Code To _____

Driver's License information: List all licenses held within the previous 3 years

License number _____ Class _____ State _____ Exp. Date _____

License number _____ Class _____ State _____ Exp. Date _____

License number _____ Class _____ State _____ Exp. Date _____

Have you ever had any driver's license denied, suspended, revoked, or canceled by any state agency?
 YES ☐ NO ☐ If yes, give state of issuance and explanation of the circumstances _____

Driving Experience

Types of Equipment (Truck, tractor/trailer, tank, etc.)	Dates		Approx. mileage driven
	To	From	(total)

List all traffic violations convictions for the previous 3 years (write NONE, if none)

Date	Location	Violation	Commercial Vehicle Yes <input type="checkbox"/> No <input type="checkbox"/>
			Yes <input type="checkbox"/> No <input type="checkbox"/>
			Yes <input type="checkbox"/> No <input type="checkbox"/>
			Yes <input type="checkbox"/> No <input type="checkbox"/>
			Yes <input type="checkbox"/> No <input type="checkbox"/>
			Yes <input type="checkbox"/> No <input type="checkbox"/>
			Yes <input type="checkbox"/> No <input type="checkbox"/>
			Yes <input type="checkbox"/> No <input type="checkbox"/>

List all accidents for the previous 3 years (write NONE, if none)

Date	Nature of Accident	Fatalities	Injuries

Employment History

List all employment for the previous 3 years, all driving jobs for the previous 10 years, including any gaps between employers

Employer:	Period of Employment		Supervisor:
Address:	From:	To:	Telephone:
City, State, ZIP			
Title and Duties:			
Reason for Leaving:			
Were you subject to the Federal Motor Carrier Safety Regulations during this period? YES <input type="checkbox"/> NO <input type="checkbox"/>			
Were you subject to 49 CFR part 40 controlled substance and alcohol testing during this period? YES <input type="checkbox"/> NO <input type="checkbox"/>			
Employer:	Period of Employment		Supervisor:

Employer:	Period of Employment		Supervisor:
Address:	From:	To:	Telephone:
City, State, ZIP			
Title and Duties:			
Reason for Leaving:			
Were you subject to the Federal Motor Carrier Safety Regulations during this period? YES <input type="checkbox"/> NO <input type="checkbox"/>			
Were you subject to 49 CFR part 40 controlled substance and alcohol testing during this period? YES <input type="checkbox"/> NO <input type="checkbox"/>			
Employer:	Period of Employment		Supervisor:

Employer:	Period of Employment		Supervisor:
Address:	From:	To:	Telephone:
City, State, ZIP			
Title and Duties:			
Reason for Leaving:			
<p>Were you subject to the Federal Motor Carrier Safety Regulations during this period? YES <input type="checkbox"/> NO <input type="checkbox"/></p> <p>Were you subject to 49 CFR part 40 controlled substance and alcohol testing during this period? YES <input type="checkbox"/> NO <input type="checkbox"/></p>			
Employer:	Period of Employment		Supervisor:
Address:	From:	To:	Telephone:
City, State, ZIP			
Title and Duties:			
Reason for Leaving:			
<p>Were you subject to the Federal Motor Carrier Safety Regulations during this period? YES <input type="checkbox"/> NO <input type="checkbox"/></p> <p>Were you subject to 49 CFR part 40 controlled substance and alcohol testing during this period? YES <input type="checkbox"/> NO <input type="checkbox"/></p>			
Employer:	Period of Employment		Supervisor:
Address:	From:	To:	Telephone:
City, State, ZIP			
Title and Duties:			
Reason for Leaving:			

Were you subject to the Federal Motor Carrier Safety Regulations during this period?

YES ☐ NO ☐

Were you subject to 49 CFR part 40 controlled substance and alcohol testing during this period? YES ☐ NO ☐

Use additional sheet if needed)

For Driver applicants of commercial motor vehicles that require a Commercial Driver's License (CDL) the applicant must disclose their controlled substance and alcohol status per the requirements of 49 CFR part 40.25(j).

As a perspective driver employee, you will have the right to review information provided by previous employers. You have the right to have errors in the information corrected by the previous employer(s) and for that previous employer(s) to re-send the corrected information to the prospective employer; the right to have a rebuttal statement attached to the alleged erroneous information, if the previous employer and the driver cannot agree on the accuracy of the information.

Driver employees who have previous Department of Transportation regulated employment history in the preceding three years, and wish to review previous employer provided investigative information, must submit a written request to the prospective employer, which may be done at any time, including when applying or as late as thirty (30) days after being employed or being notified of denial of employment. The prospective employer must provide this information to the applicant within five (5) business days of receiving the written request. If the prospective employer has not yet received the requested information from the previous employer(s), then the five (5) business day deadline will begin when the perspective employer receives the requested safety performance history information. If the driver has not arranged to pick up or receive the requested records within thirty (30) days of the prospective employer making them available, the prospective motor carrier may consider the driver to have waived their request to review the records

Certification

"I certify that this application was completed by me, and that all entries on it and information in it are true and complete to the best of my knowledge."

Applicant's Signature

Date Signed

Previous Employer Driver Inquiry

Part 1 TO BE COMPLETED BY PROSPECTIVE EMPLOYER

I, _____ - _____ - _____
 _____ FIRST M.I. LAST SOCIAL SECURITY NUMBER

Do hereby authorize my:

Previous Employer: _____

Phone: _____

Address: _____

Fax: _____

City, State, Zip: _____

E-mail: _____

To release all information regarding my services, character, and conduct while in your employ, and you are released from any and all liability, which may result from furnishing such information to the prospective employer listed below:

Perspective Employer: _____

Phone: _____

Address: _____

Fax: _____

City, State, Zip: _____

E-mail: _____

In compliance with Part 391.23(h), release of this information must be made in a written form that ensures confidentiality, such as fax, e-mail or letter.

 APPLICANT'S SIGNATURE DATE

PART 2: TO BE COMPLETED BY PREVIOUS EMPLOYER

1. The applicant named above was employed by us from (m/y) _____ to (m/y) _____
2. What kind(s) of work did the applicant do? _____
3. Did the applicant drive a motor vehicle for your? ☐ Straight Truck ☐ Tractor Semi-trailer ☐ Bus
☐ Passenger Vehicle ☐ Other
4. Was the applicant a safe and efficient driver? _____
5. Was the applicant involved in any accidents? If so, include dates (d/m/y), and brief explanation:

6. Was the driver ever placed out-of-service for hours of service violations? ☐ Yes ☐ No
 Explanation: _____

7. Did the applicant misuse alcohol or use a controlled substance? _____
8. Was the applicant's general conduct satisfactory? _____
9. Reason for leaving your employ: ☐ Discharged ☐ Laid Off ☐ Resigned
10. Remarks: _____

Print Name: _____ Position: _____
 Signature: _____ Date: _____

The Federal Motor Carrier Safety Regulations require all previous employers of this applicant to respond to this request for information within 30 days. Failure to comply with this request is in violation of 49 CFR Part 391.23.

ANNUAL VIOLATION AND REVIEW RECORD

Driver's Name:

(PLEASE PRINT OR TYPE)

I. CERTIFICATION OF VIOLATIONS (Part 391.27)

I certify that the following is a true and complete list of traffic violations (other than parking violations) for which I have been convicted or forfeited bond or collateral during the past 12 months.

Date	Offense	Location	Type Vehicle Operated

If no violations are listed above, I certify that I have not been convicted or forfeited bond or collateral on account of any violation required to be listed during the past 12 months.

(DATE OF CERTIFICATION)

(DRIVER'S SIGNATURE)

(MOTOR CARRIER'S NAME)

(MOTOR CARRIER'S ADDRESS)

(REVIEWED BY SIGNATURE)

(TITLE)

II. ANNUAL REVIEW OF DRIVING RECORD (Part 391.25)

In accordance with Section 391.25, Motor Carrier Safety Regulations, all information pertinent to the above driver's safety of operations, including the list of violations furnished by him in accordance with Section 391.27, has been reviewed for the past 12 months. I considered any evidence that the driver has violated applicable provisions of the FMCSR's and the Hazardous Materials Regulations. I considered the driver's accident record and any evidence that he/she has violated laws governing the operations of motor vehicles and gave great weight to violations such as: speeding, reckless driving, and operations while under the influence of alcohol or drugs, that indicate that the driver has exhibited a disregard for the safety of the public. Having done the above, I find that

- ☐ The driver meets the minimum requirements for safe driving, or
- ☐ The driver is disqualified to drive a commercial motor vehicle pursuant to Part 391.15

(MOTOR CARRIER'S NAME)

(MOTOR CARRIER'S ADDRESS)

(REVIEWED BY SIGNATURE)

(TITLE)

(DATE)

DRIVER'S ROAD TEST EXAMINATION

Driver's Name: _____ Phone: _____

Driver's Address: _____

City: _____ State: _____ Zip: _____

The road test shall be given by the motor carrier or a person designated by it. However, a driver who is a motor carrier must be given the test by another person. The test shall be given by a person who is competent to evaluate and determine whether the person who takes the test has demonstrated that he or she is capable of operating the vehicle and associated equipment that the motor carrier intends to assign.

Rating of Experience

_____ The pre-trip inspection. (As required by Sec. 392.7)

_____ Coupling and uncoupling of combination units, if the equipment he or she may drive includes combination units

_____ Placing the equipment in operation

_____ Use of vehicle's controls and emergency equipment

_____ Operating the vehicle in traffic and while passing other vehicles

_____ Turning the vehicle

_____ Braking, and slowing the vehicle by means other than braking

_____ Backing and parking the vehicle

_____ Other, Explain: _____

Type of equipment used in giving test: _____

Date

Examiner's Signature

If the road test is successfully completed, the person who gave it shall complete a certificate of driver's road test.

Remarks _____

CERTIFICATE OF DRIVER'S ROAD TEST

Instructions: If the road test is successfully completed, the person who gave it shall complete a certificate of the driver's road test. The original or copy of the certificate shall be retained in the employing motor carrier's driver qualification file of the person examined and a copy given to the person who was examined. (49 CFR 391.33(e)(f)(g))

CERTIFICATION OF ROAD TEST

Driver's Name _____

Social Security Number _____

Operator's or Chauffeur's License Number _____

State _____

Type of Power Unit _____

Type of Trailer(s) _____

If passenger carrier, type of bus _____

This is to certify that the above named driver was given a road test under my supervision on _____, 20_____, consisting of approximately _____ miles of driving.

It is my considered opinion that the driver possesses sufficient driving skill to operate safely the type of commercial motor vehicle listed above.

Signature of Examiner)

Title

(Organizations and Address of Examiner)

Chapter 10

Driving of Commercial Motor Vehicles

Title 49 CFR, Part 392

Illness or Fatigue

No driver is permitted to operate a motor vehicle when his/her ability and/or alertness is impaired by fatigue, illness, or any other cause that makes it unsafe to begin (or continue) to drive the vehicle.

Drugs

No driver may be on duty and possess, be under the influence of, or use:

- Any schedule I drug or other substance listed in Appendix D.
- Any amphetamine or formulation of any amphetamine (including pep pills and bennies)
- Narcotics or derivatives
- Any other substance, to a degree which renders the driver incapable of safely operating a motor vehicle.

Alcohol

A driver is forbidden to consume or be under the influences of alcohol within four hours of going on duty, while on duty, or while driving. A driver is forbidden to possess an alcoholic beverage while on duty, unless it is a manifested part of the shipment

Safe Loading

No one may drive or require anyone to drive a commercial motor vehicle unless the cargo is properly loaded and secured.

Railroad Crossing/Stopping

Motor vehicles transporting hazardous materials and most buses transporting passengers are forbidden to cross railroad tracks without first stopping and looking both ways. Additionally, the driver must not shift gears while crossing the tracks

Seat Belts

A driver must not drive before correctly restraining him/herself, if the vehicle is equipped with seat belt assemblies

Emergency Signals for Stopped Vehicles

A vehicle stopped upon a highway must activate the vehicle's hazard warning flashers at once. The driver must leave the flashers on until warning devices are activated. The flashers must again be used while the warning devices are being picked up and before the vehicle moves on

Placement of warning devices

The warning devices must be placed as follows (except where special rules apply)

- One warning device must be placed on the traffic side of the vehicle, within ten feet, in the direction of approaching traffic.
- A second device must be placed facing approaching traffic approximately 100 feet away in the center of the lane or shoulder where the vehicle is stopped.
- The third device must be placed about 100 feet away from the stopped vehicle, in the direction away from approaching traffic

Emergency Equipment

- Each truck, truck tractor, and bus must be equipped with a fire extinguisher and three bidirectional emergency triangles or 3 liquid burning flares

Radar detectors shall not be used by a driver in a commercial vehicle

- A driver shall not operate any commercial motor vehicle that is equipped with a radar detector.
- Motor carriers shall not require or permit a driver to violate the radar detector provisions

Prohibition against texting:

- No driver shall engage in texting while driving.
- No motor carrier shall allow or require its drivers to engage in texting while driving.

Hands free cell phone:

- No driver shall use a hand-held mobile telephone while driving a CMV.
- No motor carrier shall allow or require its drivers to use a hand-held mobile telephone while driving a CMV.

Chapter 11

Hours of Service- Property Carriers

Title 49 CFR, Part 395

Definitions

“On duty time” is all time a driver spends performing work, or being ready to work, until being relieved by the carrier of all responsibility. “On duty” time also includes any compensated work performed by the driver for a non-motor carrier entity. On duty time does not include time spent resting in or on a parked vehicle; (also applies to passenger-carrying drivers); any time spent resting in a sleeper berth; or up to 2 hours riding in the passenger seat of a property-carrying vehicle moving on the highway immediately before or after 8 consecutive hours in sleeper-berth.

“Driving time” is all time spent at the driving controls of a commercial motor vehicle in operation.

“Off duty” means the driver has been relieved of all responsibilities and is not ready for work.

“Record of duty status” is the tracking of work hours by means of a graph, grid, log or an automatic on-board recording device.

“Time record” is a time card or other record showing the time the driver reports for duty each day, total hours the driver is on duty each day, time the driver is released from duty each day, and total time for preceding days if the driver is used for first time or intermittently

A motor carrier may not permit or require a driver to drive after:

11 hours driving time following 10 consecutive hours off duty; the 14th hour of coming on duty following 10 consecutive hours off duty; being on duty 60 hours in a 7 day period; being on duty 70 hours in any period of 8 consecutive days if the carrier operates every day of the week. Any 7 or 8-day period may restart after 34 consecutive hours off duty.

Time spent by a driver in a compensated non-motor carrier position is considered on duty time and must be included in the 14 and 60/70 hour rules. (Example: a part time position at a retail store.)

Calculation of the 14-hour limit includes all time except any off duty time of at least 10 consecutive hours or longer or any sleeper-berth time of at least 8 consecutive hours or longer.

Drivers Record of Duty Status (Logs)

A motor carrier shall require every driver to record duty status in duplicate for each 24-hour period.

A record of duty status must:

- Be on specific grid or automatic on-board recorder;
- Be current to the last change of duty status;
- Have legible entries in driver’s own handwriting;
- Show month, day, and year with total miles driven and

- Total hours in each duty status for each 24 hours of record;
- Show vehicle number, or state and license number of each vehicle operated during that 24-hour period;
- Have carrier(s) name, for whom work was performed, and beginning and finishing times for each carrier.

Meaningful System

A meaningful system must be in place to track and verify accuracy of driver's records of duty status/time records. This shall include records with dates, times, locations and vehicle driven.

A driver is required to have in possession a record of duty status for the previous 7 consecutive days unless an exception from regulation is granted

Retention of Record of Duty Status (Logs)

Record of duty status, supporting documents and time cards must be maintained and retained by the carrier for a period of six months.

Automatic on-board recording devices

Automatic on-board recording devices must provide immediate status verification of time and sequence of duty, status changes, current hours of service and previous 7-day status. Electronic devices must follow the rules under Section 395.15.

Roadside Violations

Drivers found driving in excess of maximum hours or failing to keep a proper record of duty status for current day and 7 prior consecutive days shall be put out of service. A driver that is out of service (OOS) shall not be required or permitted to drive and a driver may not drive a commercial motor vehicle until the driver has hours available.

Short-haul operations: (100 Air Mile radius exemption)

Drivers are exempt from keeping logbooks if all of the following requirements are met:

- Operates within 100 air-mile radius (115.08 miles) of the normal work reporting location;
- Returns to the same work reporting location and is relieved of duties within 12 hours;
- Does not exceed 11 hours driving;
- Ten consecutive hours off duty separate each 12 hours on duty; and
- Carrier maintains for a period of 6 months, true and accurate time records showing; start, end and total hours worked. Total time for preceding seven days if driver is used for the first time or intermittently.
- **A driver may extend the 14 hour rule to 16 hours once every 7/8 day period, provided he does not drive after the 16th hour after coming on duty and keeps a log for that day.**
- **If the time card exception is not met a logbook must be prepared.**

150 air-mile provision for non-CDL drivers

A Non-CDL driver is exempt from the requirements of 395.3(a)(2) and 395.8 and ineligible to use the provisions of 395.1(e)(1), (g), and (o) if:

- Operates within 150 air-mile radius (172.06 miles) of the normal work reporting location;
- The driver returns to the normal work reporting location at the end of each duty tour; and
- The driver does not exceed 11 hours maximum driving time following 10 consecutive hours off duty; and
- Does not drive after the 14th hour after coming on duty 5 days of any period of 7 consecutive days; and
- Does not drive after the 16th hour after coming on duty on 2 days of any period of 7 consecutive days; and
- Carrier maintains for a period of **six months**, true and accurate time records showing; start and ending times, total hours worked, total time for preceding seven days if driver is used for the first time or intermittently.

If the time card exception is not met a logbook must be prepared.

Rest Period Options

The driver may meet the rest requirements using one of the four options described in the table below:

Option 1	Off Duty Time 395.1(g)(1)(i)(A)(1)	At least 10 <u>consecutive</u> and <u>uninterrupted</u> hours off duty.
Option 2	Continuous Sleeper Berth Provision 395.1(g)(1)(i)(A)(1)	At least 10 <u>consecutive</u> and <u>uninterrupted</u> hours in the sleeper berth.
Option 3	Continuous Off Duty and sleeper Berth Provision 395.1(g)(1)(i)(A)(3)	At least 10 consecutive hours sleeper berth and off duty time combined and <u>uninterrupted</u>.
Option 4	Sleeper Berth Provision 395.1(g)(1)(i)(A)(4) and 395.1 (g)(1)(ii)	The equivalent of at least 10 consecutive hours off duty (equivalent means at least 8 hours but less than 10 consecutive hours in a sleeper berth <u>and</u> a separate period of at least 2 but less than 10 consecutive hours either in the sleeper berth or off duty, or any combination of both).

Agricultural Provisions

The provisions of Part 395 do not apply to drivers transporting agricultural commodities or farm supplies for agricultural purposes if such transportation is limited to an area within a **150** air-mile radius from the source of the commodities or the distribution point for the farm supplies.

Ground water well drilling operations and construction materials and equipment

- (1) In the instance of driver of a commercial motor vehicle used exclusively in the transportation of oilfield equipment, including the stringing and picking up of pipe used in pipelines, and servicing of the field operation of the natural gas and oil industry, any period of 8 consecutive days may end with the beginning of any off-duty period of 24 or more successive hours.
- (2) In the case of specially trained drivers of commercial motor vehicles which are specially constructed to service oil wells, on-duty time shall not include waiting time at a natural gas or oil well site; provided that all such time shall be fully and accurately accounted for in records to be maintained by the motor carrier. Such records shall be made available upon request of the Federal Motor Carrier Safety Administration.
- (3) "Waiting time" for certain drivers at oil fields must be shown on log book or electronic equivalent as off duty and identified my annotations in "remarks" or a separate line added to "grid".

Note: Water trucks, vacuum trucks and those vehicles which can be utilized in other areas of transportation are not allowed to use the exception relating to waiting time at the well site.

Utility Service Vehicles

The provisions of Part 395 do not apply to a driver of a utility service vehicle as defined in 395.2:

- (1) Used in the furtherance of repairing, maintaining, or operating any structures or any other physical facilities necessary for the delivery of public utility services, including the furnishing of electric, gas, water, sanitary sewer, telephone and television cable or community antenna service;
- (2) While engaged in any activity necessarily related to the ultimate delivery of such public utility services to consumers, including travel or movement to, from, upon, or between activity sites (including occasional travel or movement outside the service area necessitated by any utility emergency as determined by the utility provider); and
- (3) Except for an occasional emergency use, operated primarily within the service area of a utility's subscribers or consumers, without regard to whether the vehicle is owned, leased, or rented by the utility.

Over Hours and/or false log

Drivers found driving in excess of maximum hours or failing to keep a proper record of duty status for current day and 7 prior consecutive days shall be put out of service. A driver that is out of service (OOS) shall not be required or permitted to drive and a driver may not drive a motor vehicle until the driver has hours available.

Penalties

Drivers (or allowing a driver to drive) 3 or more hours beyond the driving-time limit maybe considered egregious violation and subject to the maximum civil penalties. Also applies to passenger-carrying drivers.

Time Card – Log Book Combination (Part 395)

Company Name: _____ City: _____
Driver: _____ Date: _____
Job Name: _____ Fuel: _____
Truck # _____ Pup # _____ Total Miles: _____ Lunch: _____
Start Time: _____ End Time: _____ Total Hours: _____

Ticket #	Time Loaded	Weight/Time
1		
2		
3		
4		
5		
6		
7		
8		
9		
10		
11		
12		
13		
14		
15		
Total		

DRIVERS INSPECTION REPORT

Oil _____ Antifreeze _____ Mileage _____

Power Unit

- | | |
|---|--|
| <input type="checkbox"/> Service brakes including trailer connections | <input type="checkbox"/> Steering mechanism |
| <input type="checkbox"/> Parking (hand) brake | <input type="checkbox"/> Tires |
| <input type="checkbox"/> Lighting devices & reflectors | <input type="checkbox"/> Windshield wipers |
| <input type="checkbox"/> Horn | <input type="checkbox"/> Coupling devices |
| <input type="checkbox"/> Rear vision mirrors | <input type="checkbox"/> Emergency equipment |
| | <input type="checkbox"/> Wheel and rims |

Towed Unit(s)

- | | |
|---|---|
| <input type="checkbox"/> Service brakes including connections | <input type="checkbox"/> Tires |
| <input type="checkbox"/> Brakes | <input type="checkbox"/> Coupling devices |
| <input type="checkbox"/> Lighting devices & reflectors | <input type="checkbox"/> Cargo securement |
| <input type="checkbox"/> Wheels and rims | |

Reporting Driver: _____

Maintenance Action:

- | | |
|---------------------------------------|--|
| <input type="checkbox"/> Repairs Made | <input type="checkbox"/> No Repairs Needed |
| | Date: _____ |

Mechanics Signature: _____

Reviewing Driver: _____ Date: _____

DRIVER'S DAILY LOG
One calendar day - 24 hours

ORIGINAL - File each day at home terminal
DUPLICATE - Driver retains possession for one month

(Month)

(Day)

(Year)

(Total miles driving today)

(Vehicle numbers - (Show each unit))

I certify these entries are true and correct:

(Name of Carrier or Carriers)

(Main Office Address)

(Driver's signature in full)

(Name of co-driver)

	MID-NIGHT	1	2	3	4	5	6	7	8	9	10	11	NOON	1	2	3	4	5	6	7	8	9	10	11	TOTAL HOURS
1. OFF DUTY																									
2. SLEEPER BERTH																									
3. DRIVING																									
4. ON DUTY (Not Driving)																									
REMARKS																									

Pro or Shipping No. _____

DRIVERS TIME RECORD FOR A PROPERTY CARRYING COMMERCIAL DRIVER

Driver's Name (print) _____ Employee No. _____ Month _____ Year _____

Driver's may prepare this report instead of the "drivers daily log" if the following applies:

- The driver operates within a 100 air-mile radius of the normal work reporting location; and
- The driver returns to the work reporting location and is released from work within 12 consecutive hours; and
- The driver has at least 10 consecutive hours off duty separating each 12 hours on duty; and
- The driver does not exceed 11 hours maximum driving time following 10 consecutive hours off duty; and
- The motor carrier that employs the driver maintains and retains for a period of 6 months accurate and true time records showing:
 - The time the driver reports for duty each day;
 - The total number of hours the driver is on duty each day;
 - The time the driver is released from duty each day; and
 - The total time for the proceeding 7 days in accordance with Part 395.8(j)(2) for drivers used for the first time or intermittently.

The driver may extend the 14 hour rule to 16 hours 1 day in a 7/8 day period provided he does not drive after the 16th hour of coming on duty and keeps a log for that day.

Date	Start Time All Duty	End Time All Duty	Total Hours	Truck #	Driving CMV Yes/No	DVIR Yes/No	Comments
1							
2							
3							
4							
5							
6							
7							
8							
9							
10							
11							
12							
13							
14							
15							
16							
17							
18							
19							
20							
21							
22							
23							
24							
25							
26							
27							
28							
29							
30							
31							

DRIVERS TIME RECORD FOR A PROPERTY CARRYING NON-CDL COMMERCIAL DRIVER

Driver's Name (print) _____ Employee No. _____ Month _____ Year _____

Driver's may prepare this report instead of the "drivers daily log" if the following applies:

- The driver operates within a 100 air-mile radius of the normal work reporting location; and
- The driver returns to the work reporting location and is released from work within 12 consecutive hours; and
- The driver has at least 10 consecutive hours off duty separating each 12 hours on duty; and
- The driver does not exceed 11 hours maximum driving time following 10 consecutive hours off duty; and
- **The driver does not drive after the 14th hour of coming on duty 5 days of any period of 7 consecutive days: and**
- **The driver does not drive after the 16th hour of coming on duty 2 days of any period of 7 consecutive days and**
- The motor carrier that employs the driver maintains and retains for a period of 6 months accurate and true time records showing:
 - The time the driver reports for duty each day;
 - The total number of hours the driver is on duty each day;
 - The time the driver is released from duty each day; and
 - The total time for the proceeding 7 days in accordance with Part 395.8(j)(2) for drivers used for the first time or intermittently.

Date	Start Time All Duty	End Time All Duty	Total Hours	Truck #	Driving CMV Yes/No	DVIR Yes/No	Comments
1							
2							
3							
4							
5							
6							
7							
8							
9							
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29							
30							
31							

DRIVER STATEMENT OF ON-DUTY HOURS

INSTRUCTIONS: Motor carriers, when using a driver for the first time or intermittently, shall obtain from the driver a signed statement giving the total time on duty during the immediately preceding 7 days and time at which the driver was last relieved from duty prior to beginning work for the motor carrier as per Part 395.8(j)(2) Federal Motor Carrier Safety Regulations (FMCSR). NOTE: Hours for any compensated work during the preceding 7 days, including work for a non-motor carrier entity, must be recorded on this form.

Drivers Name (Print): _____ Drivers License Number _____

Class: _____ Endorsement: _____ Restriction (s): _____

Type of License: _____ Issuing State: _____

DAY	1 (Yesterday)	2	3	4	5	6	7	
DATE								
HOURS WORKED								TOTAL HOURS

I hereby certify that the information given above is correct to the best of my knowledge and belief and that I was

last relieved from work at: Time: _____ Day: _____ Month: _____ Year _____

Driver's Signature: _____ Date: _____

INSTRUCTIONS: When employed by a motor carrier, a driver must report to the carrier all on duty time including time working for other employers. The definition of on duty time found in Section 395.2(8)(9) of the FMCSR's includes time performing any other work in the capacity of, or in the employ or service of, a common, contract or private motor carrier, also performing any compensated work for any non-motor carrier entity.

Are you currently working for another employer? ☐ Yes ☐ No

At this time do you intend to work for another employer while still employed by this company? ☐ Yes ☐ No

I hereby certify that the information given above is true and I understand that once I become employed with this company, if I begin working for any additional employer's for compensation that I must inform this company immediately of such employment activity.

Drivers Signature _____

Date _____

Chapter 12

Hours of Service-Passenger Carriers

Title 49 CFR, Part 395

Definitions

“On duty time” is all time a driver spends performing work, or being ready to work, until being relieved by the carrier of all responsibility. “On duty” time also includes any compensated work performed by the driver for a non-motor carrier entity. On duty time does not include time spent resting in or on a parked vehicle; (also applies to passenger-carrying drivers); any time spent resting in a sleeper berth; or up to 2 hours riding in the passenger seat of a property-carrying vehicle moving on the highway immediately before or after 8 consecutive hours in sleeper-berth.

“Driving time” is all time spent at the driving controls of a commercial motor vehicle in operation.

“Off duty” means the driver has been relieved of all responsibilities and is not ready for work.

“Record of duty status” is the tracking of work hours by means of a graph, grid, log or an automatic on-board recording device.

“Time record” is a time card or other record showing the time the driver reports for duty each day, total hours the driver is on duty each day, time the driver is released from duty each day, and total time for preceding days if the driver is used for first time or intermittently.

A motor carrier may not permit or require a driver to drive and no driver shall drive after:

10 hours driving time following 8 consecutive hours off-duty; being on duty 15 hours following 8 consecutive hours off duty; being on duty 60 hours in a 7 day period; being on duty 70 hours in any period of 8 consecutive days if the carrier operates every day of the week.

Time spent by a driver in a compensated non-motor carrier position is considered on duty time and must be included in the 15 hour and 60/70 hour rules. (Example: a part time position is a retail store.)

Drivers Record of Duty Status (Logs)

Motor carriers shall require every driver to record duty status in duplicate for each 24-hour period.

A record of duty status must:

- Be on specific grid or automatic on-board recorder;
- Be current to the last change of duty status;
- Have legible entries in driver’s own handwriting;
- Show month, day, and year with total miles driven and
- Total hours in each duty status for each 24 hours of record;
- Show vehicle number, or state and license number of each vehicle operated during that 24-hour period;

- Have carrier(s) name, who the work was performed for, and beginning and finishing times for each carrier.

Meaningful System

A meaningful system must be in place to track and verify accuracy of driver's records of duty status/time records. This shall include records with dates, times, locations and vehicle driven.

A driver is required to have in possession a record of duty status for the previous 7 consecutive days unless an exception from regulation is granted

Retention of Record of Duty Status (logs)

Record of duty status, supporting documents and time cards must be maintained and retained by the carrier for a period of six months.

Automatic on-board recording devices

Automatic on-board recording devices must provide immediate status verification of time and sequence of duty, status changes, current hours of service and previous 7-day status. Electronic devices must follow the rules under Section 395.15.

Roadside Violations

Drivers found driving in excess of maximum hours or failing to keep a proper record of duty status for current day and 7 prior consecutive days shall be put out of service. A driver that is out of service (OOS) shall not be required or permitted to drive and a driver may not drive a motor vehicle until the driver has hours available.

Short-haul operations: (100 Air Mile radius exemption)

Drivers are exempt from keeping logbooks if all of the following requirements are met:

- Operates within 100 air mile radius (115.08 miles) of the normal work reporting location.
- Returns to the work reporting locations and is relieved of duties within 12 hours.
- Does not exceed 10 hours driving.
- Eight consecutive hours off duty separate each 12 hours on duty, and
- Carrier maintains for a period of 6 months, true and accurate time records showing; start and ending times, total hours worked, total time for preceding seven days if driver is used for the first time or intermittently.
- **A driver may extend the 14 hour rule to 16 hours once every 7/8 day period, provided he does not drive after the 16th hour after coming on duty and keeps a log for that day.**
- **If the time card exception is not met a logbook must be prepared**

Over Hours and/or false log

Drivers found driving in excess of maximum hours or failing to keep a proper record of duty status for current day and 7 prior consecutive days shall be put out of service. A driver that is out of service (OOS) shall not be required or permitted to drive and a driver may not drive a motor vehicle until the driver has hours available.

Chapter 13

Maintenance File Requirements

Title 49 CFR, Part 396

Vehicle inspections, repair and maintenance are critical to the safe operation of commercial motor vehicles. They are designed to reduce accidents, injuries and fatalities resulting from unsafe vehicles operating on the highways. Maintenance standards cover systematic maintenance, pre-trip/post-trip inspection reports and annual inspections.

General Standards

393 (Parts and Accessories) and 396 (Inspection, Repair and Maintenance)

- A carrier is responsible for ensuring that it properly inspects repairs and maintains vehicles under its control.
- A motor vehicle may not be operated when its mechanical condition is likely to cause an accident or breakdown.
- Parts and accessories must be in safe operating condition at all times.
- A vehicle must be maintained according to the vehicle manufacture's recommended schedule, or an improved schedule based on actual operating conditions, and
- Push out windows, emergency doors and emergency door markings, and lights in buses must be inspected at least every 90 days.

Required records

For each vehicle a carrier controls for 30 consecutive days or more, the carrier must ensure the proper vehicle maintenance records are maintained. Each vehicle record must contain:

- Vehicle identification including company number, make, serial number, year and tire size.
- If the vehicle is leased, the person furnishing the vehicle must be identified.
- A means to indicate the nature and due date of various inspection and maintenance operations to be performed.
- A record or log of inspections, repair and maintenance indicating their date and nature.

A record of tests conducted on push out windows, emergency doors, and emergency door marking lights on buses.

Where records must be maintained?

Vehicle maintenance records must be retained where the vehicle is maintained for a period of one year and for six months after the vehicle leaves the carrier's control.

Roadside Inspection Report

Any driver who receives a roadside inspection report must deliver it to the motor carrier. An official of the motor carrier is to examine the roadside inspection report and ensure that the violations or defects noted on the report are corrected before the vehicle is re-dispatched. Within 15 days after the

inspection, the carrier must sign the completed roadside inspection report to certify that all violations have been corrected and then return it to the indicated address. **A copy must be retained for 12 months from the date of inspection.**

Out of Service Vehicles

[49 CFR Part 396.9\(c\)](#)

An inspector may place a vehicle out-of-service if its mechanical condition or load is likely to cause an accident or breakdown in accordance with the CVSA Out-Of-Service criteria. An "Out-Of-Service" sticker will be placed on the Vehicle Inspection, Repair, and Maintenance vehicle and the vehicle cannot be moved until those out-of-service defects are corrected. A copy of the Out-Of-Service criteria may be obtained at <http://www.cvsa.org/>.

A driver may be disqualified for at least 90 days and fined not less than \$1,000.00 if convicted of violating an out-of-service order. An employer may be fined up to \$10,000.00 for requiring an employee to operate a commercial vehicle while it is under an out-of-service order.

All OOS violations must be corrected before the vehicle can be moved again. Any remaining violations shall be corrected before the vehicle can be re-dispatched. One inspection form shall be signed by the appropriate personnel and returned to the issuing department within 15 days, the other form should be carried in the vehicle.

Driver Vehicle Inspection Report

The motor carrier shall require a driver vehicle inspection report to be completed at the end of each day's work on each vehicle operated and the report shall cover the following:

Parking (hand) brake	Steering mechanism
Lighting devices and reflectors	Horn
Tires	Windshield wipers
Rear vision mirrors	Coupling devices
Wheels and rims	Emergency equipment
Service brakes including trailer brake connections	

The report shall identify the vehicle, any defect or deficiency discovered by the driver, which would affect the safety of the operation of the vehicle or result in its mechanical breakdown. If no defect is discovered the report shall so indicate. In all instances, the driver shall sign the report.

When defects are noted on the Driver Vehicle Inspection Report

Prior to requiring or permitting a driver to operate a vehicle, the motor carrier shall repair any defect or deficiency listed on the driver vehicle inspection report, which would likely affect the safety of operation of the vehicle. Every motor carrier or its agent shall certify on the original driver vehicle inspection

report any listed defect that has been repaired or that repair is unnecessary before the vehicle is operated again. **These reports shall be maintained for a period of three months.**

Before driving a motor vehicle the driver shall be satisfied that the vehicle is in safe operating condition, review the last driver vehicle inspection report; and sign the report if defects or deficiencies were noted by the driver who prepared the report, to acknowledge that the driver has reviewed it and that there is a certification that the required repairs have been performed.

Periodic (Annual) Inspection

Every commercial vehicle shall have a periodic (annual) inspection that must be performed at least once every 12 months. At a minimum, inspections must include all items enumerated in the Minimum Periodic Inspection Standards, Appendix G, Subchapter B, Part 396. Carriers may perform required annual inspections themselves, with a qualified inspector. The original or copy of the periodic inspection report must be retained by the motor carrier for **14 months** from the report date. Utah based vehicles must be inspected at State Inspection stations unless they meet the exceptions outline in UCA 53-8-205 (1) (b) (iv). <http://www.find-laws.com/statutes/utah/title-53/chapter-08/53-8-205>

Documentation of the most recent periodic inspection must be kept on the vehicle. (Report, sticker or decal) Utah based carriers must meet the mandatory State Inspection Standards.

Inspector qualifications

A motor carrier must ensure that the individual(s) performing an annual inspection is qualified. The inspector must:

- Understand the inspection standards of Part 393 and Appendix G.
- Be able to identify defective components.
- Have knowledge and proficiency in methods, procedures and tools.
- Utah based vehicles must be inspected at approved State Inspection Stations.

Inspectors may have gained experience or training by:

- Completing a State/Federal training program, or earning a State or Canadian Province qualifying certificate in commercial motor vehicle safety inspections.
- A combination of other training or experience totaling at least a year.

Evidence of Qualifications

Evidence of the inspector's qualification must be maintained until one year after the inspector ceases to perform inspections for the carrier.

Brake Inspectors

The motor carrier is responsible for ensuring that all inspections, maintenance, repairs and service to brakes of commercial motor vehicles comply with these regulations. Employees responsible for brake inspections, maintenance, service or repairs must meet minimum brake inspector qualifications. (Sample certificate on next page)

Driver's Vehicle Inspection Report

Check any defective item and give details under "Remarks."

DATE: _____

TRUCK/TRACTOR NO. _____

- | | | |
|--|--|--|
| <input type="checkbox"/> Air Compressor | <input type="checkbox"/> Horn | <input type="checkbox"/> Springs |
| <input type="checkbox"/> Air Lines | <input type="checkbox"/> Lights | <input type="checkbox"/> Starter |
| <input type="checkbox"/> Battery | Head – Stop | <input type="checkbox"/> Steering |
| <input type="checkbox"/> Brake Accessories | Tail – Dash | <input type="checkbox"/> Tachograph |
| <input type="checkbox"/> Brakes | Turn Indicators | <input type="checkbox"/> Tires |
| <input type="checkbox"/> Carburetor | <input type="checkbox"/> Mirrors | <input type="checkbox"/> Transmission |
| <input type="checkbox"/> Clutch | <input type="checkbox"/> Muffler | <input type="checkbox"/> Wheels |
| <input type="checkbox"/> Defroster | <input type="checkbox"/> Oil Pressure | <input type="checkbox"/> Windows |
| <input type="checkbox"/> Drive Line | <input type="checkbox"/> On-Board Recorder | <input type="checkbox"/> Windshield Wipers |
| <input type="checkbox"/> Engine | <input type="checkbox"/> Radiator | <input type="checkbox"/> Other |
| <input type="checkbox"/> Fifth Wheel | <input type="checkbox"/> Rear End | |
| <input type="checkbox"/> Front Axle | <input type="checkbox"/> Reflectors | |
| <input type="checkbox"/> Fuel Tanks | <input type="checkbox"/> Safety Equipment | |
| <input type="checkbox"/> Heater | Fire Extinguisher | |
| | Flags-Flares-Fuses | |
| | Spare Bulbs & Fuses | |
| | Spare Seal Beam | |

TRAILER(S) NO.(S) _____

- | | | |
|--|---------------------------------------|------------------------------------|
| <input type="checkbox"/> Brake Connections | <input type="checkbox"/> Hitch | <input type="checkbox"/> Tarpaulin |
| <input type="checkbox"/> Brakes | <input type="checkbox"/> Landing Gear | <input type="checkbox"/> Tires |
| <input type="checkbox"/> Coupling Chains | <input type="checkbox"/> Lights-All | <input type="checkbox"/> Wheels |
| <input type="checkbox"/> Coupling (King) Pin | <input type="checkbox"/> Roof | <input type="checkbox"/> Other |
| <input type="checkbox"/> Doors | <input type="checkbox"/> Springs | |

Remarks: _____

☐ **CONDITION OF THE ABOVE VEHICLE IS SATISFACTORY**

DRIVER'S SIGNATURE _____

☐ **ABOVE DEFECTS CORRECTED**

☐ **ABOVE DEFECTS NEED NOT BE CORRECTED FOR SAFE OPERATION OF VEHICLE**

MECHANIC'S SIGNATURE _____ DATE _____

DRIVER'S SIGNATURE _____ DATE _____

BRAKE CERTIFICATION - 49 CFR, Part 396.25

A "brake inspector" is any employee of a motor carrier who is responsible for ensuring all brake inspections, maintenance, service, or repairs to any commercial motor vehicle, subject to the motor carrier's control, meet the applicable Federal standards.

No motor carrier shall require or permit any employee who does not meet minimum brake inspector qualifications to be responsible for the inspection, maintenance, service or repairs of any brakes on its commercial motor vehicles.

Minimum Qualifications

_____ Understands and can perform brake service and inspection.

_____ is knowledgeable of and has mastered the methods, procedures, tools and equipment necessary to perform brake service and inspection.

_____ is capable of performing brake service or inspection by reason of experience, training or both, and qualify in one of the following categories (check all that apply):

I. _____ Has successfully completed an apprenticeship program sponsored or approved by a State, Canadian Province, a federal agency or labor union or has a certificate from a State or Canadian Province which qualifies the person to perform brake service or inspections. Specify: _____

OR

II. _____ Has brake-related training or experience or a combination thereof totaling at least one year as follows (check all that apply):

_____ Participation in a brake maintenance or inspection training program sponsored by a brake or vehicle manufacturer or similar commercial training program. Where and Date: _____

_____ (years) experience performing brake maintenance or inspection in a motor carrier maintenance program. Name and Date: _____

_____ (years) experience performing brake maintenance or inspection at a commercial garage, fleet leasing company, or similar facility. Name of Facility and Dates: _____

I certify the above information is true and accurate to the best of my knowledge:

Employee _____
Signature of Mechanic/Inspector Date

Motor Carrier/Company _____
Signature of Employer/Supervisor Date

Evidence of Inspector's Qualifications on file at: _____

Chapter 14

Commercial Driver's License (CDL)-Part 383

Title 49 CFR, Part 383

Standards

The licensing provisions in Part 383 are intended to help reduce accidents by setting standards that:

- Require commercial drivers to be properly qualified and to hold a single valid driver's license (CDL); and
- Disqualify drivers who do not operate commercial motor vehicles (CMV) safely.

Commercial Driver's License Information System (CDLIS)

The CDLIS enables the State to exchange information about the driving records and driver's licenses of CMV drivers. This helps assure that only one license is issued to a driver and that drivers currently disqualified are prevented from obtaining a CDL. Employers have access to the CDLIS clearinghouse through their State's vehicle licensing agency.

Vehicles requiring commercial driver's licenses

Drivers must hold a CDL if they operate in interstate, intrastate or foreign commerce and drive a vehicle:

- With a gross vehicle weight rating (GVWR) or gross combination weight (GCVW) of at least 26,001 pounds or;
- Designed to transport at least 16 passengers including the drivers; or
- Transporting a quantity of hazardous material requiring placarding.

Because the CDL is a state-issued license, you should check with appropriate State officials regarding particular license classes and specific exemptions.

LCV Certification Requirements

CDL Drivers of Longer Combination Vehicles (LCV) must meet minimum requirements for special training through:

- LCV Driver-Training Certificate of Grandfathering, or
- LCV Driver-Training Certificate
- For more information regarding LCV requirements refer to Part 380 at::

<http://www.fmcsa.dot.gov/rules-regulations/administration/fmcsr/fmcsrcguidetails.aspx?menukey=380>

Notification to employer and licensing state

Upon conviction for any State or local traffic violations, a driver must notify his/her employer(s) within 30 days. This notification must be in writing and must include the following information:

- Driver's full name
- Driver's license number
- Date of conviction
- Details about the offense, including any resulting suspension, revocation, or cancellation of driving privileges
- Indication of whether the violation happened in a CMV
- Location of offense, and
- Driver's signature

Disqualifying Offenses

No employer shall knowingly allow, require, permit or authorize a disqualified driver to drive a CMV. Disqualifying offenses include:

- Driving a CMV while under the influence of alcohol
- Driving a CMV while under the influence of a disqualifying drug or other controlled substance
- Leaving the scene of an accident that involves a CMV
- Using a CMV to commit a felony
- Using a CMV to commit serious traffic violations
- Using a CMV to violate an Out-of-Service Order
- Using a CMV to violate the Railroad-Highway Grade Crossing rule.

Penalties

A driver convicted of a felony offense for using a CMV for manufacturing, distributing or dispensing a controlled substance is disqualified for life, but may be eligible for reinstatement after 10 years.

60-Day Suspensions

A 60-day suspension will be imposed following conviction for two serious traffic violations within three years while driving a CMV. These violations include:

- Excessive speeding (15 miles per hour or more above the posted speed limit in a single offense)
- Reckless driving, improper or erratic lane changes, or following the vehicle ahead too closely; and
- Traffic offenses involving a fatal accident

120-Day Suspensions

A 120-day suspension will be imposed following three convictions of any serious violations within three years

Implied Consent

Any CDL holder is automatically considered to have consented to alcohol testing by a State or jurisdiction

Endorsements

In addition to general knowledge and skills tests, drivers who operate specialized commercial motor vehicles must pass additional tests and obtain endorsements on the CDL, as follows:

- T-Double/triple trailers (knowledge test only)
- P-Passenger (knowledge and skills test)
- N-Tank vehicle (knowledge test only)
- H-Hazardous materials (knowledge test only)
- X-Combination of tank vehicle and hazardous materials (knowledge tests)

Air Brake Restrictions

If an applicant fails the air brake section of the knowledge test, or performs the skills test in a vehicle not equipped with air brakes, his/her CDL, if issued, will indicate that the license holder may not operate any CMV equipped with air brakes.

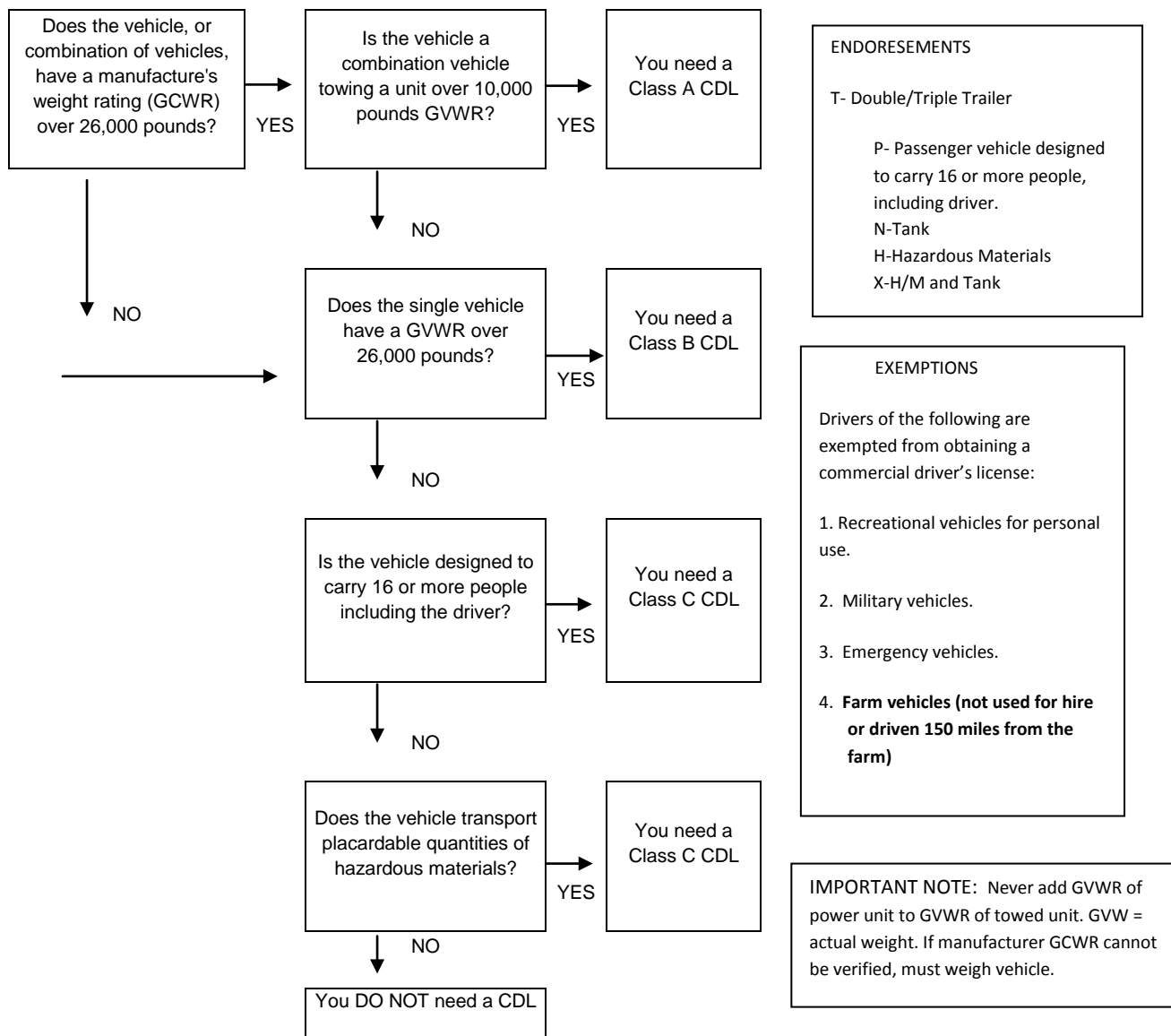
Note: For the purposes of the skills test and the license restriction, air brakes include any braking system that operates fully or partially on the air brake principle

DO YOU NEED A CDL?

There are three types of commercial driver's licenses: **Class A, B, and C**. Drivers of light vehicles – passenger cars and pickup trucks – will be issued an operator's license

- **Class A** - Combination of vehicles with a gross combined weight rating (GCWR) over 26,000 pounds, provided the GVWR of the vehicle(s) being towed is greater than 10,000 pounds. A driver with a Class A license will be able to drive Class A, B, and C commercial vehicles with proper endorsements when required.
- **Class B** – Single vehicles with a GVWR over 26,000 pounds, or any such vehicle towing a vehicle under 10,000 GVWR. A driver with a Class B license will be able to drive Class B and C Commercial vehicles with proper endorsement when required.
- **Class C** – Vehicles with a GVWR less than 26,001 pounds. Class C is strictly for vehicles designed to carry 16 or more people or carry hazardous material in quantities large enough to require a placard. A class C license holder will be able to drive Class C commercial vehicles only.

Check the chart below to see what kind of commercial driver's license you will need:



Chapter 15

Alcohol and Drug Testing Requirements

Title 49 CFR, Part 382

Most drivers of commercial motor vehicles (CMV) engaged in interstate and intrastate transportation is subject to controlled substances and alcohol testing under the Code of Federal Regulations, Title 49 Parts 40 and 382. Employers are required to have a program and procedures in place to insure that all drivers who are required to possess a commercial driver's license (CDL), *and* who operate a commercial vehicle, are test for drug and alcohol use. The purpose of controlled substance testing is to reduce highway accidents that result from driver use of these substances.

A commercial motor vehicle (CMV)

A commercial motor vehicle:

- Has a gross combination weight rating of 26,001 or more pounds inclusive of a towed unit with a gross vehicle weight rating or more than 10,000 pounds; or
- Has a gross vehicle rating of 26, 001 pounds; or
- Is designed to transport 16 or more passengers, including the driver; or
- Is of any size and is used in transportation of hazardous materials which requires a placard on the vehicle.

This law applies to:

- For hire and private companies;
- Federal, state and local governments;
- Civic and church organizations;
- Farmers and custom harvesters unless exempted from CDL;
- Apiarian industries (beekeeping)

Alcohol Definitions

"Alcohol" is the intoxication agent in beverage alcohol, ethyl alcohol, or low molecular weight alcohol including methyl and isopropyl alcohol.

"Alcohol use" is the consumption of any beverage, mixture or preparation (including medication) that contains alcohol.

Alcohol Prohibitions

The alcohol rule prohibits any alcohol use that could affect performance of a safety-sensitive function, including:

- Reporting for duty or remaining on duty to perform safety-sensitive functions with alcohol concentration of 0.04 BAC or greater;
- Use while performing safety-sensitive functions;
- Use during the 4 hours before performing safety-sensitive functions;
- Possession of alcoholic substances, unless transported and manifested as part of a shipment.
- Use during 8 hours following an accident, or until driver undergoes a post-accident test, whichever occurs first.
- Refusal to take a required test.

Tests Required

Drivers required to have a commercial driver's license (CDL) are subject to:

- Pre-employment drug testing (a verified **negative** test result)
- Post-accident drug and alcohol testing, drivers must be tested for alcohol within 2 hours, but not more than 8 hours, of the accident and within 32 hours for controlled substances. (If testing is not completed within the allotted time frame a written record stating the reason the test was not promptly administered shall be maintained on file).

Post Accident Testing Chart		
Type of accident involved	Citation issued to CMV driver	Test must be performed by employer
Human Fatality	Yes No	Yes Yes
Bodily injury with immediate medical treatment away from the scene	Yes No	Yes No
Disabling damage to any motor vehicle requiring tow away	Yes No	Yes No

- Random testing; must include more than one driver, use a scientifically valid method and ensure each driver has an equal chance of being tested each time. (50% of all driver positions must be tested for controlled substances and 10% of all driver positions must be alcohol tested in a calendar year).
- Reasonable suspicion testing (supervisor receive 60 min. each drug and alcohol training)
- Return to duty (a verified **negative** test result must be received before performing a safety sensitive function)
- Follow-up (SAP to determine a substance abuse plan to include at a minimum 6 unannounced follow-up tests in the first 12 months.)

Controlled Substance Testing

Employers are required to do a five-panel test for the following controlled substances:

- Marijuana
- Cocaine
- Opiates
- Amphetamines
- Phencyclidine (PCP)

Drug Prohibitions

A driver may not report for duty (or remain on duty) to perform safety-sensitive functions if:

- The driver uses any of the above-mentioned drugs unless prescribed by a doctor who has told the driver that the use will not adversely affect the driver's ability to operate a CMV safely.
- The driver tests positive for controlled substance use; or
- The driver refuses to take a required test

Consequences

A driver who violates any of the above provisions must:

- Not perform, nor be permitted to perform, a safety-sensitive function
- Be referred to a substance abuse professional (SAP) for evaluation to determine what assistance is needed.
- Undergo a return-to-duty test for alcohol or drugs resulting in a verified negative result.

Be subject to unannounced follow-up testing after returning to work

Access to Records

An employer **must obtain**, pursuant to a driver's written consent, the driver's alcohol and drug compliance records from previous employers for the prior 3 years. A previous employer, pursuant to a driver's written consent, **must release** a driver's alcohol and drug compliance records to a prospective employer. Carriers **must make** all records related to the administration of their testing programs and individual test results available to agencies with regulatory jurisdiction. Requests may be placed in drivers qualification file.

Medical Review Officer

A carrier must use a Medical Review Officer (MRO) to review and verify individual controlled substance test results. It is the MRO who must notify the carrier whether a

Drivers' drug test was positive or negative, and which controlled substance tested positive

Education Materials and Company Policy

Employers must provide educational materials to each driver explaining the requirements and employer policies and procedures with respect to drug and alcohol testing, and shall ensure all persons who supervise drivers receive 60 minutes training on alcohol and 60 minutes training on substance abuse as required in Part 382

Carrier Responsibility

Parts 40 and 382 standards allow carriers to contract with outside services and consortiums that assist with administration of the carrier's program. Responsibility for compliance is ultimately the responsibility of the employer.

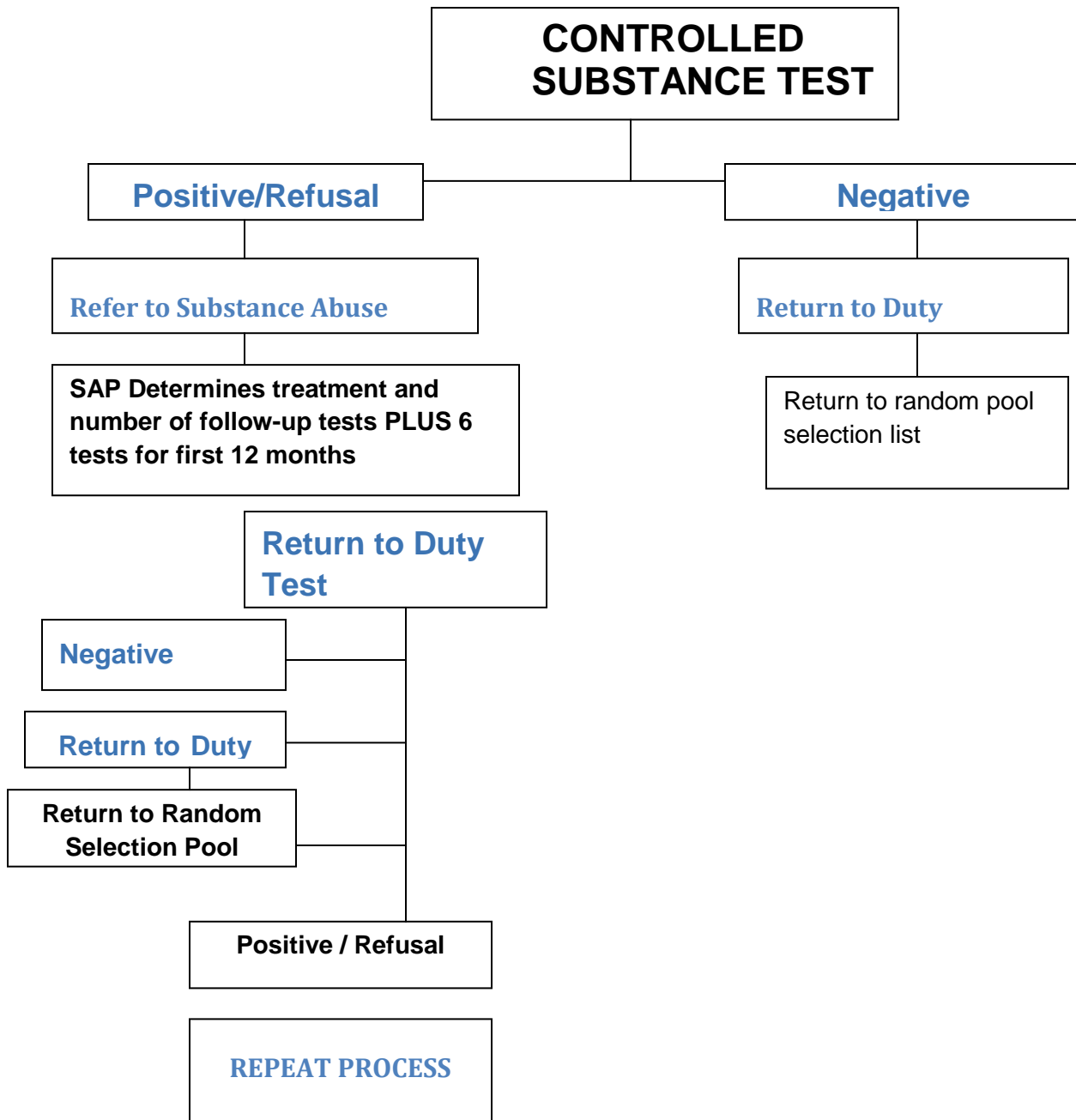
SUPERVISOR TRAINING

Persons who supervise CDL drivers are required to receive 60 minutes training on alcohol and 60 minutes training on substance abuse as required in Part 382. This required training may be obtained from a variety of sources.

CONSULTING GROUPS (SAPS)

There are a number of government agencies and private companies which specialize in mental health and rehabilitation therapies that offer Certified Substance Abuse Professional Services (SAPS). Look under “Drug Detection & Testing” in the local yellow pages or online, for a complete list of available services in your area. These companies provide various services related to drug and alcohol testing. The office of Motor Carriers does not endorse any individual program. It is the responsibility of the employers to ensure that their program is administered in compliance with Part 40 and Part 382 of the Federal Motor Carrier Safety Regulations.

CHART
Part 382



CANNOT USE DRIVER UNTIL A NEGATIVE TEST HAS BEEN RECEIVED!

Chapter 16

Hazardous Materials Regulations

Title 49 CFR Part 100-185

Applicability

The hazardous materials regulations are applicable to the transportation of hazardous materials in commerce and their offering to:

1. Interstate, intrastate, and foreign carriers by rail car, aircraft, motor vehicle and vessel.
2. The representation that a hazardous material is present in a package, container, rail car, aircraft, motor vehicle or vessel.
3. The manufacture, fabrication, marking, maintenance, reconditioning, repairing or testing of a package or container which is represented, marked, certified or sold for use in the transportation of hazardous materials (49 CFR 171.1(a)).

Who is required to have HM Registration (PHMSA)?

Persons who offer for transportation, or transport in foreign, interstate or intrastate commerce: (a) any highway route controlled quantity of a Class 7 (radioactive) material; (b) more than 55 lbs of a Division 1.1, 1.2, or 1.3 (explosive) material in a motor vehicle, rail car or freight container; (c) more than 1 L per package of a material extremely poisonous by inhalation; (d) a hazardous material in a bulk packaging having a capacity of 3,500 gallons for liquids or gases, or more than 468 cubic feet for solids; (e) a shipment in other than bulk packaging of 5,000 lbs. gross weight or more of one class of hazardous material for which the transport vehicles requires placarding; (f) any quantity of materials requiring placarding.

Hazardous Materials Safety Permits

After January 1, 2005, the Federal Motor Carrier Safety Administration (FMCSA) requires motor carriers to obtain a Hazardous Materials Safety Permit (HMSP) prior to transporting certain highly hazardous materials. An HMSP is required to transport any of the following materials.

1. A highway route-controlled quantity of a Class 7 (radioactive material);
2. More than 55 lbs. of a Division 1.1, 1.2 or 1.3 (explosive) material or any amount of a Division 1.5 (explosive) material requiring placarding under 49 CFR 172;
3. More than one liter per package of a “material poisonous by inhalation,” that meets the criteria for “hazard zone A”;
4. A “material poisonous by inhalation,” that meets the criteria for “hazard zone B,” in a bulk packaging (greater than 119 gallons);
5. A “material poisonous by inhalation,” that meets the criteria for “hazard zone C,” or “hazard zone D,” in a packaging having a capacity equal to or greater than 3,500 gallons.
6. A shipment of compressed or refrigerated liquefied methane or liquefied natural gas, or other liquefied gas with a methane content of at least 85 percent, in a bulk packaging having a capacity equal to or greater than 3,500 gallons.

If applicable, motor carriers will be required to apply for a HMSP the next time they are scheduled to file the MCS-150 form after January 1, 2005

Shipper Responsibilities

- Determine whether a material meets the definition of a “hazardous material”
- Proper Shipping Name
- Class/Division
- Identification Number
- Hazard Warning Label
- Packaging
- Marking
- Employee Training
- Shipping Paper
- Emergency Response Information
- Emergency Response Telephone Number
- Certification
- Compatibility
- Blocking and Bracing
- Security Plan
- Incident Reporting

Listed above are the major responsibilities of HM shippers. General shipper responsibilities are contained in 49 CFR 173. Identification of a hazardous material is the first step, and frequently the most difficult. Of all the shippers’ (offerors) responsibilities, the requirement to properly classify a hazardous material is very important. It is from the proper identification of the hazardous material that the other requirements are based. A list of all material regulated by the DOT is located in section 172.101.

Carrier Responsibilities

- Shipping Paper
- Placard and Mark Vehicle
- Loading and unloading
- Compatibility
- Blocking and Bracing
- Incident Reporting
- Security Plan
- Employee training

The list above contains some of the major responsibilities of HM carriers. Carrier and offeror (shipper) responsibilities frequently overlap. When a motor carrier performs a shipper function, the carrier is responsible for performing that function in accordance with 49 CFR.

HM Classes

Class 1 = Explosives

Class 2 = Gasses

Class 3 = Flammable Liquid

Class 4 = Flammable Solids

Class 5 = Oxidizing Substances; Organic Peroxides

Class 6 = Poisonous (Toxic) and Infectious Substances

Class 7 = Radioactive Material

Class 8 = Corrosives

Class 9 = Misc. Dangerous Goods

Definitions

Bulk packaging means a packaging other than a vessel or barge, including a transport vehicle or freight container, in which hazardous materials are loaded with no intermediate form of containment and which has:

1. A maximum capacity greater than 119 gallons as a receptacle for a liquid;
2. A maximum net mass greater than 882 pounds and a maximum capacity greater than 119 gallons as a receptacle for a solid;
3. A water capacity greater than 1000 pounds as a receptacle for a gas as defined in 173.115.

Consumer commodities are materials that are packaged and distributed in a form intended for, or suitable for sale through retail sales. In order to determine if a particular hazardous material may qualify as a consumer commodity, refer to the section number in Part 173 identified in column 8 of the 172.101 Table for that material.

Large packaging means a packaging that—(1) Consists of an outer packaging that contains articles or inner packaging; (2) Is designated for mechanical handling; (3) Exceeds 400 kg net mass or 450 liters (118.9 gallons) capacity; (4) Has a volume of not more than 3 cubic meters (m³) (see § 178.801(i) of this subchapter); and (5) Conforms to the requirements for the construction, testing and marking of Large Packaging as specified in subparts P and Q of part 178 of this subchapter.

Materials of Trade transportation (see 49 CFR 171.8) by highway may be accepted from many of the requirements of the HM regulations when transported in accordance with the procedures contained in 49 CFR 173.6.

ORM-D materials are materials such as a consumer commodity, which although is subject to the regulations presents a limited hazard during transportation due to its form, quantity, and packaging. Each *ORM-D* material and category of *ORM-D* material is listed in the 49 CFR 172.101 Table and 173.144.

Marking

The basic marking requirement consists of the proper shipping name and identification number of the hazardous materials contained in the package. Markings must be durable, in English and not obscured by other markings or labels.

Labeling

General labeling requirements are contained in 49 CFR Subpart E Part 172. Each person who offers for transportation or transports a hazardous material shall ensure the package is properly labeled. There are a number of exceptions to the labeling requirements contained in 172.400

Placarding

General placarding requirements are contained in 172.504. Each bulk packaging, freight container, unit load device, transport vehicle or rail car containing any quantity of hazardous materials must be placarded on each side and each end with the placards specified in Tables 1 and 2

Table 1(Placard any quantity)

Hazard class or division	Placard name
1.1	Explosives 1.1
1.2	Explosives 1.2
1.3	Explosives 1.3
2.3	Poison Gas
4.3	Dangerous When Wet
5.2 (Organic peroxide, Type B, liquid or solid, temperature controlled)	Organic Peroxide
6.1 (Inhalation Hazard, Zone A or B)	Poison Inhalation Hazard
7 (Radioactive Yellow III label only)	Radioactive

Table 2 (Placard 1,001 lbs or more or bulk amount)

Hazard class or division	Placard name
1.4	Explosives 1.4
1.5	Explosives 1.5
1.6	Explosives 1.6
2.1	Flammable Gas
3	Non-Flammable Gas
Combustible Liquid	Combustible
4.1	Flammable Solid
4.2	Spontaneously Combustible
5.1	Oxidizer
8	Corrosive 72
9	Class 9 (172.504 (f)(9))

5.2 (Other than organic peroxide, Type B,
liquid or solid, temperature controlled)Organic Peroxide

6.1 (Other than inhalation hazard, Zone A or B) Poison

6.2(None)

8Corrosive

9Class 9 (172.504 (f)(9))

ORM-D.....(None)

Required Hazardous Materials Training

Hazardous Material Training

It is the hazmat employer's responsibility to determine the adequacy of the training being presented. Training may be in any appropriate format including lecture, conference, self paced instruction, interactive video, etc. All new hazmat employees must be trained within 90 days of employment and must be supervised until properly trained. Recurrent training must take place every three years.

A record of current training, inclusive of the preceding three years, in accordance with this subpart, shall be created and retained by each hazmat employer for as long as that hazmat employee is employed.

General awareness training is intended to raise the hazmat employees' awareness of the HMR and the purpose and meaning of the hazard communication requirements. All hazmat employees must have this training

Function-specific training is intended to teach the necessary knowledge, skills and abilities for an individual's job function.

Safety training provides information concerning the hazards posed by materials in the workplace and personal protection measures

Security training must include an awareness of security risks associated with hazardous materials transportation and methods designed to enhance transportation security.

Security Plan

Each hazmat employer subject to the security plan requirements (under 172.800) is required to establish and implement a security plan. The purpose of this requirement is to enhance the security of hazardous materials transported in commerce. Employers must train each employee on the security plan and its

implementation as it pertains to each employee. Any changes implemented in the security plan requires employees to be retrained.

Your Security Plan should be a complete document and should include: (a) information on your security assessment; (b) how you address any vulnerabilities identified in the assessment; (c) what security measures you have adopted; (d) how, when, and by whom they will be implemented; (e) your organizational structure; and (f) the responsibilities of the various employee positions. In essence, your security plan is the detailed map of how you address the security assessment.

Each motor carrier should evaluate the threats it faces and its vulnerabilities based on its unique operations and facilities and should recognize that a cookie-cutter approach is not appropriate. The measures adopted by your company to address your vulnerabilities do not need to be complex or expensive to be effective, but the justification and rationale to support them needs to be sound and documented. The key to developing adequate security measures are to think "prevention." Understand that the threat is very real and try to think like a terrorist when assessing your security weaknesses. More information on security plans can be obtained at

www.fmcsa.dot.gov.

Shipping Papers and Retention

A person may not accept a hazardous material for transportation or transport a hazardous material by highway unless that person has received a shipping paper prepared in accordance with Part 172.

Shippers must retain a copy of the shipping paper for three years for hazardous waste and two years for all other hazardous materials, after the material is accepted by the initial carrier.

Carriers must retain a copy of the shipping paper for one year after the material is accepted.

Section 3

Securement of Loads

Leave space for picture/from Trucking Guide

Chapter 17

Load Securement-UCA-72-7-709

A vehicle may not be operated or moved on any highway unless the vehicle is constructed or loaded to prevent its contents from dropping, sifting, leaking, spilling, blowing off, falling off or otherwise escaping.

Vehicles carrying dirt, sand, gravel, rock fragments, pebbles, crushed base, aggregate, any other similar material or scrap metal, shall have a covering over the entire load unless:

- The highest point of the load does not extend above the top of any exterior wall or sideboard of the cargo compartment of the vehicle; and
- The outer edges of the load are at least six inches below the top inside edges of the exterior walls or sideboards of the cargo compartment of the vehicle.
- A chemical substance capable of coating or bonding a load so that the load is confined on a vehicle, may be considered a covering for purposes of dirt, sand, gravel, rock fragments, pebbles, crushed base, aggregate, any other similar material, or scrap metal so long as the chemical substance remains effective at confining the load.
- Cargo must be contained, immobilized or secured to prevent shifting to the extent that the vehicle's stability or maneuverability is not adversely affected.

The following material is exempt from the provisions listed above:

- Hot mix asphalt;
- Construction debris or scrap metal if the debris or scrap metal is a size and in a form not susceptible to being blown out of the vehicle;
- Material being transported across a highway between two parcels of property that are adjacent except for the highway that is being crossed; and
- Material (dirt, sand, gravel, rock fragments, pebbles, crushed base, aggregate, any other similar material, or scrap metal) that is enclosed on all sides by containers, bags, or packaging.
- An authorized vehicle performing snow removal services on a highway, if the vehicle's load is screened to a particle size established by a rule of the Department of Transportation. [UCA 72-7-409\(5\)](#)

Note: This provision does not prohibit the necessary spreading of any substance connected with highway maintenance, construction, securing traction, or snow removal.

Provisions listed above do not apply to a vehicle or implement of husbandry carrying an agricultural product, if the agricultural product is: [UCA 72-7-409\(4\)\(a\)\(b\)](#)

- Being transported in a manner which is not a hazard or a potential hazard to the safe operation of the vehicle or to other highway users, and
- Loaded in a manner that only allows minimal spillage.

A vehicle with a load may not be operated on any highway unless the load and any load covering is fastened, secured, and confined to prevent the covering or load from becoming loose, detached, or in any manner a hazard to the safe operation of the vehicle, or to other highway users.

Before entering a highway, the operator of a vehicle carrying any material listed above, shall remove all loose material on any portion of the vehicle not designed to carry the material.

GENERAL INTRASTATE AND INTERSTATE OPERATIONS

Each commercial motor vehicle (CMV) transporting cargo must be loaded and equipped to prevent the cargo from leaking, spilling, blowing off or falling from the motor vehicle.

49CFR393.106 *The aggregate working load limit of tie downs used to secure an article or group of articles against movement must be at least one-half times the weight of the article or group of articles. The aggregate working load limit is the sum of:*

- One-half the working load limit of each tie down that goes from an anchor point on the vehicle to an anchor point on an article of cargo;
- One-half the working load limit of each tie down that is attached to an anchor point on the vehicle, passes through, over, or around the article of cargo, and is then attached to an anchor point on the same side of the vehicle.
- The working load limit for each tie down that goes from an anchor point on the vehicle, through, over, or around the article of cargo, and then attaches to another anchor point on the other side of the vehicle.

[Click here to see a table of working load limits.](#)

Commercial motor vehicles are subject to the cargo securement standards of this subpart and these general requirements:

- The rules in 49CFR393 Subpart I are applicable to trucks, truck tractors, semitrailers, full trailers, and pole trailers.
- Prevention against loss of load. Each commercial motor vehicle must, when transporting cargo on public roads, be loaded and equipped, and the cargo secured, in accordance with this subpart to prevent the cargo from leaking, spilling, blowing or falling from the motor vehicle.
- Prevention against shifting of load. Cargo must be contained, immobilized or secured in accordance with this subpart to prevent shifting upon or within the vehicle to such an extent that the vehicle's stability or maneuverability is adversely affected.

The minimum number of tie downs required to secure an article or group of articles against movement depends on the length of the article(s) being secured.

When an article is not blocked or positioned to prevent movement in the forward direction by a header board, bulkhead, other cargo that is positioned to prevent movement, or other appropriate blocking devices, it must be secured by at least:

- One tie down for articles 5 feet (1.52 meters) or less in length, and 1,100 pounds (500 kg) or less in weight
- Two tie downs if the article is:
 - 5 feet (1.52 meters) or less in length and more than 1,100 pounds (500 kg) in weight; or
 - Longer than 5 feet (1.52 meters) but less than or equal to 10 feet (3.04 meters) in length, irrespective of the weight.
- Two tie downs if the article is longer than 10 feet (3.04 meters), and one additional tie down for every 10 feet (3.04 meters) of article length, or fraction thereof, beyond the first 10 feet (3.04 meters) of length.

If an individual article is blocked, braced, or immobilized to prevent movement in the forward direction by a header board, bulkhead, other articles which are adequately secured or by an appropriate blocking or immobilization method, it must be secured by at least one tie down for every 3.04 meters (10 feet) of article length, or fraction thereof.

The rules in this section do not apply to a vehicle transporting one or more articles of cargo such as, but not limited to, machinery or fabricated structural items (e.g., steel or concrete beams, crane booms, girders, and

trusses, etc.) which, because of their design, size, shape, or weight, must be fastened by special methods. However, any article of cargo carried on that vehicle must be securely and adequately fastened to the vehicle.

SPECIFIC SECUREMENT REQUIREMENTS BY COMMODITY TYPE

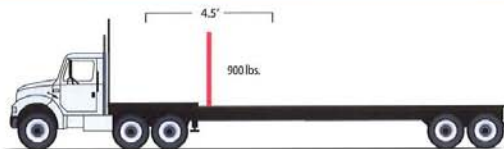
Complete information on specific securement requirements by commodity type can be found in [49 CFR 393.100 - 136](#) at the following link:

http://www.fmcsa.dot.gov/rules-regulations/administration/fmcsr/rules_search_results.aspx?Cat_type=A&keyword=393.100

- Logs
- Dressed lumber or similar building products
- Metal coils
- Paper rolls
- Concrete pipe
- Intermodal containers
- Automobiles, light trucks and vans
- Heavy vehicles, equipment and machinery
- Flattened or crushed vehicles
- Roll-on/roll-off or hook-lift containers
- Large boulders

GENERAL TIEDOWN PROVISIONS (49 CFR 393.118) NUMBER OF TIEDOWNS REQUIRED BASED ON LENGTH OF ARTICLE

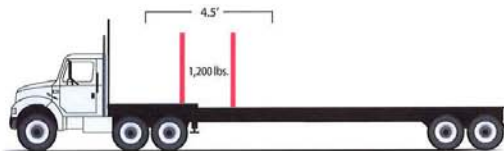
Refer to FMCSA Regulation 49 CFR 393.100-136 for complete load securement requirements



NOT BLOCKED

5 feet or less in length AND
1,100 lbs. or less in weight

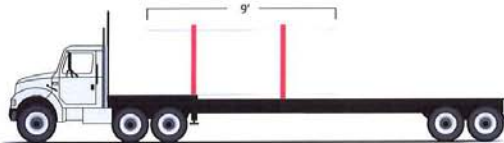
STRAPS NEEDED: 1



NOT BLOCKED

5 feet or less in length AND
more than 1,100 lbs.

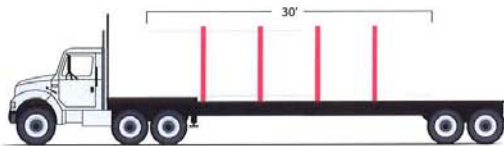
STRAPS NEEDED: 2



NOT BLOCKED

Longer than 5 feet but less
than or equal to 10 feet;
ANY weight

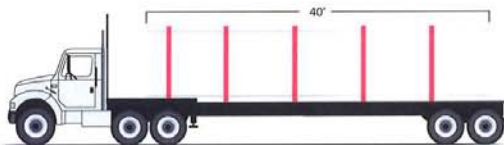
STRAPS NEEDED: 2



NOT BLOCKED

Longer than 10 feet - Needs 2 for
the first 10 feet AND 1 additional
for every 10 feet or fraction of 10 feet;
ANY weight

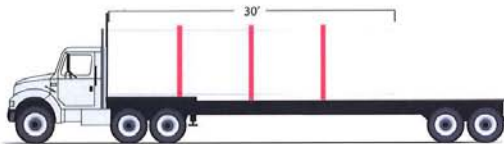
STRAPS NEEDED: At least 4



NOT BLOCKED

Longer than 10 feet - Needs 2 for
the first 10 feet AND 1 additional
for every 10 feet or fraction of 10 feet;
ANY weight

STRAPS NEEDED: At least 5



BLOCKED

One for every 10 feet or
fraction of 10 feet;
ANY weight

STRAPS NEEDED: At least 3

Note: The above examples cover tiedown requirements for length only. Weight of article and type of tiedowns also determine final number of tiedowns required.

QUESTIONS? Email: mccustomerservice@utah.gov or call (801) 965-4892 option 5

SECTION 4

Vehicle Measurement, Size/Weight and Permitting

Save space for picture/from Trucking Guide

Chapter 18

Vehicle Configuration

Vehicle Configuration Diagrams & Measuring Protocol

REMOVE METRIC (PICTURES WILL NOT ALLOW REMOVAL OF METRICS)

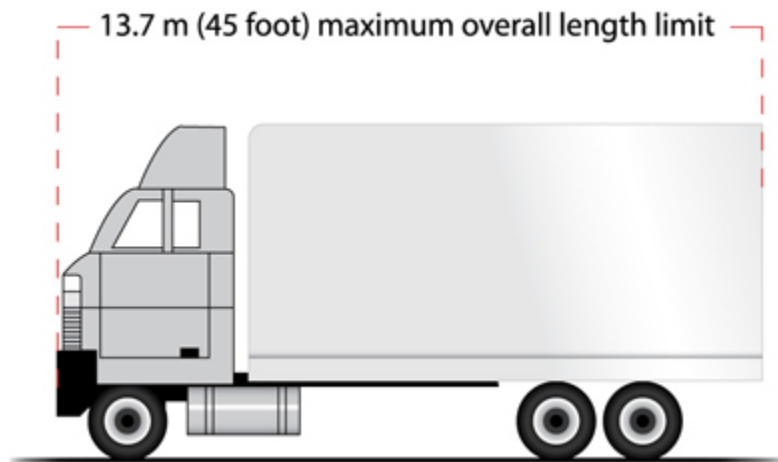
TRUCK TRACTOR-SEMITRAILER COMBINATIONS

Exception: When measuring a vehicle to determine legal dimensions, do not include appurtenances. Appurtenances include the following:

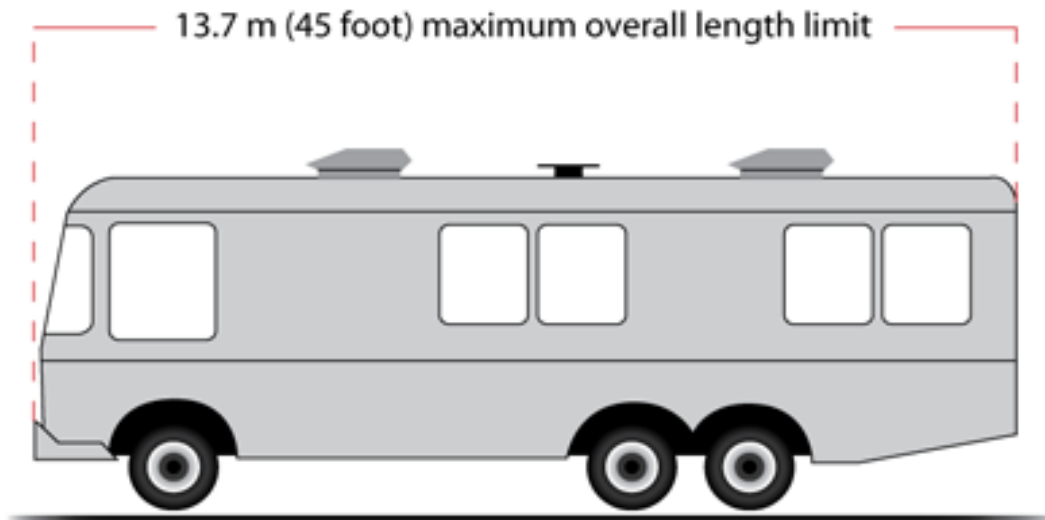
- Refrigerator units
- Hitches
- Air line connections
- Load securing devices

Note: This exception does not apply to devices designed or used to carry cargo.

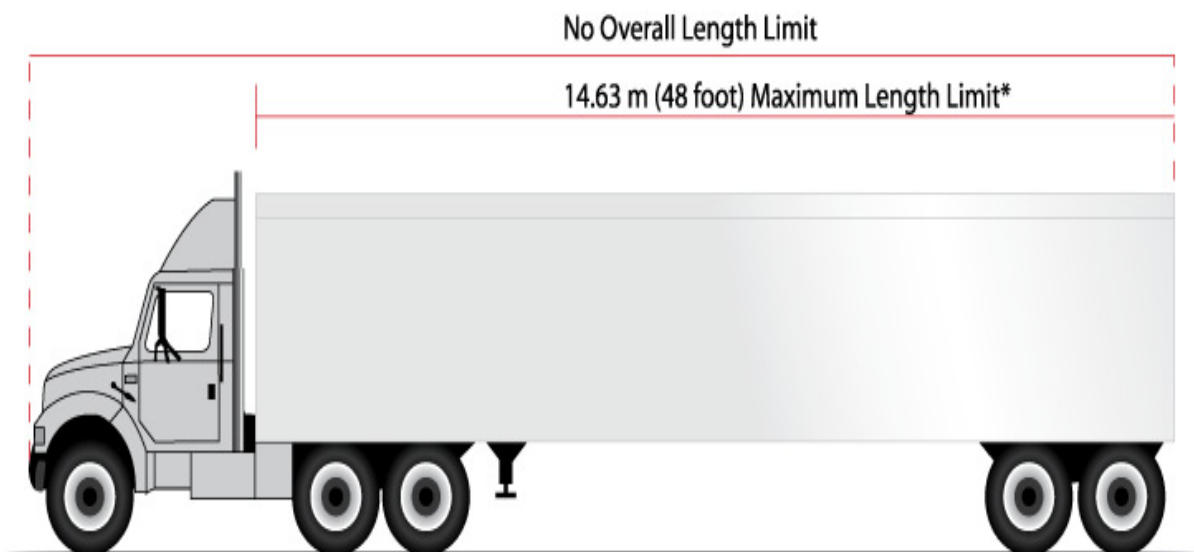
TRUCK OR STRAIGHT TRUCK AND SINGLE MOTOR VEHICLE



RECREATIONAL VEHICLE

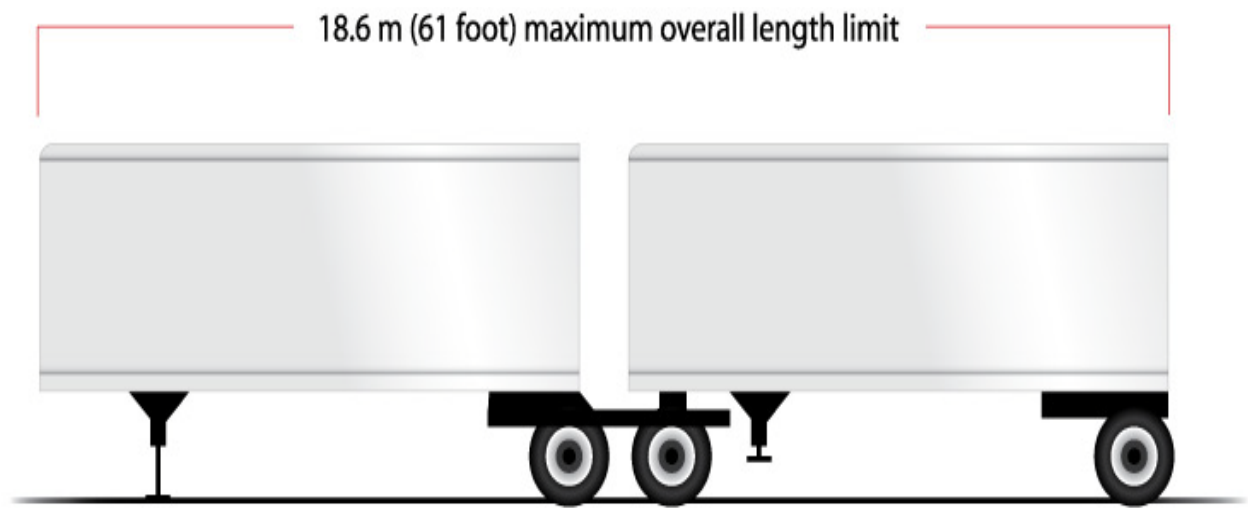


SEMI-TRAILER

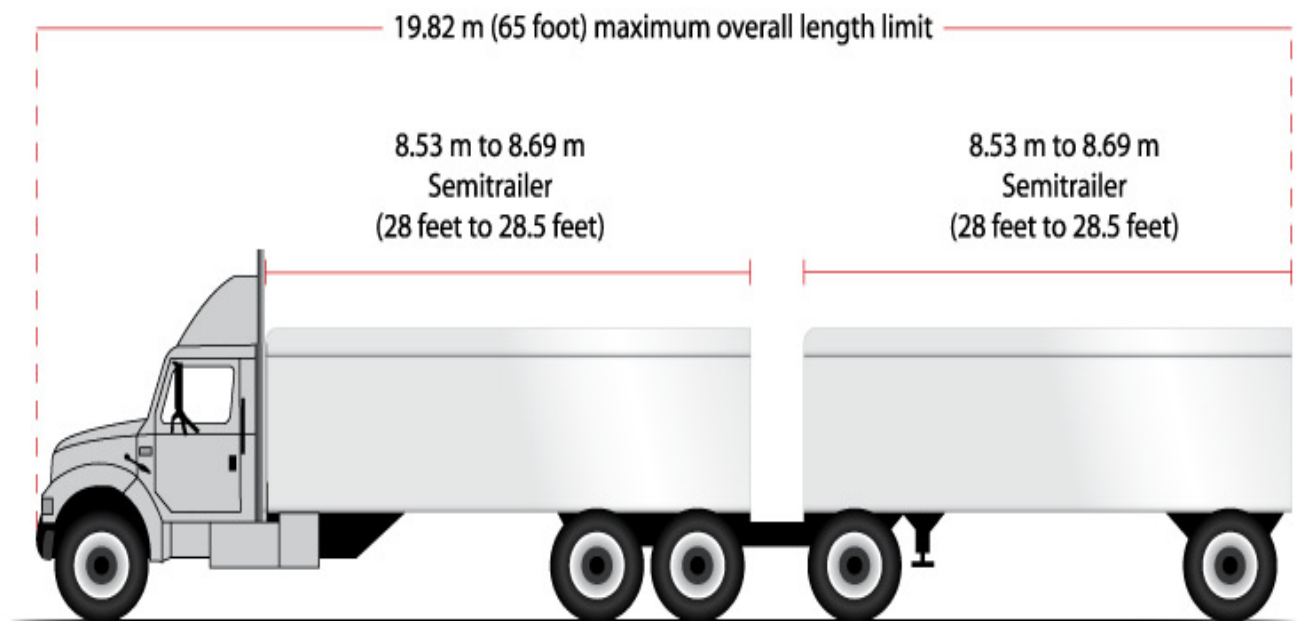


* or Grandfathered Semitrailer Length

Double Trailers



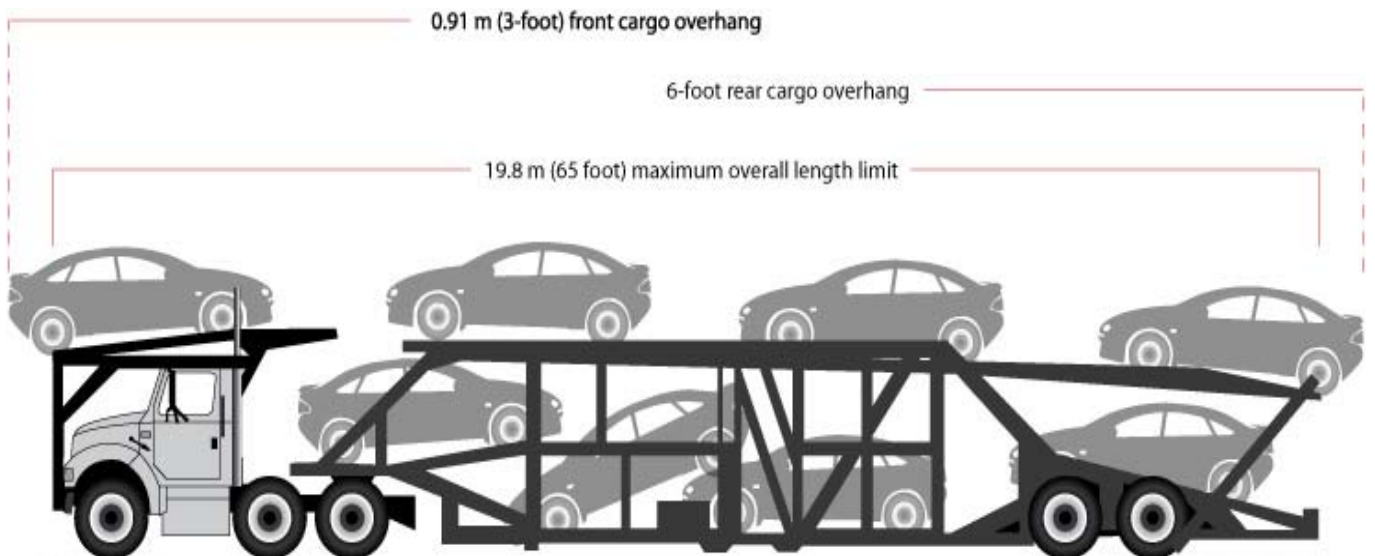
TRUCK TRAILER COMBINATIONS



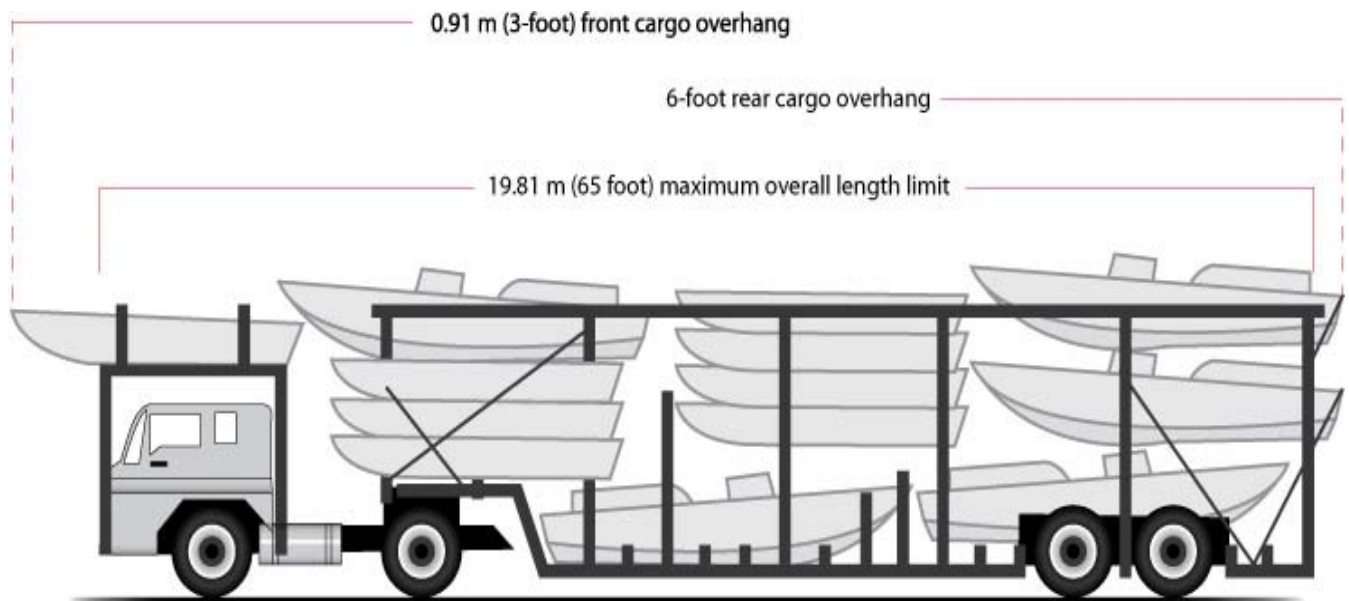
TRUCK TRAILER



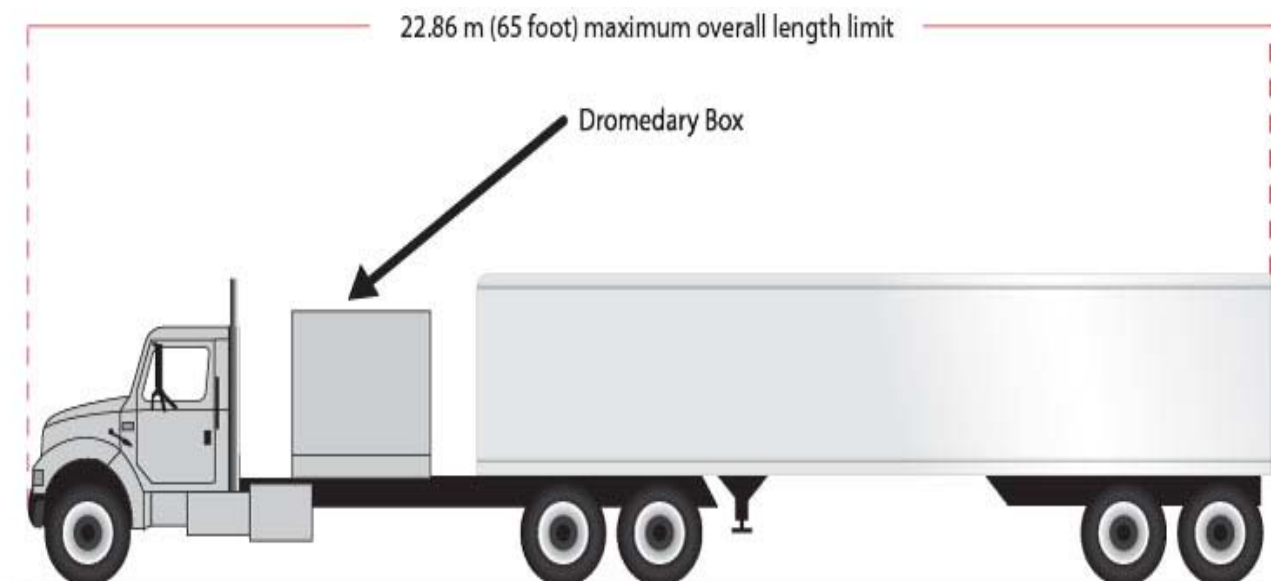
TRUCK TRAILER



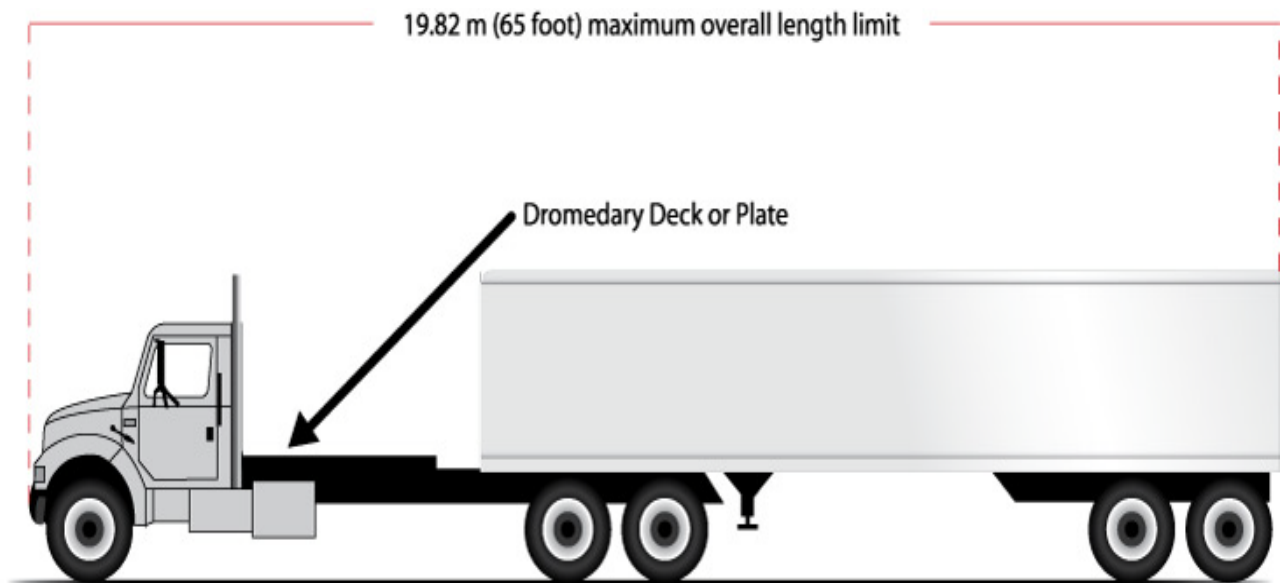
TRUCK TRAILER



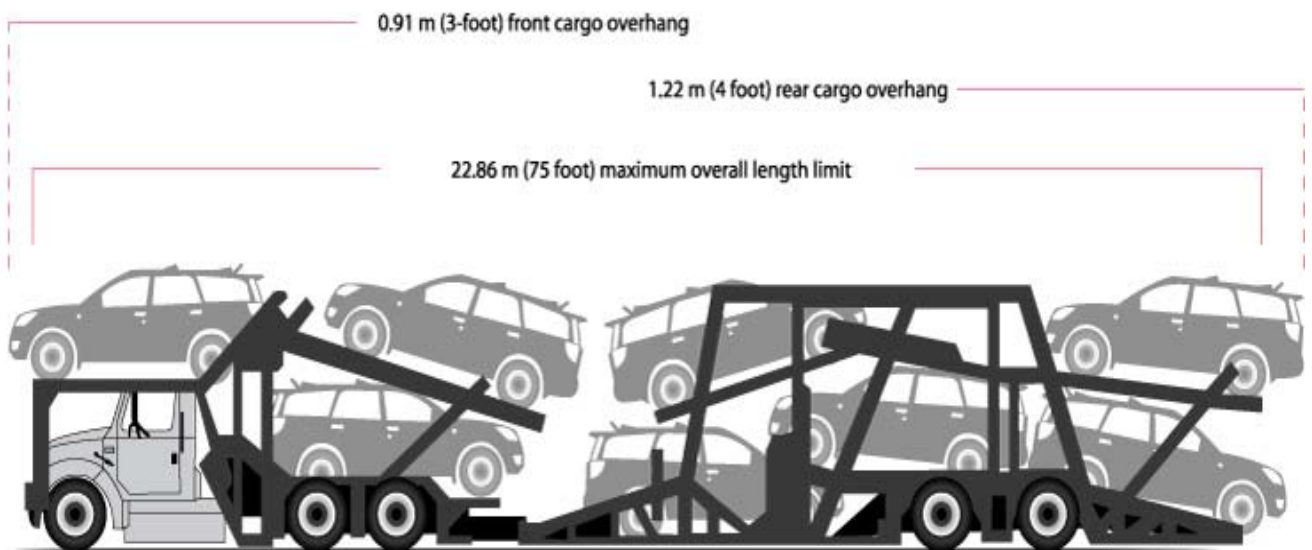
DROMEDARIES



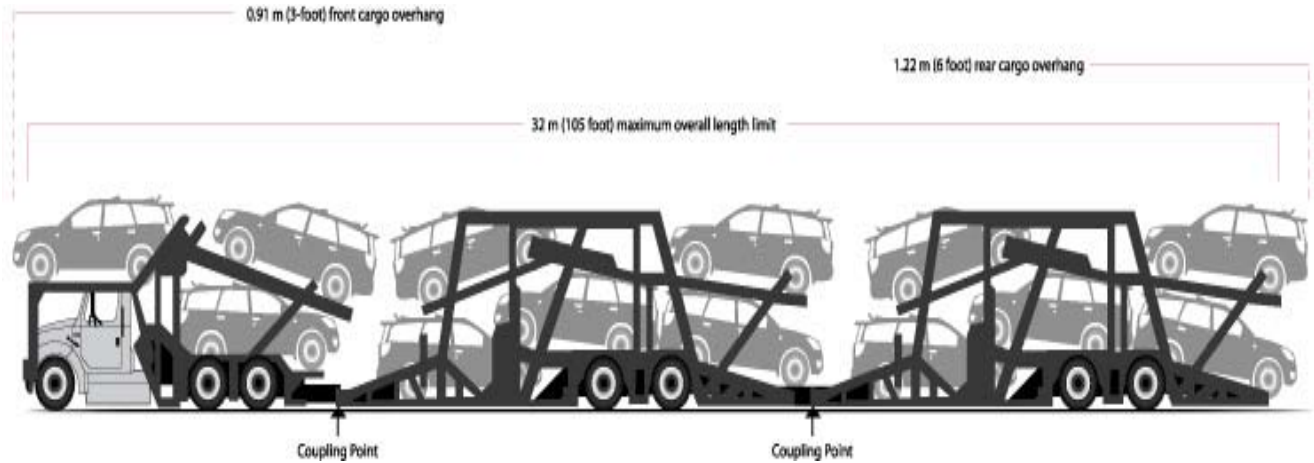
DROMEDARIES



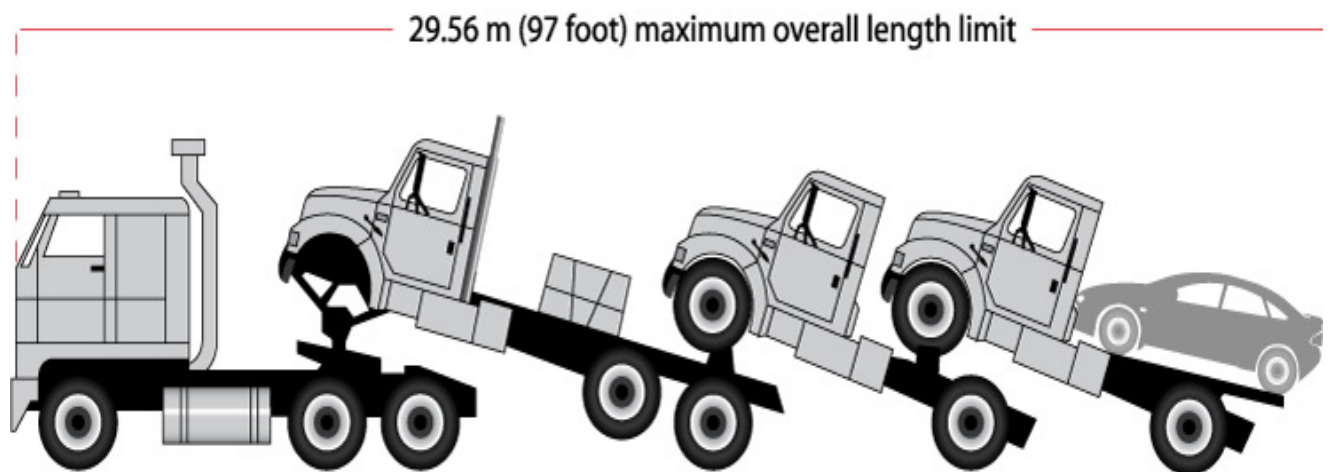
Stinger Steered Auto Transporter



Stinger Steered Auto Transporter



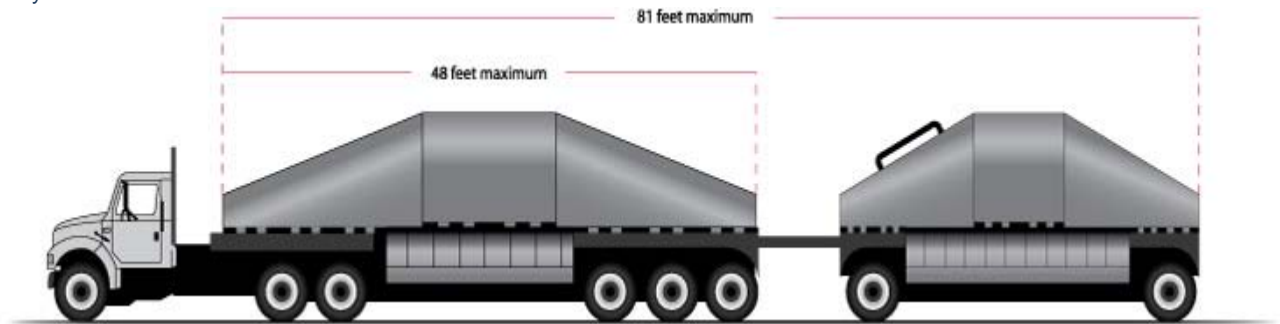
SADDLEMOUNT WITH FULLMOUNT COMBINATIONS



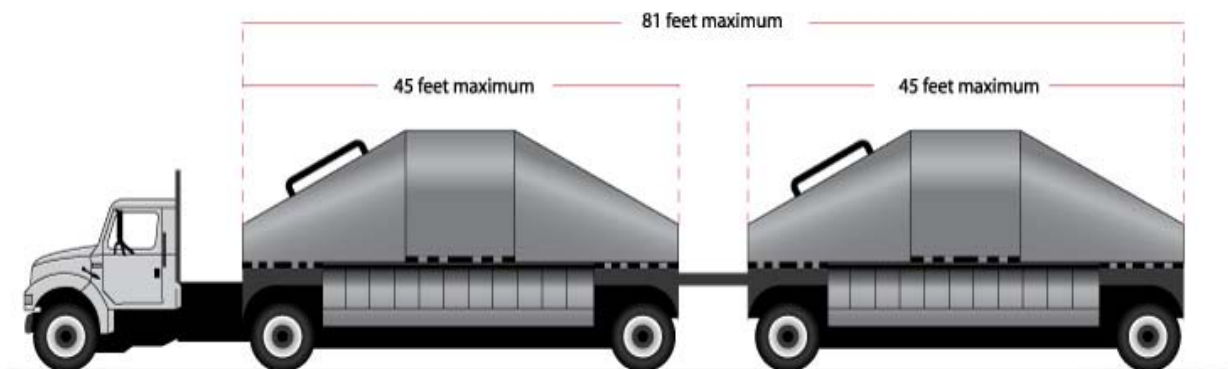
TRUCK TRACTOR-SEMITRAILER COMBINATIONS

Maximum Cargo/Cargo Carrying Length: 61' Legal, 81' Permitted, 95' with LCV authority

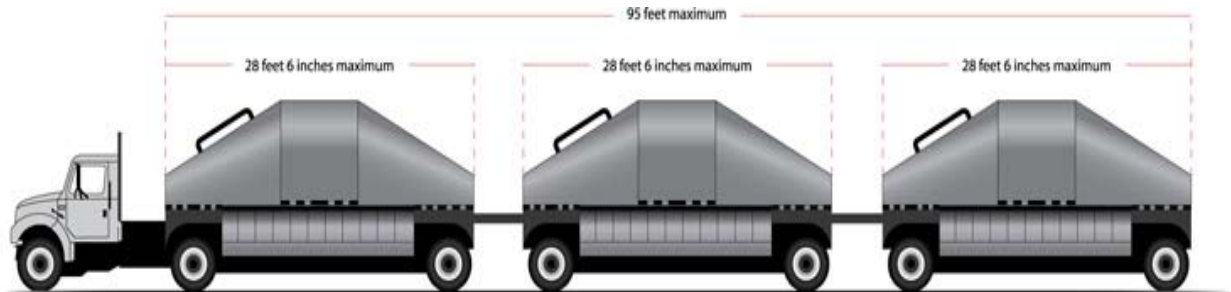
Rocky Mountain Double



Turnpike Double

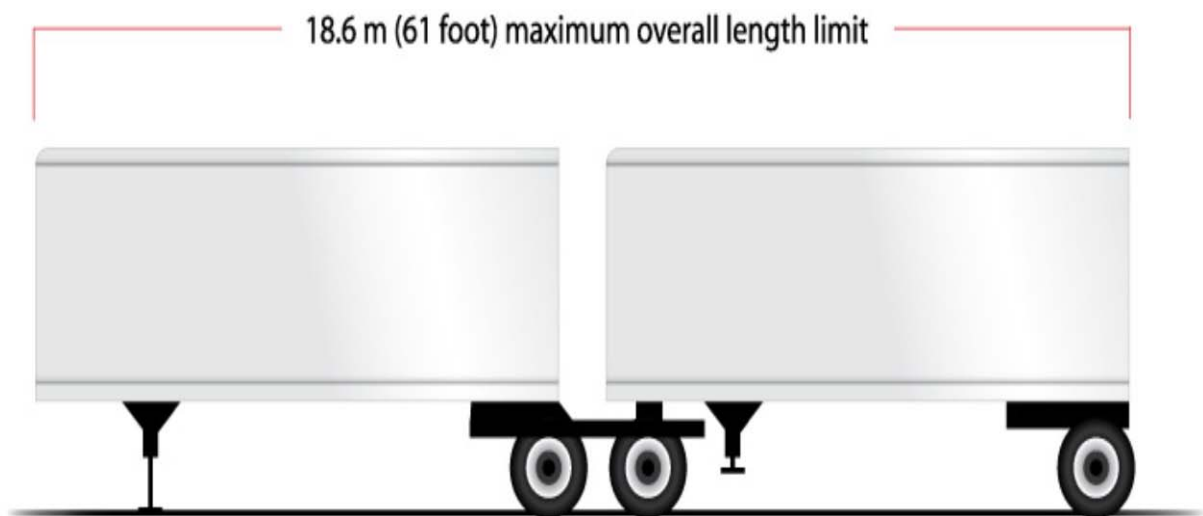


Triple Combination



Need picture of truck-2 trailer combination- legal 65 Permitted 88' LCV Authority 95'

B-TRAIN SEMITRAILER COMBINATION





Chapter 19

Legal Size Vehicle Dimensions

HEIGHT, WIDTH, & LENGTH

The following are the maximum legal vehicle dimensions, loaded or unloaded, that may be operated without special permits on Utah highways:

Height:	14'	Measured from level road surface to top of load or vehicle.
Width:	8' 6"	With or without load. Measured from outside to outside of widest extremities, excluding appurtenant devices as referenced in UCA 72-7-402(1).
Length:	Maximum lengths for various vehicles are as follows:	
Vehicle	Maximum Length (1)	Comments
Single motor vehicle	45'	Measured from bumper to bumper.
Trailer	48'	A trailer may not exceed 48 feet
Semi trailer	53'	There is no overall length limitation on a tractor and semi-trailer combination when the semi-trailer length is 53 feet or less.
Double trailer combinations	61'	Measured from the front of the first trailer to the rear of the second trailer, excluding appurtenances. There is no overall length limitation on a truck tractor and double trailer combination when the trailers coupled together measure 61 feet or less. On secondary routes the maximum number of vehicles in combination may not exceed three including the power unit.
Stinger-steered transporters	75'	Stinger steered combinations are measured from bumper to bumper. Transporters may have 3 feet of front 4 feet of rear overhang, but may not exceed 82 feet over all length.
Saddle mount	97'	This will allow a maximum of three saddle mount vehicles, one power unit and on full mount.
Truck trailer combination	65'	Measured from bumper-to-bumper.

Dromedary unit	65'	Truck tractor, unloaded box/deck and trailer. A dromedary unit is considered a truck trailer configuration whether laden or un-laden.
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75'	Dromedary units transporting Class 1 explosives or munitions-related security materials, as specified by the Department of Defense, are allowed up to 75 feet of overall length on the Interstates, US highways and reasonable access routes without requiring a permit. Reasonable access means to the interstate or US highway system.
-----	--

All other combinations, including recreational vehicles

65'

Measured from bumper to bumper.

Overhang	3' Front 6' Rear	Overhang may not carry any load extending more than 3 feet beyond the front of the power unit or more than 6 feet beyond the rear of the bed or body of the vehicle.
----------	---------------------------	--

Drawbar

15'

The drawbar or other connection between any two vehicles, one of which is towing or drawing the other on a highway, may not exceed 15 feet in length from the one vehicle to the other (measured from box to box and/or frame to frame), except in the case of a connection between any two vehicles transporting poles, pipe, machinery, or structural material that cannot be dismembered when transported upon a pole trailer.

Drawbars exceeding 15 feet in length shall be marked with retro-reflective tape the entire length of the drawbar on both the left and the right side of the drawbar.
--

Drawbars exceeding 15 feet in length shall display an amber light on both the right and the left side of the drawbar located near the center of the drawbar.
--

Drawbar in excess of 15' shall be required to obtain an oversize permit when exceeding legal limits.

Note:

When measuring a drawbar length on vehicles that have the coupling device, hitch mounted on the end of the box, bed or frame, do not include the coupling device/hitch in the measurement. An example of this would be a drawbar that when not attached to a tow vehicle, does not exceed 15 feet but when attached to the towing vehicle, the distance from frame to frame or box to frame is greater than 15 feet as a result of the coupling device/hitch.

Defining and Measuring:

Defining and Measuring Legal Height: Commercial vehicles may operate within the legal height dimension of 14 feet. This is determined by measuring from the surface of the roadway to the highest point on the load or vehicles.

Defining and Measuring Legal Width: Commercial vehicles may operate within the legal width dimension of 8 feet 6 inches. This is determined by measuring from the widest points on each side of the vehicle or load.

Defining and Measuring Legal Length:

- **Single Unit Motor Vehicle:** A single motor vehicle is a power unit that is not towing or pulling another vehicle. These vehicles may not exceed 45 feet in length. This is determined by measuring from the front-most point of the vehicle to the rear-most point of the vehicle, excluding overhangs. In most cases, this would be measured from front bumper to rear bumper.
- **Semi Trailer or Full Trailer Combinations:** Semi trailers or full trailers are vehicles that cannot be moved under their own power and are designed to carry a load while being drawn behind a power unit. The legal length for semi-trailer or full trailer is 48 feet. This is determined by measuring from the front most point of the trailer to the rear most point of the trailer and should not include any cargo with the measurement.

Note: There is no overall length limitation on a tractor and semi-trailer combination when the trailer length is 53 feet or less.

Exception: Although trailers in excess of 48 feet up to 53 feet in length exceed legal dimensions, an oversize permit is not required provided they are traveling one mile or less from and interstate or a state route system.

- **Truck Trailer Combinations:** A Truck Trailer Combination is allowed up to 65 feet of overall length. This is determined by measuring from the front bumper of the truck to the rear bumper of the trailer, including the drawbar or connecting devices. Front and rear overhangs are not included in the overall length.
- **Rear Overhang-** Determined by measuring from the rear or body of the vehicle or from the center of the closest axle in the absence of bumper/bed of vehicle.
- **Front Overhang** - Determined by measuring from the front of the power unit
- **Double Trailer Combinations:** A double trailer combination consisting of a tractor and two trailers may have a combined trailer length up to 61 feet. This is determined by measuring from the front of the lead trailer to the back of the rear trailer including the connecting equipment. This measurement is referred to as cargo carrying length. There is no overall length limit for a tractor and double trailer combination when the trailers coupled together measure 61 feet or less.
- **Stinger-Steered Transporters:** Stinger-steered transporters are considered specialized equipment and have been given additional length allowances by the Federal Highway Administration. These additional length allowances are available only when transporting automobiles, boats, or camp trailers. These industries were able to demonstrate that this vehicle type can be safely operated at these dimensions. Additionally, this allowance has provided for a more efficient mode of transportation and has increased productivity, thereby, facilitating commerce.

A stinger-steered transporter is a truck and trailer combination where the fifth wheel is located on a drop frame behind and below the rear most axle of the power unit. These units are most recognized as auto or boat transporters.

The legal length of a stinger-steered transporter is 75 feet measured from bumper to bumper. These combinations are allowed to have an additional 3 feet of front overhang and 4 feet of rear overhang.

To determine proper measuring points on these combinations when extendable ramps or flippers are used and to ensure consistency and uniformity within the Division, measurements will be taken as follows:

The cargo carrying length of 75 feet is measured from the front bumper of the power unit rearward to the rear bumper of the trailer. When flippers or ramps are used, and the front bumper of the power unit to rear bumper of the trailer measurement is less than 75 feet, the measurement will be taken from the front bumper rearward 75 feet.

Note: This 75 foot point may extend beyond the rear bumper of the trailer and be located within the extended ramp or flipper.

- The 3 feet of front overhang is measured from the front bumper of the power unit to the end of the overhang.
- The 4 feet of rear overhang is measured from the rear bumper of the trailer to the end of the overhang, or from the 75 foot point on the flipper or ramp to the end of the overhang.
- The total overall length allowed including overhangs with or without ramps or flippers may not exceed 82 feet.

Note: Stinger steered combinations exceeding 3 feet of front overhang, 4 feet of rear overhang, and 82 feet of overall length will not be permitted and corrective action will be necessary before returning to the highway. Corrective actions may include but are not limited to shifting or removing vehicles.

- **Saddle Mount Combinations:** A saddle mount configuration consists of a truck or tractor transporting up to four additional trucks, tractors, or vehicles. You should note that the industry may refer to this configuration as a "piggy back" unit. The additional trucks or tractors are connected by a saddle to the frame or 5th wheel of the power unit or preceding vehicle. A saddle mount configuration is legal up to 97 feet overall length and may consist of:
 - The power unit
 - No more than three saddle mounted vehicles, and/or
 - One fully mounted vehicle loaded without any part touching the road surface

To determine legal dimensions, measure from the front bumper of the power unit to the rear most point of the last vehicle.

Dromedary Units: A dromedary is a box, deck, or plate mounted behind the tractor's cab and forward of the fifth wheel on its frame. Truck tractor, unloaded box/deck and trailer. A dromedary unit is considered a truck trailer configuration whether laden or un-laden.

Exception: Dromedary units transporting Class 1 explosives or munitions-related security materials, as specified by the Department of Defense, are allowed up to 75 feet of overall length on the Interstates, US highways and reasonable access routes without requiring a permit. Reasonable access means any other highway that facilitates access to the Interstate or US highway system.

All Other Combinations: All other configurations not defined above are considered to be "other combinations". Maximum legal length for "other combinations" is 65 feet. This is determined by measuring from the front of the first vehicle, to the back of the rear most vehicle within the combination including connecting devices.

Exception: Appurtenances attached to the sides or rear of a recreational vehicle (non-commercial), are excluded for the purposes of measuring width and length, provided the additional width or length does not exceed 6 inches such as door handles, light fixtures and awnings

Chapter 20

Legal Weight Limitations, Axle and Tire Loads

MAXIMUM WEIGHT LIMITATIONS FOR VEHICLES

The maximum gross and axle weight limitations are as follows and may not be operated in excess of:

WHEEL, AXLE, GROSS	WEIGHT LIMITATION
Single Wheel	10,500 Pounds
Single Axle	20,000 Pounds
Tandem Axle	34,000 Pounds
Tridem Axle	Must comply with bridge formula
Gross Vehicle Weight	80,000 Pounds

An overweight permit must be obtained to authorize any exception to the maximum weight limitations listed above.

A highway authority may prohibit the operation of any vehicle or impose restrictions on the weight of a vehicle upon any highway under its jurisdiction. [UCA 72-7-408](#)

Exception: *Passenger buses may carry 24,000 lbs. on each axle provided they do not exceed manufacturer tire load ratings. This section exempts them from the 34,000 lb. tandem and bridge limitations.*

[Title 23: 658.17\(k\)](#) *Any over the road bus, or any vehicle which is regularly and exclusively used as an intrastate public agency transit passenger bus, is excluded from the axle weight limits in paragraphs (c) through (e) of this section until Oct. 1, 2009. Any State that has enforced, in the period beginning Oct. 6, 1992, and ending Nov. 30, 2005, a single axle weight limitation of 20,000 pounds or greater but less than 24,000 pounds may not enforce a single axle weight limit on these vehicles of less than 24,000 pounds.*

TIRE WIDTH

In circumstances where weight limitations are based on tire width, the manufacturer's size, as indicated on the sidewall will be used to determine maximum tire width for divisible loads only. This provision does not apply to non-divisible loads.

- The maximum allowable load for nonpermitted/legal vehicles, no tire shall exceed 600 pounds per inch of tire width as indicated on the sidewall.
- Permitted divisible configurations equipped with tires 11 inches wide or greater will be allowed 500 pounds per inch of tire width.
- Permitted divisible configurations equipped with tires less than 11 inches wide shall not exceed 450 pounds per inch of tire width.

Example: Tire sidewall reads: 265 R 22 265 is the metric width, to convert, multiply 265 by .04 which gives you 10.6. Your tire would be 10.6 inches wide.

SINGLE TIRE CONFIGURATION

The use of narrow single tires, that are less than 14 inches wide, on any combination vehicle requiring an overweight or oversize permit shall not be allowed on single axles, except for steering axles, including self steering VLS, or retractable axles, or wide base tires, that are 14 inches or greater.

All axles having a weight in excess of 10,000 pounds shall be equipped with four tires per axles, or wide base single tires.

In circumstances where weight limitations are based on tire width, the manufacturer's size, as indicated on the sidewall will be used to determine maximum tire width:

- for non-permitted or legal vehicles, no tire shall exceed 600 pounds per inch of tire width as indicated on the sidewall;
- tire loading on vehicles requiring an oversize or overweight permit shall not exceed 500 pounds per inch of tire width for tires 11 inches wide or greater;
- tires less than 11 inches wide shall not exceed 450 pounds per inch of tire width; and
- except as provided in R909-2-6, single axle loading shall not exceed 20,000 pounds, and tandem axle loading shall not exceed 34,000 pounds.

Except for steering axles, self steering VLS and retractable axles, or wide based tires, that are 14 inches wide or greater as indicated by the manufacturer's sidewall rating, all axles weighing more than 10,000 pounds shall have at least four tires per axle.

Dual or super single tires, that are 14 inches or greater, are required on all trailer axles.

(Refer to R909-2-6 Tire Load Provision for Single Tires)

Exemption: 14 inch wide single tire requirement does not apply to steering axles, or self-steering VLS retractable axles.

TIRE LOAD PROVISIONS

Except for steering axles, including self steering VLS, or retractable axles, or wide base tires, that are 14 inches or greater, the use of narrow single tires, that are less than 14 inches wide, on any combination vehicle requiring an overweight or oversize permit shall not be allowed on single axles.

All axles having a weight in excess of 10,000 pounds shall be equipped with four tires per axles, or wide base single tires.

In circumstances where weight limitations are based on tire width, the manufacturer's size, as indicated on the sidewall will be used to determine maximum tire width:

- for non-permitted or legal vehicles, no tire shall exceed 600 pounds per inch of tire width as indicated on the sidewall;
- Permitted divisible configurations equipped with tires 11 inches wide or greater will be allowed 500 pounds per inch of tire width.
- tires less than 11 inches wide shall not exceed 450 pounds per inch of tire width; and
- except as provided in R909-2-6, single axle loading shall not exceed 20,000 pounds, and tandem axle loading shall not exceed 34,000 pounds.

Except for steering axles, self steering VLS and retractable axles, or wide based tires, that are 14 inches wide or greater as indicated by the manufacturer's sidewall rating, all axles weighing more than 10,000 pounds shall have at least four tires per axle.

For example: A tridem axle group that is designed for equalized weight distribution, equipped with single tires less than 14 inches in width, will be allowed 30,000 pounds. A tandem axle group that is designed for equalized weight distribution, equipped with single tires less than 14 inches in width will be allowed 20,000 pounds. All axles in the group must be duals or super singles to be allowed maximum weight.

Dual or super single tires, that are 14 inches or greater, are required on all trailer axles. A permitted vehicle can have a single tire, less than 14 inches, but only gets 10,000 pounds per axle.

(Refer to R909-2-6 Tire Load Provision for Single Tires)

Exemption: 14 inch wide single tire requirement does not apply to steering axles, or self-steering VLS retractable axles.

VARIABLE LOAD AXLES

Vehicles with variable load axles are limited as follows:

- no more than three fixed axles shall be allowed in any group;
- retractable or variable load suspension axles installed after January 1990 shall be self steering on power units or when augmenting a tridem group on trailers;

Non-divisible loads may be exempt from these restrictions upon written approval from the division

- no axle in a group with a retractable or VLS axle shall exceed legal or bridge formula weight requirements, or the manufacturer's tire rating; and
- Controls for raising or lowering retractable or VLS axles may be located in the cab of the power unit. The pressure regulator valve shall be positioned outside of the cab and be inaccessible from the driver's compartment.

(Refer to R909-2-7 Variable Load Axles.)

Auxiliary Power Unit:

Any vehicle that utilizes an auxiliary power unit (APU) or idle reduction technology unit may be allowed up to an additional 400 lbs. total in gross, axle, tandem or bridge formula weight limits.

To be eligible for this exception the operator of the vehicle must be able to prove:

- The weight of the APU by written certification, and
- Be able to show by demonstration or certification that the idle reduction technology is fully functional at all times

Certification of the weight of the APU must be available to law enforcement officers if the vehicle is found in violation of applicable weight laws.

The additional weight allowed for the APU cannot exceed 400 lbs. or the weight certified, whichever is less.

(Refer to 23 CFR 658.17 Weight)

Chapter 21

Bridge Table Calculations

DIVISIBLE/NON-DIVISIBLE

UtahMC Online Bridge Calculator [LINK to Calculator http://utahmc.com/bridgecalc](http://utahmc.com/bridgecalc) This website will be replaced Utahmc.com is going away

This section is designed to provide background and detailed information used to determine the distribution of weight on structures. It is not just the structures we are looking at but also the road itself.

WHY THE FORMULA IS NECESSARY

Bridges on Interstate System highways are used by a wide variety of traffic. They are designed to support expected loadings. However, as trucks grew heavier in the 1950s and 1960s, something had to be done to protect bridges. The solution was to tie allowable weights to the number and spacing of axles.

Axle spacing is as important as axle weight in bridge design. A bridge is analogous to thin ice on a pond. Walking on the ice concentrates a person's weight on the small area covered by the individual's feet, and then the ice may break. Lying down, however, spreads the same weight over a much larger area, and the ice is less likely to break. Consider trucks crossing a bridge:

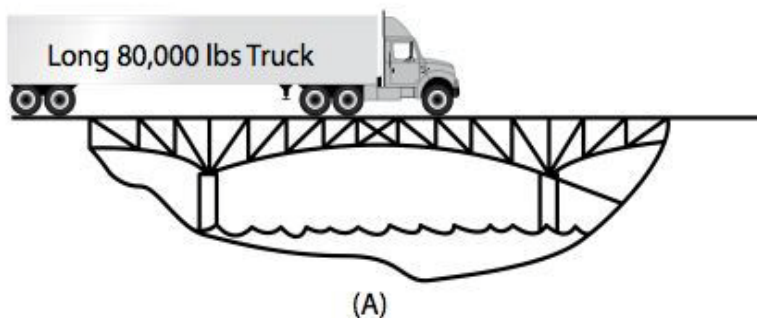


Figure 1

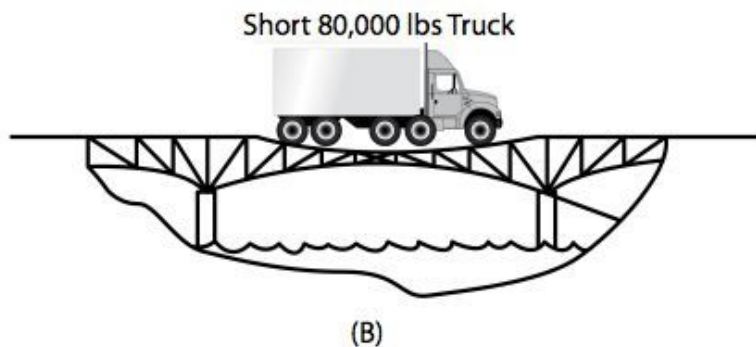


Figure 1

In Figure 1 (A), the stress on bridge members as the longer truck rolls across is much less than that caused by the short vehicle in Figure 1 (B), even though both trucks have the same total weight and individual axle weights. The weight of the longer vehicle is spread out, while the shorter vehicle has all of the weight

concentrated on a small area. The Federal-Aid Highway Amendments of 1974 increased the weights allowed on the Interstate system to 20,000 pounds on a single axle, 34,000 pounds on a tandem axle, and 80,000 pounds gross weight ([23 U.S.C. 127](#)). But Congress balanced this concession to productivity by enacting the Bridge Formula $\{500[(LN/N-1) + 12N + 36]\}$. The result is that motor vehicles may be loaded to the maximum weight only if each group of axles on the vehicle and their spacing also satisfy the requirements of the Formula. This prevents the vehicle from overstressing bridges in the same way that a person lying down on thin ice would minimize the risk of breaking through

WHAT THE FORMULA IS AND WHAT IT MEANS.

$$W = 500 \left[\frac{LN}{N-1} + 12N + 36 \right]$$

W = the maximum weight in pounds that can be carried on a group of two or more axles to the nearest 500 pounds.

L = the distance in feet between the outer axles of any two or more consecutive axles.

N = the number of axles being considered.

Note: When the distance in feet includes a fraction of a foot of one inch or more the next larger number of feet shall be used. This only applies to Divisible Loads.

The formula limits the weight on groups of axles in order to reduce the risk of damage to highway bridges. Allowable weight depends on the number of axles a vehicle has and the distance between those axles. However, the single-or-tandem-axle weight limits supersede the Bridge Formula limits for all axles not more than 96 inches apart.

Until 1982, Federal law set only upper limits (or ceilings) on Interstate System weight limits. A few states retained significantly lower weight limits, which eventually became barriers to long-distance truck traffic. In 1982, Federal law was amended to make Interstate Systems weights limits, including the bridge formula limits, both the maximum and the minimum weights (i.e., floors and ceilings) that states must allow on the Interstate System.

How the Formula Is Used

To use the formula, $\{W=500[(LN/N-1) + 12N + 36]\}$, you take the number of axles and plug them in for N and the distance measured rounded to the next foot for L. The calculation would then go in the following order:

- Everything within the parenthesis first
 - So you would take the length and multiply it by the number of axles being measured and divide that answer by the answer of the number of axles minus 1. So if you had a length of 36' and Number of axles equaled 5. It would look like this. $(36 \times 5 / 5-1)$ then you reduce it to $(180 \text{ divided by } 4)$ and this would equal 45.
 - Multiple 12 by the number of axles (5), we would get 60.
- So our formula now looks like this: $W = 500[45 + 60 + 36]$
- We would add all the additions on the inside of the bracket like this: $W = 500[141]$
- Now we multiply 141 by 500 and this gives us: $W = 70,500 \text{ lbs}$

When calculating the formula by hand you would round the measurement after adding all the other measurements together. For instance if I had the following measurements of 13', 4'4", 14', 4'6" they would give me a measurement of 35' 10"

So this means that on a bridge measurement of 36 ft with 5 axles the total allowed weight is 70,500 lbs.

Gross Weight

The weight of a vehicle or vehicle combination and any load thereon. The federal gross weight limit on the Interstate System is 80,000 pounds.

Single-Axle Weight

The total weight on one or more axles whose centers are not more than 40 inches apart. The federal single-axle weight limit on the Interstate System is 20,000 pounds.

Tandem-axle Weight

The total weight on two or more consecutive axles more than 40 inches but not more than 96 inches apart. The Federal tandem axle weight limit on the Interstate System is 34,000 pounds.

Interstate System weight limits in some States may be higher than these figures due to "grandfather" rights. When the Interstate System axle and gross weight limits were adopted in 1956, states were allowed to keep or "grandfather" those, which were higher. In 1975, states were allowed to keep or "grandfather" those, axle, gross and bridge formula limits which were higher than those that were established for the interstate system.

Bridge Formula calculations yield a series of weights [HYPERLINK BRIDGE](#). However, the single axle weight limit replaces the Bridge Formula weight limit on axles not more than 40 inches apart, and the tandem-axle weight limit replaces the Bridge Formula weight limit for axles over 40 but not more than 96 inches apart. At 97 inches apart, two axles can carry 39,000 pounds and three axles 42,500 pounds, as shown in Figure 2.

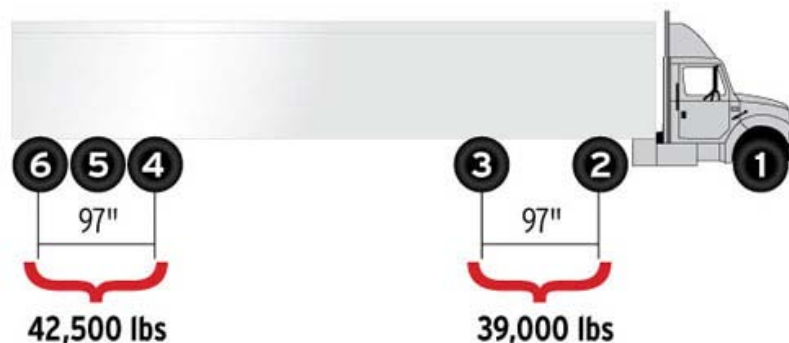


Figure 2

Federal law provided that any two or more consecutive axles may not exceed the weight computed by the Formula even though single axles, tandem axles, and gross weight are within legal limits. In other words, the axle group that includes the entire truck--sometimes called the "outer bridge" group--must comply with the Bridge Formula. But interior combinations of axles, such as the "tractor bridge" (axles 1, 2, and 3) and "trailer bridge" (axles 2, 3, 4, and 5), must also be in compliance with weights computed by the Formula (Figure 3).



Figure 3

The most common vehicle checked for compliance with weight limit requirements is shown in Figure 3. While the Bridge Formula applies to each combination of two or more axles, experience shows that axle combinations 1 through 3, 1 through 5, and 2 through 5 are critical and must be checked. If these combinations are found to be satisfactory, all of the others on this type of vehicle will normally be satisfactory. The vehicle with weights and axle dimensions as shown in Figure 4 will be used to illustrate a Bridge Formula check.

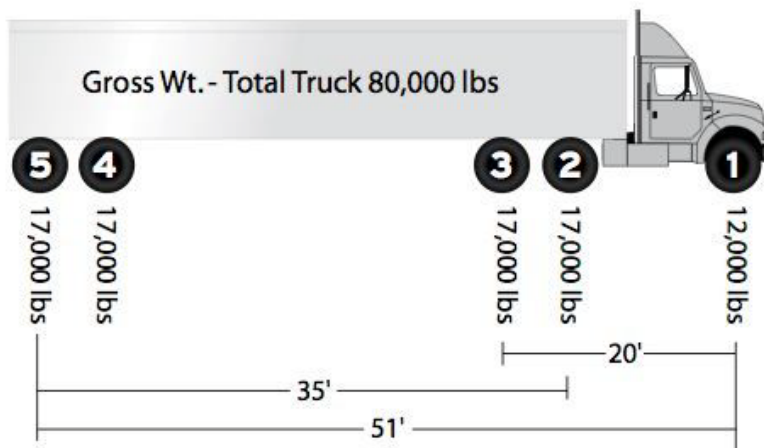


Figure 4

Before checking a vehicle for compliance with the Bridge Formula, its single-axle, tandem axle, and gross weight should be checked. Here the single axle (number 1) does not exceed 20,000 pounds, tandems 2-3 and 4-5 do not exceed 34,000 pounds each, and the gross weight does not exceed 80,000 pounds. These preliminary requirements are thus satisfied. The first Bridge Formula combination is checked as follows:

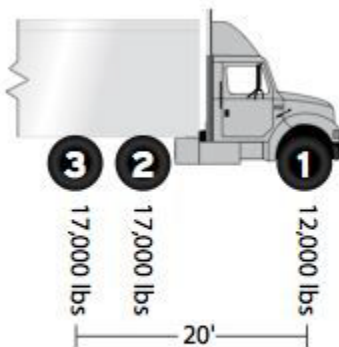


Figure 5

Check axle of 1 thru 3 (Figure 5)

Actual weight = 12,000 + 17,000 + 17,000 =
46,000 pounds

N = 3 axles

L = 20 feet

$$W = 500 \left[\frac{LN}{N-1} + 12N + 36 \right]$$
$$W = 500 \left[\frac{LN}{N-1} + (12 \times 3) + 36 \right]$$
$$= 51,000 \text{ lbs.}$$

W maximum = 51,000 lbs., which is more than the actual weight of 46,000 lbs., so the Bridge Formula requirement is satisfied.

Example--from The Bridge Table

This same number (51,000 pounds) could have been obtained from the Bridge Table by reading down the left side to L = 20 and across to the right where N = 3.

Now check axles 1 thru 5 (Figure 6) Actual weight = 12,000 + 17,000 + 17,000 + 17,000 + 17,000 = 80,000 lbs. W maximum, from the Bridge Table for "L" of 51 feet and "N" of 5 = 80,000 lbs. Therefore, this axle spacing is satisfactory.

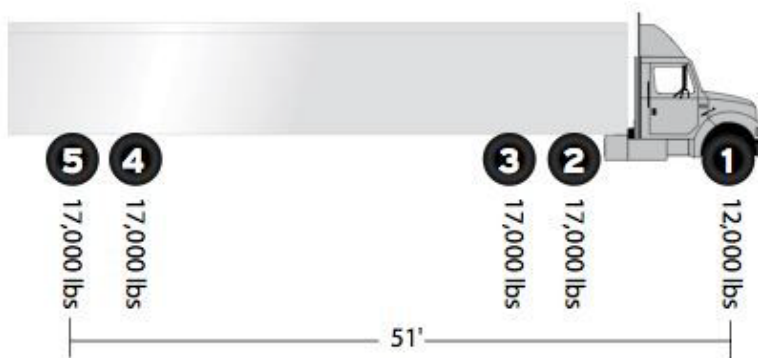


Figure 6

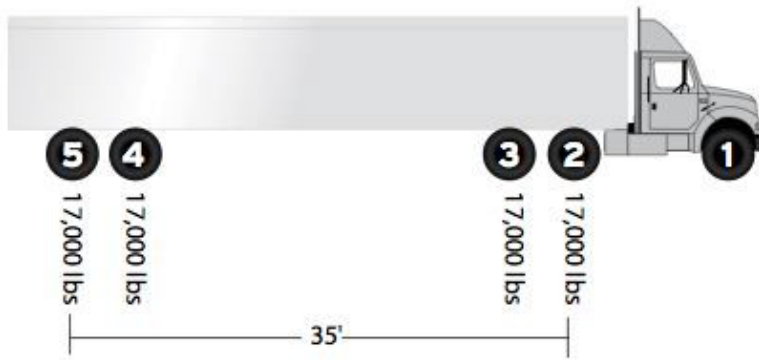


Figure 7

Now check axles 2 thru 5 (Figure 7) Actual weight = 17,000 + 17,000 + 17,000 + 17,000 = 68,000 lbs. W maximum, from the Bridge Table for "L" of 35 feet and "N" of 4 = 65,500 lbs. This is a violation because the actual weight exceeds the Bridge Formula by 2,500 Lbs. To correct the situation, some load must be removed from the vehicle or the axle spacing (35 feet) must be increased.

Federal law (**23 U.S.C. 127**) includes one exception to the Bridge Formula and the Bridge Table--two consecutive sets of tandem axles may carry 34,000 pounds each if the over-all distance between the first and last axles of these tandems is 36 feet or more. For example, a five-axle tractor-semitrailer combination may carry 34,000 pounds both on the tractor tandem (axles 2 and 3) and the trailer tandem (axles 4 and 5), provided axles 2 and 5 are spaced at least 36 feet apart. Without this exception, the Bridge Formula would allow an actual weight of only 66,000 to 67,500 pounds on tandems spaced 36 to 38 feet apart.

The procedure described above can be used to check any axle combinations, but several closely spaced axles usually produce the most critical situation.

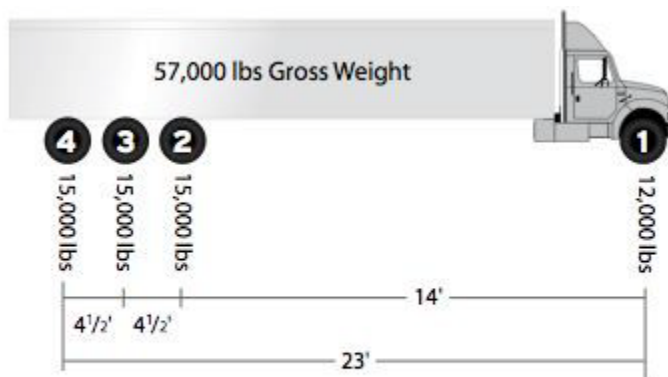


Figure 8

- This vehicle has all legal axle weights but is still in violation. Axles 2-4 (Group 2) a (tridem) has a 9-foot spread measured from center of axle to center of axle, and is weighing 45,000 lbs. in figure 8. If you use the bridge chart for a 3-axle group at a 9-foot spread the tridem can only weigh 42,500 lbs. The vehicle would be over bridge on axles 2-4 (Group 2) by 2,500 lbs.

Note: This is a violation. The load would have to be reduced, axles added, or spacing increased, to comply with the Bridge Formula

The following chart is a Federal Divisible chart (This title needs to be added to show difference)

BRIDGE TABLE B - EXTENDED | BASED ON WEIGHT FORMULA $W = 500(LN \text{ DIVIDED BY } N-1)+12N+36$
 MAXIMUM LOAD IN POUNDS CARRIED ON | GROUP OF 2 OR MORE CONSECUTIVE AXLES - Page 1 of 2
 DISTANCE IN FEET BETWEEN THE EXTREMES OF ANY GROUP OF 2 OR MORE

(N)	2 Axles	3 Axles	4 Axles	5 Axles	6 Axles	7 Axles	8 Axles	9 Axles	10 Axles	11 Axles	12 Axles	13 Axles
4	34,000											
5	34,000											
6	34,000											
7	34,000											
8	34,000	34,000										
9	39,000	42,500										
10	40,000	43,500										
11		44,000										
12		45,000	50,000									
13		45,500	50,500									
14		46,500	51,500									
15		47,000	52,000									
16		48,000	52,500	58,000								
17		48,500	53,500	58,500								
18		49,500	54,000	59,000								
19		50,000	54,500	60,000								
20		51,000	55,500	60,500	66,000							
21		51,500	56,000	61,000	66,500							
22		52,500	56,500	61,500	67,000							
23		53,000	57,500	62,500	68,000							
24		54,000	58,000	63,000	68,500	74,000	79,500					
25		54,500	58,500	63,500	69,000	74,500	80,500					
26		55,500	59,500	64,000	69,500	75,000	81,000					
27		56,000	60,000	65,000	70,000	75,500	81,500					
28		57,000	60,500	65,500	71,000	76,500	82,000					
29		57,500	61,500	66,000	71,500	77,000	82,500					
30		58,500	62,000	66,500	72,000	77,500	83,000					
31		59,000	62,500	67,500	72,500	78,000	83,500					
32		60,000	63,500	68,000	73,000	78,500	84,000	90,000				
33			64,000	68,500	74,000	79,000	85,000	90,500				
34			64,500	69,000	74,500	80,000	85,500	91,000				
35			65,500	70,000	75,000	80,500	86,000	91,500				
36			68,000	70,500	75,500	81,000	86,500	92,000	98,000			
37			68,000	71,000	76,000	81,500	87,000	93,000	98,500			
38			68,000	71,500	77,000	82,000	87,500	93,500	99,000			
39			68,000	72,500	77,500	82,500	88,500	94,000	99,500			
40			68,500	73,000	78,000	83,500	89,000	94,500	100,000	106,000		
41			69,500	73,500	78,500	84,000	89,500	95,000	101,000	106,500		
42			70,000	74,000	79,000	84,500	90,000	95,500	101,500	107,500		
43			70,500	75,000	80,000	85,000	90,500	96,000	102,000	107,500		
44			71,500	75,500	80,500	85,500	91,000	96,500	102,500	108,000	114,000	
45			72,000	76,000	81,000	86,000	91,500	97,500	103,000	108,500	114,500	
46			72,500	76,500	81,500	87,000	92,500	98,000	103,500	109,500	115,000	
47			73,500	77,500	82,000	87,500	93,000	98,500	104,000	110,000	115,500	
48			74,000	78,000	83,000	88,000	93,500	99,000	104,500	110,500	116,000	122,000
49			74,500	78,500	83,500	88,500	94,000	99,500	105,500	111,000	116,500	122,500
50			75,500	79,000	84,000	89,000	94,500	100,000	106,000	111,500	117,500	123,000

ALL COMBINATIONS OF VEHICLES WITH GROSS VEHICLE WIEGHTS IN EXCESS OF 80,000 POUNDS MUST BE IN COMPLIANCE WITH THE UTAH WEIGHT TABLE AND WILL REQUIRE AN OVERWEIGHT PERMIT

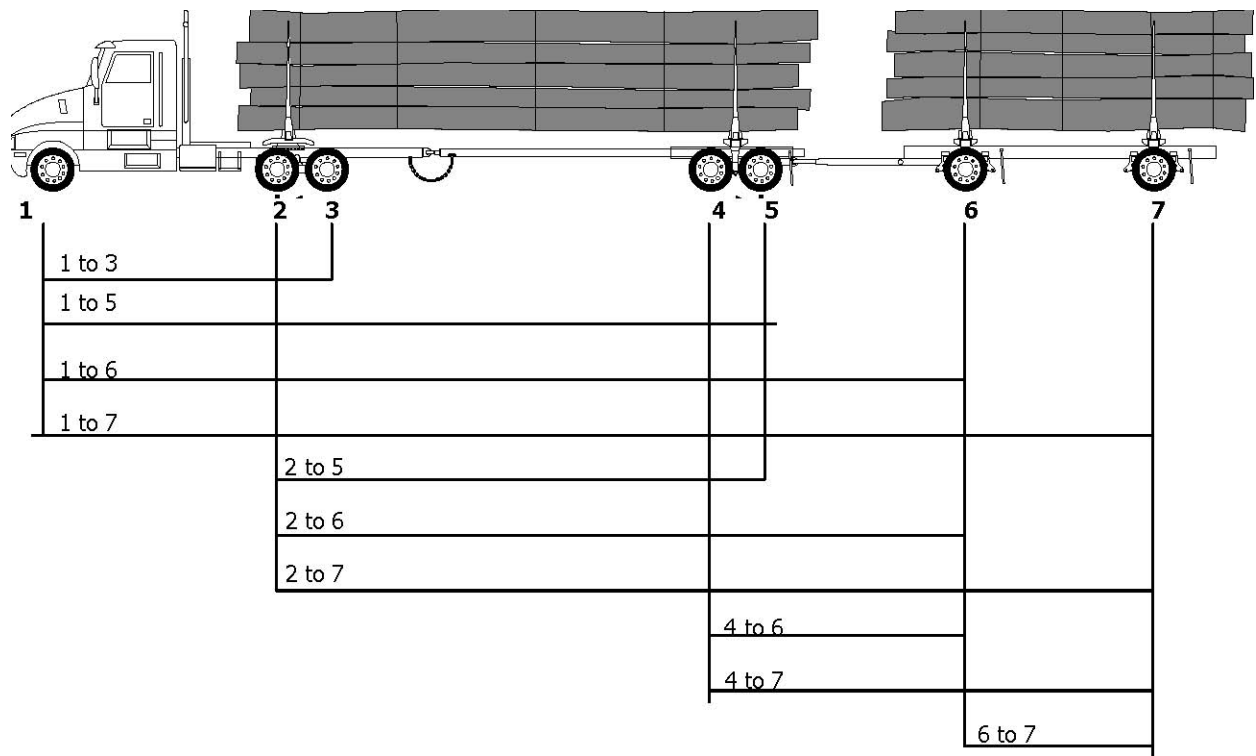
AXLE GROUPS IN THESE SPACINGS ARE IMPRACTICAL

BRIDGE TABLE B - EXTENDED | BASED ON WEIGHT FORMULA $W = 500(LN \text{ DIVIDED BY } N-1)+12N+36$

MAXIMUM LOAD IN POUNDS CARRIED ON | GROUP OF 2 OR MORE CONSECUTIVE AXLES – Page 2 of 2

(N)	2 Axles	3 Axles	4 Axles	5 Axles	6 Axles	7 Axles	8 Axles	9 Axles	10 Axles	11 Axles	12 Axles	13 Axles
51			76,000	80,000	84,500	89,500	95,000	100,500	106,500	112,000	118,000	123,500
52			76,500	80,500	85,000	90,500	95,500	101,000	107,000	112,500	118,500	124,000
53			77,500	81,000	86,000	91,000	96,500	102,000	107,500	113,000	119,000	124,500
54			78,000	81,500	86,500	91,500	97,000	102,500	108,000	113,500	119,500	125,000
55			78,500	82,500	87,000	92,000	97,500	103,000	108,500	114,000	120,000	126,000
56			79,500	83,000	87,500	92,500	98,000	103,500	109,000	115,000	120,500	126,500
57			80,000	83,500	88,000	93,000	98,500	104,000	109,500	115,500	121,000	127,000
58				84,000	89,000	94,000	99,000	104,500	110,000	116,000	121,500	127,500
59				85,000	89,500	94,500	99,500	105,000	111,000	116,500	122,000	128,000
60				85,500	90,000	95,000	100,500	105,500	111,500	117,000	122,500	128,500
61				86,000	90,500	95,500	101,000	106,500	112,000	117,500	123,500	129,000
62				86,500	91,000	96,000	101,500	107,000	112,500	118,000	124,000	
63				87,500	92,000	96,500	102,000	107,500	113,000	118,500	124,500	
64				88,000	92,500	97,500	102,500	108,000	113,500	119,000	125,000	
65				88,500	93,000	98,000	103,000	108,500	114,000	119,500	125,500	
66				89,000	93,500	98,500	103,500	109,000	114,500	120,500	126,000	
67				90,000	94,000	99,000	104,500	109,500	115,000	121,000	126,500	
68				90,500	95,000	99,500	105,000	110,000	116,000	121,500	127,000	
69				91,000	95,500	100,000	105,500	111,000	116,500	122,000	127,500	
70				91,500	96,000	101,000	106,000	111,500	117,000	122,500	128,000	
71				92,500	96,500	101,500	106,500	112,000	117,500	123,000	128,500	
72				93,000	97,000	102,000	107,000	112,500	118,000	123,500	129,000	
73				93,500	98,000	102,500	107,500	113,000	118,500	124,000		
74				94,000	98,500	103,000	108,500	113,500	119,000	124,500		
75				95,000	99,000	103,500	109,000	114,000	119,500	125,000		
76				95,500	99,500	104,500	109,500	114,500	120,000	126,000		
77				96,000	100,000	105,000	110,000	115,500	121,000	126,500		
78				96,500	101,000	105,500	110,500	116,000	121,500	127,000		
79				97,500	101,500	106,000	111,000	116,500	122,000	127,500		
80				98,000	102,000	106,500	111,500	117,000	122,500	128,000		
81				98,500	102,500	107,000	112,500	117,500	123,000	128,500		
82				99,000	103,000	108,000	113,000	118,000	123,500	129,000		
83				100,000	104,000	108,500	113,500	118,500	124,000			
84					104,500	109,000	114,000	119,000	124,500			
85					105,000	109,500	114,500	120,000	125,000			
86					105,500	110,000	115,000	120,500	126,000			
87					106,000	110,500	115,500	121,000	126,500			
88					107,000	111,500	116,500	121,500	127,000			
89					107,500	112,000	117,000	122,000	127,500			
90					108,000	112,500	117,500	122,500	128,000			
91					108,500	113,000	118,000	123,000	128,500			
92					109,000	113,500	118,500	123,500	129,000			
93					110,000	114,000	119,000	124,500				
94					110,500	115,000	119,500	125,500				
95					111,000	115,500	120,500	126,000				
96					111,500	116,000	121,000	127,000				
97					112,000	116,500	121,500	127,500				
98					113,000	117,000	122,000	128,000				
99					113,500	117,500	122,500	128,500				
100					114,000	118,500	123,000	129,000				
101	40,000	60,000	80,000	100,000	114,500	119,000	123,500	129,000	129,000	129,000	129,000	129,000

Caution: This information paraphrases the actual provision in [23 U.S.C. 127](#) and [23 CFR 658](#) for the sake of clarity. In case of a dispute, the statute and regulations will govern.



steering axle needs to be stressed.

The inner bridges are where the majority of all weight is going to be carried.

A tractor may have a long wheelbase so the “outer” or overall bridge may be met, however, if the axle spacing on the trailer and/or trailers (inner bridge) is measured it may not allow the maximum outer bridge.

Example:

Outer Bridge: 1 to 7 = 91'6" (92) = 113,500 Lbs

Inner Bridge: 2 to 5 = 36'6" (37)

2 to 6 = 46'6" (47)

2 to 7 = 61'6" (62) = 91,000 Lbs

4 to 6 = 14'

4 to 7 = 29'

6 to 7 = 15'

Based on the outer bridge the combination can carry 113,500 Lbs however, the inner bridge is allowed 96,000 meaning that the steer axle would have to carry 17,500 Lbs to get maximum bridge.



Allowed steering axle weight is determined by the amount shown on the tire(s) sidewalls (single rating on steer and VLS) are rated or a maximum weight of 20,000 lbs whichever is less. Sometimes steer axles are rated for more than legal but the limit is 20,000. If one tire has a greater weight rating than the other, the lesser of the two rating (weakest link) will be used.

The example above shows 6,175 lbs. if both tires are the same, the allowed weight is 12,350 lbs. If one side is rated for 6,175 lbs and the other side is rated for 6,610 lbs the total rating would be 12,350 not 12,785.

Allowed weight for a variable load suspension or “tags” axles is the weight rating of the tires or pounds per inch, whichever is less.

Permitted loads = 450 lbs per inch for tires less than 11 inches wide.

= 500 lbs per inch for tires 11 inches wide or greater.

Non-permitted = 600 lbs per inch.

Note:

Using the example above the 11 is the width of the so that would be multiplied by 500 lbs. equaling 5,500 lbs. or 11,000 lbs. if both tires or the same size.

When the tire sidewall shows the size (metric) of 255. You would multiply that by .04 to determine the standard size, which would be 10.2”.

BRIDGE ROAD RESTRICTIONS

Bridges with rated capacities less than the maximum legal limit in Utah have gross weight restrictions posted. A highway authority may prohibit the operation of any vehicle or impose restrictions on the weight of a vehicle upon any highway under its jurisdiction. [UCA 72-7-408](#)

NON-DIVISIBLE LOADS

Non-Divisible loads are given an extra allowance on weights over the Federal Divisible bridge calculations.

(Refer to 72-7-404 (4))

The bridge formula used to calculate weights allowed for non-divisible loads is:

$$W=1.47 \{500[LN/N-1+12N+36]\}$$

The groups that are 9' wide are given an extra 7.5 % over what an 8' wide axle is allowed.

The groups that are 10' wide are given an extra 15 % over what an 8' wide axle is allowed.

Trunion axles are given an extra 10% allowance over what an 8' wide axle is allowed.

The same rules apply for a trunion that is 9' wide or 10' wide axle.

Utah Non-Divisible Load Chart

*L	2 Axles		3 Axles		4 Axles		5 Axles		6 Axles	
	8 Wide	10 Wide	8 Wide	10 Wide	8 Wide	10 Wide	8 Wide	10 Wide	8 Wide	10 Wide
4	50,000	57,500								
5	50,000	57,500								
6	50,000	57,500								
7	50,000	57,500								
8	50,000	57,500	61,500	71,000						
9	57,500	66,000	62,500	72,000						
10	59,000	68,000	64,000	73,500						
11			64,500	74,000						
12			66,000	76,000	73,500	84,500				
13			67,000	77,000	74,000	85,000				
14			68,500	79,000	75,500	87,000				
15			69,000	79,500	76,500	88,000				
16			70,500	81,000	77,000	88,500	85,000	98,000		
17			71,000	81,500	78,500	90,000	86,000	99,000		
18			73,000	84,000	79,500	91,500	86,500	99,500		
19			73,500	84,500	80,000	92,000	88,000	101,000		
20			75,500	86,000	81,500	94,000	89,000	102,500	97,000	111,500
21			76,000	87,500	82,500	95,000	89,500	103,000	98,000	113,000
22			77,000	88,000	83,000	95,500	90,500	104,000	98,500	113,500
23			78,000	90,000	84,500	97,000	92,000	106,000	100,000	115,000
24			79,500	91,500	85,000	98,000	92,500	106,500	101,000	116,000
25			80,000	92,000	86,000	99,000	93,000	107,000	101,500	117,000
26			81,500	94,000	87,500	100,500	94,000	108,000	102,000	117,500
27			82,500	95,000	88,000	101,000	95,000	109,000	103,000	118,500
28			84,000	96,500	89,000	101,500	96,500	110,000	104,500	120,000
29			84,500	97,000	90,500	104,000	97,000	111,500	105,500	121,000
30			86,000	99,000	91,000	104,500	98,000	112,500	106,000	122,000
31			86,500	99,500	92,000	106,000	99,000	114,000	106,500	122,500
32			88,000	101,000	93,500	107,500	100,000	115,000	107,500	123,500
33					94,000	108,000	100,500	115,500	109,000	125,500
34					95,000	109,000	101,500	117,000	109,500	126,000
35					96,000	110,000	103,000	118,500	110,500	127,000
36					97,000	111,500	103,500	119,000	111,000	127,500
37					98,000	112,500	104,500	120,000	111,500	128,000
38					99,000	114,000	106,000	122,000	113,000	130,000
39					100,000	115,000	106,500	122,500	114,000	131,000
40					101,000	116,000	107,500	123,500	114,500	131,500
41					102,000	117,000	108,000	124,000	115,500	132,500
42					103,000	118,500	109,000	125,500	116,000	133,500
43					103,500	119,000	110,500	127,000	117,500	135,000
44					105,000	120,500	111,000	127,500	118,500	136,500
45					106,000	122,000	111,500	128,000	119,000	137,000
46					106,500	122,500	112,500	129,500	120,000	138,000
47					108,000	124,000	114,000	131,000	120,500	138,500
48					109,000	125,500	114,500	131,500	122,000	140,500
49					109,500	126,000	115,500	133,000	123,000	141,500
50					111,000	127,500	116,000	133,500	123,500	142,000
51					111,500	128,000	117,500	135,000	124,000	142,500
52					112,500	129,500	118,500	136,500	125,000	144,000
53					114,000	131,000	119,000	137,000	126,500	145,500
54					114,500	131,500	120,000	138,000	127,000	146,000
55					115,500	133,000	121,500	140,000	128,000	147,000
56					117,000	134,500	122,000	140,500	128,500	148,000
57					117,500	135,000	123,000	141,500	129,500	149,000
58							123,500	142,000	131,000	150,500
59							125,000	144,000	131,500	151,000
60							126,000	145,000	132,500	152,000

Chapter 22

General Permit Provisions

OBTAINING OVERSIZE PERMITS

Before moving a vehicle or load, which exceeds legal dimensions as outlined in the Legal Vehicle Dimensions a permit must be obtained and carried in the vehicle for inspection by law enforcement personnel.

The appropriate permit for oversize vehicles and loads must be obtained prior to operating within the State of Utah, except when entering on I-15 (St. George Port of Entry) and West bound I-80 (Echo Port of Entry) and East bound I 80 at (Wendover Port of Entry).

- The permit may be in paper or electronic format.

Permits may be purchased online at www.udot.utah.gov/public/mcs prior to entry. Permits may be purchased with a major credit card are issued on a single trip, semi annual or annual basis.

Because of additional processing requirements, the Western Regional and/or loads exceeding 15' 6" H, 17' W, or over 300,000 lbs. will only be processed during Monday through Friday from 6 a.m. to 5 p.m. Mountain Standard Time.

Permit cost can be found in Chapter 25,26,27 and 28.

The following conditions that must be met to obtain an oversize or overweight permit are:

- The motor carrier complies with the financial responsibility obligations in CFR Part 387 (Interstate) and R909-1 (Intrastate)
- The vehicle or vehicles must be properly registered in UCA 41-1A-101
- The driver or drivers are properly licensed with appropriate endorsements in 49 CFR Part 382-399
- The motor carrier complies with the Federal Motor Carrier Safety Regulations in 49 CFR Part 382-399
- The motor carrier complies with the Hazardous Material Regulations 49 CFR Parts 171-180 and
- The motor carrier complies with the Unified Carrier Registration or UCR as required. ADAM find Federal Code Section 49 of Utah codes sec. 14-504a

The following information must be provided when applying for a permit:

- Motor Carrier information:
 - USDOT number
 - Address
 - Telephone number
 - Fax number (if available)
 - Web address (if available)
 - Email address (if available)
 - Vehicle information
 - License number, state, expiration date
 - Vehicle make, year, registered weight
 - Unit number
 - Complete Vehicle Identification Number (VIN)

- Specific load information
- Permit duration (Single, Semi-Annual, Annual)
- Requested routes
- Dimensions, length, width, height, front and rear overhang.

LIABILITY OF PERMITTEE

The applicant or permittee, as a condition for obtaining an oversize/overweight permit, shall assume all responsibility for crashes, including injury to any persons or damage to public or private property caused by their operations.

INDEMNITY CLAUSE

The applicant or permittee, agrees to indemnify and hold harmless the department from any and all claims resulting directly or indirectly from the operation and transportation of vehicles or combination of vehicles operating under an oversize or overweight permit.

PERMIT REVOCATION, SUSPENSION AND CONFISCATION

Vehicles operating under an oversize/overweight permit shall not be operated in excess of the posted speed limit or the speed indicated on the permit.

Violation of the terms or conditions of an oversize/overweight permit, including but not limited to:

Violations of any permit that may result in the revocation, suspension or confiscation of the permit include, but are not limited to:

- speeding in excess of the posted speed limit or the speed indicated on the permit;
- lane travel;
- weather;
- load securement;
- violations of the Federal Motor Carrier Safety Regulations; and
- violations of the Hazardous Material Regulations
- Fraudulent changes

Before a vehicle can be moved, it must be made legal, properly permitted and all of the out-of-service violations corrected. Patterns of non-compliance at a carrier level may result in the following actions:

- civil penalties;
- suspension or revocation of permit privileges; or
- an order to cease and desist operations.

Before the vehicle can be moved, it must be made legal, properly permitted, and violations corrected.

Patterns of non-compliance at a company level may constitute action taken against the carrier's overall permit privileges and may result in additional civil penalties and an order to cease operations.

A Motor carrier may not operate any commercial vehicle for the transportation of persons or property on any public highway in this state except in accordance with this chapter, and rules and orders of the Department. UCA 72-9-104.

When required, applications for oversize/or overweight permits will be evaluated with the assistance of the super load team or other agencies such as structures, region right of way and other state agencies.

Vendors and manufactures of trailers, special mobile equipment, or peculiar vehicles and/or combination of vehicles, are urged to coordinate preferably in writing with the Division before manufacturing, modifying or selling such equipment for use in this state. Care should be exercised that such equipment complies with the requirements of this state.

A permit is required when exceeding legal dimensions, and/ or weight in UCA 72-7-402 and UCA 72-7-404.

HYPERLINK TO WHERE PERMITS CAN BE OBTAINED.

Oversize/Overweight permits **BETTER FORMAT (Similar to Chapter 40 North American Standards)**

Step 1 Go to www.UtahMC.com **This website will be replaced Utahmc.com is going away**

Step 2 Click on buy permits (green button)

Step 3 User ID (If new user refer to step five)

Step 4 Password

Step 5 Enter system

If new to ordering permits click on New User Signup

Register for User Access

Online Access to the Motor Carrier System is limited to representatives or employees of a single commercial vehicle operator, government entity, private recreational vehicle owner, or a permitting agency.

Choose one of the following customer types;

Carrier, Permit Service, Government, or RV (Recreational vehicle)

A Carrier will need to enter their USDOT number and click on the next button

The Utah Motor Carrier System will ask the new user to verify that the USDOT number they entered is for the particular company and that they are employed by them and are authorized to purchase permits. Click Yes or No.

If No, the system will reroute you back to enter the appropriate USDOT number.

If yes, the system will ask you to fill in the appropriate information regarding the Users name, title, address, phone number, email. At this point the user can register a User ID and Password of their choosing – they both require being at least 6 characters long and cannot be the same.

If the carriers USDOT number has never been entered into the system they will be asked to enter their company name, their name and email address. A request will be sent to the Motor Carrier Division customer relations team to add them to the system.

If Permit Service Hit Next then

Enter company name

Enter Phone:

Enter Agency Location

Street:

City: and State:

Zip:

If agency location is same as Mailing Address Click Copy Location button

Then hit next

Create your personal profile

First name:

Last name:

Title:

Or enter new mailing address

Street:

City:

State:

Zip:

Enter phone and other information

Phone:

Extension:

Email address

Then choose user information. At this point the user can enter a User ID and Pass word they want to use. Both require being at least 6 characters long and cannot be the same.

User ID:

New password:

Verify password

Then hit submit button

Customer Type: Government Entity

Then hit the next button

Enter partial name. Example: John- by entering John, a list will appear in the matching

Existing field including any name with John in it. Such as John Smith, Don Johnson, etc.

Select from the list by clicking on the name and click the use selected button.

If no Match is found then use the Add new button to create a new customer.

RV

If you already have an existing user account, please try to use the “I forgot my password” link on the opening page to retrieve your username and password.

Otherwise click next button.

Create your personal profile

RV Owner

First Name:

Last name:

Title:

Mailing address

Street:

City:

State:

Zip:

Phone and other information

Phone:

Extension:

Email Address:

Now the user can enter a User ID and Pass word they want to use. Both require being at least 6 characters long and cannot be the same no caps required

Create User ID:

New Password:

Verify Password:

Then hit Submit button

My Company: RV Owner

First Name

Last Name

Title

Mailing address

Street

City

State

Zip

Phone and other information

Phone:

Extension:

Email Address:

User information

User ID:

Password must have 6 or more characters and cannot be the same no caps required

New Password:

Verify Password:

Click Submit

WESTERN REGIONAL

NOTE: Western Regional permits are issued by Washington, Idaho, Arizona, New Mexico, Texas and Utah. Western Regional permits cannot be issued by Utah, unless part of the route is in Utah, as well as other states.

Oversize/Overweight permits

Step 1 Go to UtahMC.com **This website will be replaced Utahmc.com is going away**

Step 2 Click on buy permits

Step 3 User ID

Step 4 Password

Step 5 Enter system

Step 6 Box on left hand side of screen select order permits

Step 7 Permit type (Check all that apply)

Overweight and/or Oversize

Step 8 Load type chose one that applies

Vehicle Configuration Single Unit (Power Unit No Trailer)

Combination

Step 9 Start date

Vehicle

If you have purchased a permit previously for the vehicle, click the Vehicle list button to select the vehicle.

Step 10 Unit #

VIN/Serial # (The letters I,O, and Q never appear in a VIN

YR:

Make:

License:

State:

Registered Gross Weight:

Registration Expire Date

Step 11 Next page

Step 12 Duration

Single Trip

Annual

Semi-Annual

Maximum Gross Weight: (if over 80,000 pounds to 125000) If over 125000+

Continue with step 14,15,16,

Step 14 Description of load

Step 15 then fill in boxes

Width

Height

Overall length

Front overhang

Rear overhang

WRP OVERSIZE/OVERWEIGHT RESTRICTIONS AS OF 2/21/2013

ARIZONA

- http://www.azsos.gov/public_services/Title_17/17-06.pdf (Administrative Rules)
- <http://www.azdot.gov/Highways/bridge/BridgeInventory/PDF/AzBridgeInv-All.pdf> (Bridge Inventory)
- www.azdot.gov/mvd/commercialenforcement/document/WeightRestrictions.pdf (AZ Wt Restricted Bridges)
- www.az511.gov (Daily Construction Restrictions)

As of March 8, 2013 Utah will not process Western Regional permits unless Utah is the origin, destination or pass through.

IH10	11' width	Left lanes closed E & W-bound from MP204.50, 0.60 miles East of Toltec Road to MP202.50, 1.30 miles West of Toltec Road from February 18 - March 1 from 8:00 P.M. to 6:00 A.M. Monday, through Thursday.
IH10	12' Width	Lane closed East-bound from MP390.0, 1.10 miles West of New Mexico State Line to MP391.40, 0.20 miles East of New Mexico State Line From Thu, 7 Feb 2013 5:00 AM to Fri, 8 Mar 2013 5:00 PM. Road reduced to one 12 foot lane. Speed limit reduced through work zone to 35 mph. Please drive with caution.
IH10	On- and off-Ramps Closed	East-bound from mile post 124.70, at Cotton Lane until Wed, 15 Oct 2014 4:00 PM. The Cotton Lane on and off ramps will be closed during construction of the new I-10/Loop 303 Interchange. Detour signs will be installed to assist travelers.
IH10	On- and off-Ramps Closed	West-bound from mile post 124.70, at Cotton Lane until Wed, 15 Oct 2014 4:00 PM. The Cotton Lane on and off ramps will be closed during construction of the new I-10/Loop 303 Interchange. Detour signs will be installed to assist travelers.
IH17	Narrow Lanes	NS-bound from mile post 263.0, 0.40 miles North of SR 69 to mile post 264.0, 1.40 miles north of SR69 from Mon, 11 Feb 2013 7:00 AM to Fri, 22 Mar 2013 3:00 PM. Additional Restrictions for Commercial Carriers Only: DPS Escort Required, please contact DPS for details and to arrange escorts. Two-way traffic with two 11 foot lanes in each direction. Loads restricted to 22 feet wide (2 - 11ft lanes). DPS required for Class C Loads Daytime Buss Ph: 1-855-712-8530
IH40	12' Width	E & W-Bound from MP300.8, 5.60 miles East of Sun Valley to MP303.55,

		6.50 miles West of Petrified Forest from Fri, 9 Mar 2012 1:07 PM to Mon, 15 Apr 2013 5:00 AM
IH40	12' Width	Left lane closed E & W-bound from MP52.0, 3.10 miles East of US93 North to MP54.0, 3.80 miles West of Kingman from Wed, 2 Jan 2013 4:00 AM to Mon, 30 Sep 2013 3:59 PM. Speed reduced to 45 mph. Officers and flaggers may be on site. Expect slow traffic. Detour for loads wider than 12 feet: IH40 East bound, will have to exit IH40 at MP53 and then enter back onto IH40.

COLORADO

IH25	13' Height	Permanent Restriction: MP200.11 (Jct IH225 & IH25) TO 213.8 (Jct IH70 & IH 25) (NB & SB)
IH70	Loads Over 13' Width & 85' Length	MP114.0 To 133.0 - Glenwood Springs To Dotsero (Glenwood Canyon): Length Restriction: Loads that exceed 85 Feet Length must have a Certified Pilot car in the rear. Width Restriction: Loads that exceed 13 Feet wide must have a Certified Pilot car in the rear.
IH70	Tunnel Restriction	MP213.6 - Eisenhower Tunnel Width Restriction: Permitted Loads over 11 feet must pull over to get permission to pass through the tunnel. Height Restriction: Loads 13'11" tall or less may now travel through the Eisenhower Tunnel.
IH70	Complete Closure	West Idaho Springs Exit Ramp (MP239 Eastbound Only) Complete closure of this ramp until further notice.
IH70	11' Width	MP240 To 245 - Idaho Springs east for 5 miles (Twin Tunnels Eastbound And Westbound) 30-minute full closures followed by a one-hour break may occur at anytime.
IH70	Complete Closure	Complete Closure Eastbound of exit ramp to Hidden Valley (Central City Parkway - MP243) through at least March 11, 2013.
IH70	Axle Weights	MP274.76 - Intersection of IH70 and US6, 265 (Brighton Blvd) Rr And City Streets. Weight Restriction: No Permits that

	Restriction	exceed the orange axle weights on the Bridge Weight Limit Map (See WWW.Coloradodot.Info/Business/Permits/Truckpermits) for Color Grid.
IH70	No Loads Permitted	MP274.76 To 276.58 - Brighton Blvd. to Colorado Blvd. (Both Directions) No Permitted Loads allowed on this portion of IH70.
IH70	Complete Closure	of Frontage Road - Appx. 5 Miles West of Limon (US40) Through April 30, 2013
IH225	11' Width	MP9 - Colfax Ave Exit (North And Southbound 225): Width Restriction: No Permits over 11 Feet Beginning Sunday November 20, 2011 until further notice.
US287/50	90' Length Turning	Intersection Of US287 and US50 East in City Of Lamar Length Restriction: No Permits Over 90 Feet traveling north on US287 to Eastbound US50. Loads in excess of 90-feet in length should travel westbound on US50 To US287 northbound to SR96 eastbound to US385.
CO14	Travel Curfew	In the City Of Sterling: Width, Height & Length Restriction: No Oversize Permits may travel in the City Of Sterling from 8 AM to 9 AM, 12 PM to 1 PM, and 5 PM To 6 PM every day of the week.
CO14	12' Width	MP138.9 To 153.3 - Between IH25 And US85 No Permits Over 12 Feet Beginning February 18, 2013 to at least September 1, 2013:

IDAHO

THERE ARE CURRENTLY NO RESTRICTIONS IN THIS STATE

LOUISIANA

ALL HIGHWAYS	Loads Over 13 Ft 6 In	Anytime a permit load is over 13'6 High or over 30,000 Gross Vehicle Weight and getting off of the interstate system, The Louisiana Permit Office should be called at 225-377-7110, 377-7111, 377-7112 Or 377-7114 to verify the route.
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IH10	Legal Weight	Legal weight only crossing the Calcasieu River Bridge...Use IH210 to get around it. (Calcasieu Parish)
IH10	Ramp Off-Ramp On	All eastbound permit loads must ramp-off at LA26 and ramp-on after LA 26 (Jeff Davis Parish)
IH10	Legal Weight To Ramp	Legal Weight IH10 Westbound ramp in Baton Rouge. No Exceptions (East Baton Rouge Parish)
IH12	11' Width	Eastbound and Westbound Between LA447 and LA1026 (Juban Road) Per Chris Wascom @ 225-686-2689. (Livingston Parish)
IH20	Shreveport	All permit load should use IH20-IH220-IH20 when traveling thru Shreveport, LA
US61	11' 6"	From LA427 To 1/2 Mile South Of LA42 @Oak Grove in Ascension Parish. Call Aaron Elisar W/ Dist 61 225-675-5320
US61	11" Width	Between LA22 and LA74 Updated 5-7-12 Per Tim Thomas or Aaron Elisar w/ Dist 61 @225-675-5320 (Ascension Parish)*
US84	No Weight	No Weight Into Or Out Of Texas. (Desoto Parish)
US90	No Permits On	Huey P Long Bridge closed to Oversize/Overweight loads (Jefferson Parish)
US90	No Permits	No permit loads in or out of Mississippi State (Tammany Parish)
US90	No Permits	No permit loads crossing Bayou Ramos in Morgan City. Detour EB 90-182@Morgan City-Lake Palourde Bypass-90 And Reverse WB 90-Lake Palourde Bypass-182-90@Morgan City Revised 4-12-12
US90	30,000# Max	Posted at 10/15-tons at Bayou Grand Marais Bridge that is between LA395 and LA26 per Stacey White w/ Dist 07 (Jeff Davis Parish)
US167	12' Width	Southbound between Georgia Avenue and West Railroad Avenue in Ruston, LA. (Lincoln Parish)
US167	12' Width	Southbound Between LA148 and LA155 Start 9-25-12 Ufn..Per Jeff Shapiro W/ Dist 05 (Jackson Parish)
US190	Closed	In Slidell near US90 (St. Tammany Parish) across a posted drain
US190	Weight limit	Posted for 10/15 tons between LA111 and US171 starting 2-

		7-13 UFN (Beauregard parish)
LA1	Bridge Closed	Closed for bridge replacement at Bayou Jean De Jean just north of LA121 near Boyce. start 10-15-12 per Johnny Brister w/ Dist 08 Approx 6 Months (Rapides Parish)
LA12	10' Width	Hold 10' Wide between Texas line and Starks @LA109 for bridge repair. Start 7-18-12 Ufn (Calcasieu Parish) can get permission from Vicki W/ Dist 07 @ 337-437-9207 If Beginning Or Ending There.
LA12	Weight	Texas/Louisiana Line: 80,000 Lbs Max for truck and trailer, 50,000# or less for single vehicle.
LA22	11' Width	11' wide between LA444 and LA42

MONTANA

Please check our website for more up to date closures and information

<http://www.mdt.mt.gov/travinfo/scripts/alerts.pl>

US87	12' Width	Between Belt And Raynesford (EB and WB): There is a width restriction of 12-feet wide. The detours for EB and WB are: 1) WB: Raynesford, Mt (WB detoured) to Great Falls, Mt: US87, SR427, US89, US87. 2) EB: Great Falls, Mt (EB detoured) to Raynesford, Mt: US87, US89, SR427, US87 (this is a permanent restriction)
US89	MP55.5 To MP56.2 Road Closure	US89 beginning at RP55.5 extending 0.6 miles north of RP56.2. This project is partially within the City of Livingston Urban Limits: bridge, detour, and road closure. Stay alert for possible flagging operations. Work continues alongside the existing structure. Beginning January 7th the Yellowstone River Bridge will be closed until September Of 2013. Traffic control will detour vehicles around, Using IH90 between MP333 and 338. If a high wind event should occur on IH90 use secondary detour route between Three Forks Interchange (MP274) and the Mission Interchange, (MP342). Detour will be US12 Via US89 and US287. Follow Detour Signage.

MT200	No Overweight Loads	From Jordan to the junction of SR462: No Overweight loads allowed due to a timber bridge weakening (Big Dry Creek Bridge).
MT35	9' Width	All loads over 9-feet wide traveling between Polson and Bigfork with the exception of loads that are originating or are being delivered between Polson and Bigforks.
S261	No Overweight Load	Effective immediately: S-261 is closed from Wibaux to Sidney for overweight loads

NEVADA

All Wr Routes	Over 12' Wide	No holiday hours and no hours of darkness. For definitions, see http://www.nevadadot.com/business/trucker/overdimensional/ or call 800-552-2127/775-888-7410.
US6	Over 12' Wide	Front pilot car required from California State line to Utah State line.
IH15	Over 12' Wide	No weekend hours from SR160/Blue Diamond Rd/MP33, Las Vegas to California State line.
US50	Over 12' Wide	Front pilot car required from Fallon in Churchill County to Utah State line.
IH80	Over 12' Wide	No weekend hours from Greg St/Vista Way/MP21, Sparks in Washoe County to California State line.
US93	Over 12' Wide	Front pilot car required from Idaho State line to IH15.
US93	Over 20,000 Lbs S-Axle, 34,000 Lbs D- Axle, Or 42,000 Lbs Tridem	Slow to 5 MPH on bridge B219, Caliente in Lincoln County, over Meadow Valley Rd.
US93	Over 20,000 Lbs S-Axle: 34, 000 Lbs D-Axle Or 42,000 Lbs Tridem	No overweight from February 1 thru April 30, from Lincoln County, MP148 to White Pine County line due to spring thaw conditions.
US95	Over 12' Wide	Front pilot car required from Oregon State line to IH80 & from IH80 to Indian Springs in Clark County.

NEW MEXICO

IH10	12' Width	East & West bound lanes on IH10 located at MP123 to MP130 between Deming and Las Cruces
IH10	11' Width	East & West bound lanes from MP137.7 to MP146.1 in Las Cruces
IH25	11' Width	North & South bound lanes in Las Cruces from MP0 to MP1.
US 62/285/180	No Permits	Overweight loads and No self-propelled units are not to cross bridge at MP34.9 located in Carlsbad, NM, at until further notice. Please contact Joseph Valdez for routing.
NM200	No Permits	No oversize or overweight load allowed on NM200 Carlsbad Truck Route MP3.1 to MP8.5. (Contact Joseph Valdez for reroutes, if needed. – Office (505) 827 5534)

OKLAHOMA

TURNPIKE	CASH MAX. WIDTH	PIKEPASS MAX. WIDTH
TURNER	9' 6"	11' 6"
WILL ROGERS	9' 6"	11' 6"
H.E. BAILEY	9' 6"	11' 6"

H.E BAILEY T/P	No Overweight	MP5: Loads Must Use Ramps On Bridge At HE Bailey & US70. Randlett
H.E BAILEY T/P	10' Width	Between US277 & OK36 - Both Directions
WILL ROGERS T/P	No Loads Over 80,000 Lbs	No Loads Over 80,000 Lbs. On Bridge Going Over SH88 Both Directions, Rogers, Claremore County

IH35	11' Width	Warning: Between Main St & OK9, Both Directions, City of Norman
IH35	12' Width	Between US77 and US64, Both Directions, Town of Perry
IH35	Into Kansas	80,000 Pounds, No rear overhang in excess of 4 Ft. must have KS permit first. Axle Weights: - S-20000 D-34000 T-42000
IH35 & IH240	90' Length	No Loads Over 90' Long making the interchange in any direction
IH40	No Permit Loads	Between MP231 and 237, both directions il Okmulgee
IH40	IH40	Crosstown (Between IH235 and IH44) Both Directions - No overwidth, No Overheight, No Overlength Will Allow OW if meets OI-1 (Contact Ok)
IH44	No Permit Loads	On IH44 westbound exit ramp MP128A, southbound to Lincoln
IH44	10' Width	On ramp between US77 southbound going to IH44 westbound in Oklahoma City
IH44	No Overwidth	From MP125C (Westbound) to OK3/Northwest Expressway
IH44	10' Width	Westbound only between Creek Tpk & N. 193 rd East Ave. in Tulsa
US69	10' Width	On bridge going over Locust Ave - Both Directions, Eufaula, McIntosh County

OREGON

IH5	Ramp Closure	IH5 NB MP192 Exit Off-Ramp to OR99/OR126 Business (Eugene) - Effective October 3, 2011, The IH5 northbound MP192 Off-Ramp to OR99/OR126 Business will be closed. Estimated Date of Re-Opening Is January 1, 2014.
IH5	Ramp Closure	IH5 SB MP299A Exit Off-Ramp to OR43 (Portland) - Effective November 27, 2012, the IH5 southbound MP299A to OR43 will be intermittently closed between the hours of 11:30 PM & 4:30 AM. Estimated date of completion is December 31, 2012.
IH84	13' Width	IH84 MP17.82 EB & WB (Jordan Rd Underpass) - Effective January 14, 2013, This height will be restricted to 13'10" Crossing under IH84 on Jordan Rd. These restrictions are not on the IH84 mainline. Estimated

		date of completion is April 30, 2013.
IH105	Ramp Closure	Eastbound ramp to IHh5 SB (Eugene) - Effective January 27, 2013, The Eastbound IH105 ramp to IH5 Southbound will be intermittently closed at night for up to one week between the hours of 7 PM & 6 AM. Estimated date of completion is March 1, 2013.
US26	Permanent 12' Width	MP40.90 (Dennis Edwards Tunnel, West Of Portland) - All loads over 12-feet 00 In. in width must stop traffic and straddle centerline, or use certified rolling stop method. Contact Oregon for details.
US26	Ramp Closures	Effective July 5, 2012, the following ramps in the Portland area will be intermittently closed between the hours of 10 PM & 5 AM: EB On-Ramp from Skyline Blvd; Exit 72 (Zoo) EB & WB ramps; EB off-ramp to Canyon Rd; WB on-ramp from Jefferson St; EB ramps to IH405 NB/SB; WB ramps from IH405 NB/SB; WB on-ramp from Clay St; WB off-ramp to Market St. Estimated date of completion is May 31, 2013.
US26	Full Closure & Ramp Closures	US26 MP72.03 to MP73.81 EB & WB (Portland) - Effective December 3, 2012, This section of US26 will be intermittently closed between the hours of 11:30 PM & 4:30 AM (nighttime) on The Following Dates: December 3-7, 2012; January 7-11, 2013; January 14-18, 2013; One Additional Closure To Be Determined. In Addition, The Following Ramps Will Be Closed At The Same Time: On-Ramp From The Zoo To Eb Us26; Off-Ramp From Eb Us26 To Canyon Rd; On-Ramp From Jefferson St To Wb Us26 (From 8 Pm To 4:30 Am); On-Ramp From Clay St To Wb Us26 (From 8 Pm To 4:30 Am); Wb Us26 Exit 72 To The Zoo; Sb Ih405 Exit 1d To Wb Us26; Nb I-405 Exit 1d To Wb Us26 (From 10 Pm To 5 Am). Estimated Date Of Completion Is January 18, 2013.
US30	Ramp Closure	Us30 Eb Ramp To Ih405 Nb (Portland) - Effective January 18, 2013, The Us30 Eastbound Ramp To Ih405 Northbound Will Be Intermittently Closed On Friday And Saturday Nights Between The Hours Of 10 Pm & 5 Am. Effective February 3, 2013, This Ramp Will Be Intermittently Closed At Night During The Week And On The Weekend Between The Hours Of 10 Pm & 5 Am. Estimated Date Of Completion Is February 10, 2013.
OR58	Permanent 12' Width	Or58 Mp56.01 (Salt Creek Tunnel, East Of Oakridge) - All Loads Over 12 Feet 00 In. In Width Must Stop Traffic And Straddle Centerline, Or Use Certified Rolling Stop Method. Contact Oregon For Details.
OR126	Permanent 12' Width	Or126 Mp19.68 (Knowles Creek Tunnel, East Of Mapleton) - All Loads Over 12 Feet 00 In. In Width Must Stop Traffic And Straddle Centerline, Or Use Certified Rolling Stop Method. Contact Oregon For Details.
OR126	Full Closure	Or126 Mp0.25 To Mp 0.27 Eb (Railroad Crossing In Redmond) - Between January 7 And January 11, 2013, Or126 Eastbound

		(Evergreen Ave) At The Railroad Tracks In Redmond Will Be Closed To All Traffic For One Day Only Between The Hours Of 8 Am & 5 Pm For Railroad Work. Estimated Date Of Completion Is January 11, 2013.
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TEXAS

FOR TRAVEL ON THE FOLLOWING HIGHWAYS:

US59, US69, US77, US83, US84, US87, US287, US290, SH46, LP289, LP337 CONTACT TEXAS.

IH10	150' LENGTH	150' MAX LENGTH INTO/OUT OF NEW MEXICO.
IH10	12' WIDTH	ON THE WFR FROM N.LP375 TO SH178, IN EL PASO
IH10	10' WIDTH 70' LENGTH	TURNING IH10e TO SE.LP375n; IH10w TO SE.LP375s; SE.LP375n TO IH10w; AND IN EL PASO. CONTACT TEXAS FOR DETOUR
IH10	12'WIDTH 80' LENGTH	SAN ANTONIO: CLOVERLEAF @ W.IH10/LP1604. USE THE FOLLOWING DETOURS:IH10e TO LP1604e: IH10e, LP1604w, LA CANTERA (W OF IH10) X-UNDER, LP1604e. IH10w TO LP1604w: IH10w, LA CANTERA (N OF LP1604) X-UNDER, IH10e, LP1604w N.LP1604e TO IH10w: LP1604e, IH10e, SP53 X-OVER, IH10w. N.LP1604w TO IH10e: LP1604w, IH10w, LA CANTERA (NORTH OF LP1604) X-UNDER, IH10e OR LP1604w, LA CANTERA (W OF IH10) X-UNDER LP1604e, IH10e
IH10	NO PERMITS	NO PERMITS ON THE ML, EFR, AND WFR FROM TO N.LP1604 TO HUEBNER RD IN SAN ANTONIO. HUEBNER RD IS LOCATED 1/2 MILES SOUTH OF N.LP345.
IH10	HOUSTON	MUST USE IH610 TO DETOUR AROUND HOUSTON. LOADS STARTING OR STOPPING INSIDE IH160,

		CONTACT TEXAS FOR DETOUR
IH10 NFR & SFR	NO PERMITS	MP 851 TO MP853: W.US90 TO N.US69, BEAUMONT AREA
IH20	11' WIDTH	MONAHANS: FROM E.BI20 (MP83) TO FM1601 (MP101)
IH20	10' WIDTH	MIDLAND: E.BI20 TO THE MARTIN/MIDLAND COUNTY LINE
IH20	12' WIDTH 85' LENGTH &/OR 59' TRL	ROSCOE: ON THE NFR AND SFR BETWEEN 1/4 MILE WEST OF FM608 AND 1/4 MILE EAST OF FM608. 12' MAX WIDTH, 85' MAX LENGTH, AND/OR 59' MAX TRAILER LENGTH
IH20	12' WIDTH	ROSCOE: BETWEEN 1 MILE WEST OF W.US84 AND 1 MILE EAST OF W.US84.
IH20	NO WIDTH NO HEIGHT &/OR 85' LENGTH	ON NFR AND SFR IN ABILENE, BETWEEN BU83 TO SL322
IH20	TURNING LENGTH	65' MAX LENGTH TURNING FROM IH20 NFR TO US283 NB. THIS AFFECTS BOTH DIRECTIONS OF TRAVEL ON IH20 NFR, BAIRD, TX
IH20	10' WIDTH	10' MAX WIDTH BETWEEN E.BI20 IN MIDLAND AND THE MARTIN/MIDLAND COUNTY LINE. ALTERNATE EASTBOUND ROUTE: (EXIT @MP140 TO FM307 E, SH137 N, STANTON: BI20 E, IH20 E) WESTBOUND ROUTE: (EXIT @MP158 TO STANTON: BI20 W, SH137S, FM307 W, IH20 W)
IH20	NO PERMITS	NO PERMITS ON THE IH20 EB AND WB EXIT RAMPS TO E.LP254 (MP354) IN RANGER
IH20	NO WIDTH	NORTH FRONTAGE ROAD: FROM SH183 TO SP465 (FORT WORTH AREA)
IH20NFR	NO PERMITS	MP394: OVER THE BRAZOS RIVER TRUSS BRIDGE, MILLSAP AREA
IH27, US87, LP289	LUBBOCK	ALL LOADS MUST REMAIN ON IH27/US87 THROUGH LUBBOCK OR USE LP289 AROUND LUBBOCK. OTHER

		HIGHWAYS INSIDE THE LP289 MAY BE USED ONLY FOR LOADS WITH AN ORIGIN OR DESTINATION INSIDE OF LP289.
IH27EFR	NO LOWBOYS	NO LOWBOY TYPE TRAILERS OVER THE RAILROAD CROSSING LOCATED BETWEEN THE IH27 EFR AND N.US87, NORTH OF TULIA.
IH30	NO TRAVEL	NO TRAVEL THRU DOWNTOWN FT WORTH OR DALLAS WITHOUT APPROVAL. STOPPING OR STARTING IH820 FT WORTH CONTACT TEXAS FOR DETOUR. STOPPING OR STARTING LP12 DALLAS CONTACT TEXAS FOR DETOUR.
IH30	12' WIDTH	WESTBOUND FROM FM69 TO LP301 (IN SULPHUR SPRINGS)
IH35	12' WIDTH	ON THE ML, EFR, AND WFR BETWEEN 1/4 MILE SOUTH OF US82 AND 1/4 MILE NORTH OF US82 IN GAINESVILLE.
IH35 EFR & WFR	NO PERMITS	NO WIDTH FROM FM51 TO JUST SOUTH OF US82 IN GAINESVILLE
IH35EFR	NO PERMITS	NO PERMITS FROM N.LP340 in WACO TO LINCOLN CITY RD in ELM MOTT
IH35 WFR	10' WIDTH	FROM BERGER RD TO FM1237 (IN TEMPLE)
IH35WFR	10' WIDTH	N.BU77 IN LACY LAKEVIEW TO N.LP340, WACO
IH35 & N.BU77	TURNS	NO PERMITS FOR TURNS: TO OR FROM IH35 & N.BU77, LACY LAKEVIEW
IH35	13'6" HEIGHT	MAX HEIGHT UNDER FM935 – TROY (TO DETOUR RAMP OFF/RAMP ON)
IH35	80' LENGTH	MP315: FOR ALL TURNS TO OR FROM IH35 FRONTAGE ROADS & FM107/SH7 IN EDDY.
IH35	AUSTIN: SEE DETAILS	LENGTH AND/OR WEIGHT ONLY OR NOT OVER 13'6" HIGH TRAVEL THRU AUSTIN ON IH35 MUST USE INSIDE LOWER LEVEL LANE . ***DETOUR FOR AUSTIN IS: NB...IH35N, SH71E, US183N, IH35N.... VISE VERSA FOR SB TRAVEL.***
IH35	SAN	MUST USE LP1604 ON NORTH & EAST SIDE OR IH410

	ANTONIO: SEE DETAILS	ON EAST & SOUTH SIDES TO DETOUR AROUND SAN ANTONIO. FOR LOADS STOPPING OR STARTING INSIDE LP1604 CONTACT TEXAS FOR DETOUR
IH35	12' WIDTH	SOUTHBOUND ONLY S.IH410 TO SW.LP1604, SAN ANTONIO
IH35	WEIGHTS	ALL OVERWEIGHT LOADS MUST HAVE LOAD ZONED AXLE WEIGHT DISTRIBUTION WHEN MAKING TURNS IH35SB TO N.LP20, LAREDO
IH35	WEIGHTS	ALL OVERWEIGHT LOADS MUST HAVE LOAD ZONED AXLE WEIGHT DISTRIBUTION WHEN MAKING THE FOLLOWING TURNS AT THIS JUNCTION: LAREDO. AXLE WEIGHTS ARE: 22,500 SINGLE, 20,700 TANDEM, 18,000 TRIPLE, 15,750 QUAD IH35 NB TO N.US83 NB, IH35 NB TO IH35 SB, IH35 SB TO N.US83 NB, US83 SB TO N.IH35 NB, US83 SB TO N.IH35 SB
IH35E	SEE DETAILS	MUST USE IH635, and IH20 ROUTE AROUND DALLAS FOR TRAVEL ON THE WEST SIDE OF DALLAS USING LP12 & SP408 LP12 10'WIDE AND 14' TALL ONLY
IH35E	10' WIDTH	MP399A TO MP391: FM329 TO FM876, WAXAHACHIE AREA
IH35W	SEE DETAILS	MUST USE IH820 TO ROUTE AROUND FORT WORTH. LOADS STARTING OR STOPPING INSIDE ID820 CONTACT TEXAS FOR DETOUR
IH35W	NO PERMITS	N. FORT WORTH: NO PERMITS ON THE DIRECT CONNECTOR RAMP TO US287. TO TRAVEL IH35W NB TO N.US287 NB, YOU CAN ROUTE: IH35WN, FORT WORTH: EXIT @ N.TARRANT PKWY AND X-OVER TO IH35W.WFRS, N.US287.EFRN, 1 ST ONRAMP, US287N,...
IH35W	NO TURN AROUND	IN FORT WORTH: LOADS CANNOT TRAVEL IH35W NB TO IH35W SB @ N.US287.
IH37SB	NO PERMITS	NO PERMITS SB EXITING TO CARBON PLANT RD IN CORPUS CHRISTI.
IH37EFR	OVER LENGTH	A LAW ENFORCEMENT ESCORT IS REQUIRED WHEN ENTERING/LEAVING CARBON PLANT RD AT IH37 EFR,CORPUS CHRISTI

IH37EFR	11'WIDTH	MP10 TO MP11: FROM CARBON PLANT RD TO FM3386, CORPUS CHRISTI
IH37NB	NO PERMITS	NO PERMITS ON THE ENTRANCE AND EXIT RAMPS BETWEEN CARBON PLANT RD AND FM3386, CORPUS CHRISTI.
IH37 EFR & WFR	WEIGHT	4000 LBS (FOUR THOUSAND) SINGLE AXLE ON IH37 EFR AND WFR FROM ¼ MILE NORTH OF RIPPLE RD (THE "8" FR U-TURNS) TO THE NUECES/SAN PATRICIO COUNTY LINE. RIPPLE RD IS LOCATED APPROXIMATELY ¾ MILE NORTH OF S.US77 IN CALALLEN, NORTH OF CORPUS CHRISTI
IH37	NO PERMITS	BETWEEN ½ MILE SOUTH OF S. US281 AND ½ MILE NORTH OF S. US281, NORTH OF OAKVILLE (CONTACT TEXAS FOR REROUTE)
IH40	12' WIDTH	TEXAS-NEW MEXICO BORDER (IF OVER 12' WIDTH CONTACT NM FIRST)
IH40	12' WIDTH	MP96 TO MP112: 12' WIDTH FROM CONWAY (SH207) TO GROOM (FM295)
IH40NFR	NO PERMITS	NO PERMITS ON THE NFR FROM FM295 TO WHERE THE NFR ENDS WEST OF CR WEATHERLY RD IN CONWAY. CR WEATHERLY RD IS LOCATED APPROXIMATELY 3 ¼ MILES EAST OF SH207
IH40 FRS	NO PERMITS	MP121 TO MP124: NO PERMITS ON IH40 NFR AND SFR FROM W.SH90 TO E.SH70 WHERE IT IS DOUBLE SIGNED WITH IH40, JERICHO AREA
IH45	NO ACCESS	TRAVELING IH45 NB TO FM489 EB OR WB IN FREESTONE COUNTY, NEAR DEW
IH45WFR	NO WIDTH	SOUTHBOUND ON WEST FRONTAGE ROAD FROM US79 TO SH164
IH45	NO LENGTH	NO LENGTH EXITING FROM THE IH45 NB/SB ML'S TO EB/WB FM977
IH45WFR	100' LONG	WFR: FROM SH7 TO US79 & FROM FM977 TO OSR
IH45	12' WIDTH	NORTHBOUND BETWEEN N.FM1374 AND SH30 IN HUNTSVILLE
IH45	10' WIDTH	EAST FRONTAGE RD, FROM SH30 TO N.SH75 IN

		HUNTSVILLE.
IH45	NO WIDTH	FROM 1 MILE SOUTH OF US84 TO THE NAVARRO/FREESTONE COUNTY LINE.
IH45	NO WIDTH	MP178 TO MP180: NO WIDTH IN BUFFALO AREA
IH45	SEE DETAILS	NB TO S.LP336 IN CONROE MUST TAKE EXIT #84 SB TO S.LP336 IN CONROE MUST TAKE EXIT #85 NB/SB TO N.LP336 IN CONROE MUST TAKE EXIT #88
IH45 EFR & WFR	13' WIDTH	13' MAX WIDTH ON THE EFR AND WFR FROM S.LP336 TO FM830 WHERE THE FRONTAGE ROADS EXIST, CONROE
IH45	NO PERMITS	NO PERMITS TURNING TO OR FROM IH45, CONROE
IH45	NO WIDTH	MP94: NO WIDTH TURNS AT FM1097, WILLIS
IH45	HOUSTON: SEE DETAILS	MUST USE IH610 TO DETOUR AROUND HOUSTON LOADS STARTING OR STOPPING INSIDE IH610 CONTACT TEXAS FOR DETOUR
IH410	NO PERMITS	NO PERMIT ON OR INSIDE IH410 IN SAN ANTONIO. CONTACT TEXAS FOR DETOUR
IH610	SEE DETAILS	FOR LOADS STOPPING OR STARTING INSIDE IH610 AROUND HOUSTON: CONTACT TEXAS FOR DETOUR
IH610	NO PERMITS	ON THE NFR BETWEEN SP261 AND US290 IN HOUSTON.
IH610	NO PERMITS	ON SFR BETWEEN W.TC JESTER AND ELLA BLVD. HEADING EAST, HOUSTON
IH610	NO PERMITS	EFR & WFR BETWEEN US59 AND FM1093 IN HOUSTON.
IH610	NO PERMITS	ON NFR & SFR BETWEEN FM521 AND SW US90A, HOUSTON
IH635	10' WIDTH	DALLAS: BETWEEN IH35E AND US75, NORTH DALLAS AREA.
E.IH820	NO PERMITS	TRAVELING IH820NB TO SH121SB, FORT WORTH
N.IH820	100' LENGTH	IH820 / SH199 TURNING TO OR FROM MAKING LEFT TURNS

N.IH820	10' WIDTH	10' WIDE FROM IH35W TO NE.SH121
US59 SB ONLY	13' 6" HEIGHT	MAX BETWEEN FM3439 AND N.LP287 IN LUFKIN. (DUE TO LOW STRUCTURE)
US59	NO PERMITS	HEADING SOUTH IN LUFKIN AT N.LP287. (CONTACT TEXAS FOR DETOUR)
US87	13' WIDTH NO WEIGHT	BETWEEN US83 AND FM176, BETWEEN EDEN AND SAN ANGELO
US287	NO PERMIT	FROM N.LP335 (AMARILLO) TO IH40 (AMARILLO)
US287	NO WEIGHT	NO WEIGHT BETWEEN N.LP335 IN AMARILLO AND SH354.
US287	10' WIDTH	BETWEEN 1 1/4 MILES WEST OF SH207 AND 1/4 MILE EAST OF E.FM1151. THIS ALSO AFFECTS FM1151 WHERE IT IS DOUBLE SIGNED WITH US287 IN CLAUDE.
US287	NO TURNS	IN AMARILLO AT THE N.SL335 – US287 INTERSECTION: US87/US287 SB TO N.LP335 EB, N.LP335 EB TO US87/US287 NB, N.LP335 WB TO US87/US287 SB.
US287	10' WIDTH	FROM 1/4 MILE EAST OF E.FM1151 TO 1 ¼ MILE WEST OF SH207.
US287	13' 6" HEIGHT	NORTH BOUND IN WICHITA FALLS @WELLINGTON LN & @ HUNTINGTON LN (TO DETOUR RAMP OFF/RAMP ON)
US287	13' 6" HEIGHT	SOUTH BOUND IN WICHITA FALLS @HUNTINGTON LN (TO DETOUR RAMP OFF/RAMP ON)
E.LP1604	12' WIDTH	SAN ANTONIO: IH10 TO GRAYTOWN RD.
NE.LP1604	12' WIDTH 65' LENGTH	TRAVELING NE.LP1604 SE TO IH35 NB

UTAH

SR48	CLOSURE	SR48 (NEW BINGHAM HWY) AT 5600 WEST IS CLOSED. • ALTERNATE ROUTE TO COPPERTON: SR-154, 12600 SO WB, 6000 WEST NB, 11800 SO WB, U-111 NB, SR-48 WB.
SR173	NO OVERSIZE LOADS	SR173 (5400 SOUTH IN KEARNS) FROM BANGERTE HWY TO 4800 WEST – EAST AND WEST BOUND
SR252	WATCH FOR CONTRACTORS	(1000 W LOGAN) BOTH DIRECTIONS FROM SR-30 (200 NO) TO 2500 NORTH.

WASHINGTON

IH5	20,000 MAX AXLE WEIGHT	MP164.80, DIRECTION: N 20,000 LB PER AXLE WEIGHT LIMIT. TRAFFIC FROM IH90 NOT AFFECTED, SEE INTERCHANGE MAP.
IH5	20,000 MAX AXLE WEIGHT	MP18.21, DIRECTION: N, LIMITED TO 20,000 LBS. PER AXLE MAX. ALL LOADS OVER LEGAL GROSS WEIGHT MUST TRAVEL CENTER LANE OF BRIDGE. LOADS OVER 20,000 LBS. PER AXLE MUST USE ALTERNATE ROUTE. ALTERNATE ROUTE: FROM CLARK (360-397- 2446)/COWLITZ CO (360-577-3030), PERMIT AVAILABLE 8AM TO 5PM, MON-FRI, FOR GROSS WEIGHT UP TO 197,000LBS.
IH5	20,000 MAX AXLE WEIGHT	MP19.83, DIRECTION: S, LIMITED TO 20,000 LBS. PER AXLE MAX. ALL OTHER LOADS, OVER LEGAL GROSS WEIGHT, MUST TRAVEL CENTER LANE OF BRIDGE. LOADS OVER 20,000 LBS. PER AXLE MUST USE ALTERNATE ROUTE. ALTERNATE ROUTE: FROM CLARK (360-397-2446)/COWLITZ CO (360) 577-3030, PERMITS AVAILABLE 8AM TO 5PM, MON-FRI, FOR GROSS WEIGHT UP TO 197,000LBS.
IH5	20,000 MAX AXLE WEIGHT	MP228.25, DIRECTION: BOTH, IH5, MP228, NO AXLE TO EXCEED 20,000 LBS
IH5	20,000 MAX AXLE WEIGHT	MP209.35, DIRECTION: N, 20,000 LB PER AXLE WEIGHT LIMIT
IH5	20,000 MAX AXLE WEIGHT	MP156.48, DIRECTION: BOTH, 20,000 LB PER AXLE LIMIT. RESTRICTION IS ON OVERCROSSING NOT ON IH5 MAINLINE.
IH90	TRAVEL NOTE	MP45.55, DIRECTION: E, ALL LOADS OVER LEGAL WEIGHT MUST TRAVEL RIGHT LANE OF BRIDGE ONLY.
IH90	TRAVEL NOTE	MP221.95, DIRECTION: E, ALL LOADS SHALL TRAVEL

		THE CENTERLINE OF THE BRIDGE.
US12	20,000 MAX AXLE WEIGHT	MP372.57, DIRECTION: BOTH, NO AXLE TO EXCEED 20,000 LBS. WHITESTONE CREEK, 12.0 E OF WALLA WALLA, CO.
US12	20,000 MAX AXLE WEIGHT	MP307.31, DIRECTION: BOTH, 20,000 LBS PER AXLE LIMIT FOR BL AND CL-8 (5-AXLES)
US12	21,000 MAX AXLE WEIGHT	MP144.89, DIRECTION: BOTH, 21,000 LB PER AXLE WEIGHT LIMIT
US12	19,500 MAX AXLE WEIGHT	MP323.06, DIRECTION: BOTH, NO AXLE TO EXCEED 19,500 LBS FOR THE BL ON BRIDGE, HIGHWAY 12, MP323.06 BOTH DIRECTIONS
US12	BRIDGE TRAVEL	MP360.28, DIRECTION: BOTH, OVERLOADS TRAVEL CENTERLINE OF BRIDGE. TRAFFIC CONTROL IS REQUIRED.
US12	10' WIDTH	EFFECTIVE IMMEDIATELY. NO LOADS OVER 10' WIDE ON THE WILDCAT CREEK BRIDGE, MP165.95. LEGAL LOADS ONLY – BOTH DIRECTIONS
US12	12' WIDTH	MP154 TO MP165, DIRECTION: BOTH, FROM SEPTEMBER 20TH 2012 THROUGH OCTOBER 31ST 2012, MONDAY THROUGH SATURDAY DURING DAYLIGHT HOURS ONLY, NO LOADS OVER 12' WIDE. NO DETOUR AVAILABLE.
US12	OVERWEIGHT LOADS	MP0.08, DIRECTION: W, ALL OVERWEIGHT LOADS SHALL TRAVEL THE CENTERLINE OF THE BRIDGE.
US97	10' WIDTH	MP0 TO MP1.89, DIRECTION: BOTH, LOADS OVER 10' WIDE REQUIRE 2 PILOT VEHICLES. LOADS OVER 12' WIDE REQUIRE 3 PILOTS VEHICLES AND LOADS EXCEEDING 15' WIDE. OR 125' LENGTH REQUIRE 3 PILOT VEHICLES AND 24-HOUR NOTICE TO GOLDENDALE DOT OFFICE 509-773-4533. WHEN 3 PILOT VEHICLES ARE REQUIRED THE 2 FRONT WILL HAVE MINIMUM SEPARATION OF 500', #1 PILOT TO WARN ONCOMING TRAFFIC, #2 PILOT TO STOP ONCOMING TRAFFIC AS NECESSARY.
US97	11' WIDTH	NO LOADS OVER 11' WIDE FROM MP44.5 TO 46.5, BOTH DIRECTIONS. 24-HOURS A DAY. NO DETOUR AVAILABLE.

US101	20,000 MAX AXLE WEIGHT	MP338.77, DIRECTION: BOTH, 0.5 MI N OR PURDY CUTOFF. 20,000 LB PER AXLE LIMIT.
US395	20,000 MAX AXLE WEIGHT	MP241.49, DIRECTION: BOTH, 0.1 MI S OF JCT WITH SR 20, 20,000 AXLE LIMIT
US395	10' WIDTH	MP13.05 TO MP20.57, DIRECTION: BOTH THRU TRAFFIC LIMITED TO 10' WIDE LOADS AT ALL TIMES. DETOUR BY WAY OF IH182 BRIDGE. REQUESTS FOR EXEMPTION CONTACT PASCO DOT AT 509-545-2202.
SR14	10' WIDTH	MP18.89 TO MP34.66, DIRECTION: BOTH, LOADS OVER 10' WIDE MUST HAVE 1 FRONT & 1 REAR PILOT.
SR16	LOADS OVER 105,500	MP7.28, DIRECTION: N, WHEN TRAVELING FROM TACOMA TO GIG HARBOR, ALL TRUCKS OVER 105,500 LBS SHALL USE BOTH LANES IN THE DIRECTION OF TRAVEL. TRUCKS MUST STRADDLE STEEL GRATE DECKING BETWEEN LANES. ALL TIRES MUST REMAIN ON THE PAVED SURFACE.
SR124	LOADS 12'- 14'	MP0.0 TO MP44.98, PASCO TO WAITSBURG DIRECTION: BOTH, ALL LOADS 12FT TO 14 FT WIDE REQUIRE 3 PILOT CARS. ALL LOADS 14FT TO 16FT WIDE TRAVELING BETWEEN MP0.0 TO MP9.73 MUST CONTACT PASCO DOT @ 509-545-2202 WITH A TRAFFIC CONTROL PLAN. THE PLAN MUST BE APPROVED BY THE PASCO DOT OFFICE BEFORE PERMIT CAN BE ISSUED. LOADS TRAVELING BETWEEN MP9.74 TO MP44.98 MUST CONTACT THE WALLA WALLA DOT @ 509-527-4548 FOR APPROVAL BEFORE PERMIT CAN BE ISSUED. LOADS OVER 16 FT WIDE PROHIBITED.
SR167	MAXIMUM WEIGHT	MP: 5.2, DIRECTION: N, NO OVERLEGAL AXLE WEIGHTS, LEGAL LOADS ONLY. MAXIMUM WEIGHT 105,500 LBS. VEHICLES OVER 10,000 LBS, USE RIGHT LANE. NO RESTRICTION WHEN TRAVELING EB FROM SR 512 ONTO NB SR 167 OR WHEN TRAVELING SB FROM SR 161 ONTO NB SR 167; RESTRICTION ONLY WHEN CROSSING OVER THE PUYALLUP RIVER BRIDGE ON SR 167.
SR290	17,000 MAX AXLE WEIGHT	MP82, DIRECTION: BOTH, IN CITY OF SPOKANE, 17,000 LBS PER AXLE WEIGHT LIMIT
SR530	20,000 MAX AXLE WEIGHT	MP28.78, DIRECTION: 20,000 LB PER AXLE WEIGHT LIMIT

HOW TO OBTAIN LCV AUTHORITY

Carriers wanting to operate with a trailer in excess of 53', a truck-trailer configuration in excess of 88' up to 92', a truck-trailer-trailer configuration in excess of 92' up to 95', a tractor and two trailers or three trailers in excess of 81' cargo/cargo carrying length must obtain LCV authority from the Motor Carrier Division of the Department.

To obtain LCV authority a carrier will log into the online permitting system and on the left side of the screen click on the Apply for longer combination vehicle link.

From the drop down list chose which type of LCV Authority you are applying for:

- Longer combination vehicles – 2 or 3 trailers
- Truck Trailer (88 to 92')
- Trailer Exceeding 53'

Then click on the next button.

Following is a step by step for each type of LCV Authority. A carrier must apply for each individual type.

Longer Combination Vehicle – 2 to 3 trailers

Enter all the routes requesting to travel on.

- Enter the point of origin (street address if within the state or the state line if leaving or entering the state)
- Destination (address if within the state or the state line if leaving or entering the state)
- Route traveled – listed all roads traveled.

Then click the next button.

On the Application for longer Combination Vehicle: Alter Route page you can use this page to request a change to an approved route. If this is the first time of application there will be no approved routes yet and you can just click on the next button.

On the Submit Application page you as the carrier is certifying that your company is actively meeting the safety criteria in order to obtain the privilege to operate as an LCV vehicle.

When you have read and agree to the standards then click on the submit application button. If you do not agree to the safety criteria or our company does not currently meet click on the cancel button.

After clicking the submit application button the application is submitted to the Motor Carrier Division. At this point they will review your route requested and your safety record including size and weight compliance. You will then be contacted regarding their decision.

TRANSFER / REPLACEMENT OF PERMITS

Motor Carrier Division personnel may transfer permits from one vehicle to another for a \$35 fee under the following conditions:

- Annual and semi-annual permits may be transferred to another unit within the same company.
- Customer has sold or bought a vehicle.
- Lease changes from one company to another. Documentation providing evidence of permit ownership will be required prior to permit transfer.
- Company reorganization or name change.
- The original permit, or evidence of having an original permit, shall be presented to the issuing agent.
- A transfer permit will be issued the same expiration date as the original permit.

(Refer to R09-2-9)

The following information must be provided when applying for a permit:

- Motor Carrier Information:
 - USDOT number
 - Address
 - Telephone number
 - Fax number
 - Web address (if available)
 - Email address (if available)
- Vehicle information
 - License number, state, expiration date
 - Vehicle make, year, registered weight
 - Unit number
 - Complete Vehicle Identification Number (VIN)
- Specific load information
- Permit duration (Single, Semi-Annual, Annual)
- Requested routes
- Dimensions, length, width, height, front and rear overhang.

PERMIT REFUNDS

To request a permit refund, please contact the Motor Carrier Division at (801) 965-4892. For refunds on self issued permits also contact the Division at (801) 965-4892.

PERMIT REVOCATION, SUSPENSION, AND CONFISCATION

Violations of any permit that may result in the revocation, suspension or confiscation of the permit include, but are not limited to:

- speeding in excess of the posted speed limit or the speed indicated on the permit;
- lane travel;
- weather;
- load securement;
- violations of the Federal Motor Carrier Safety Regulations; and
- violations of the Hazardous Material Regulations.

Before a vehicle can be moved, it must be made legal, properly permitted and all of the out-of-service violations corrected.

Patterns of non-compliance at a carrier level may result in the following actions:

- civil penalties;
- suspension or revocation of permit privileges; or
- an order to cease and desist operations.

(Refer to R909-2-10)

COMMERCIAL VEHICLES TRAVELING ON A FREEWAY OR INTERSTATE HIGHWAY

Vehicles operating under an oversize/overweight permit shall not be operated in excess of the posted speed limit or the speed indicated on the permit, whichever is less. In no case shall a permitted movement proceed at a speed, which shall endanger the safety of persons or property.

VIOLATION OF WEIGHT LIMITS

Overweight vehicles or combination of vehicles shall be brought within legal limits at the point of violation unless there is a potential hazard, as determined by enforcement personnel. If it is determined that a hazard exists, the vehicle shall be moved to the closest location where the excessive weight can be safely transferred to another vehicle unless, in the judgment of the enforcement personnel, it is safer to proceed to the original destination. UCA 72-7-405(3)(b)

AUTHORIZED WEIGHING EQUIPMENT

Utah uses a number of different types of weighing equipment. These include portable scales, certified privately owned scales, and official port of entry facilities along state highways and interstates.

PERMIT PROVISIONS for Mobile/Manufactured Homes

- Mobile/manufactured homes to be moved on semi-trailer lowboys may be permitted.
- For loads originating in Utah, a copy of the Tax Commission Movable Structure Tax Clearance/Moving Permit (TC-138) must be:
 - Affixed to the rear end of the mobile/manufactured home or moveable structure, and
 - Be visible to any enforcement officer or agent.
 - A TC-138 must be obtained from the County Assessor in the county from which the load originates. UCA 41-1A-1320

Proof of a TC-138 permit **must be carried** in the Mobile Home.

Chapter 23

General Highway Restrictions

TRAVEL LANE RESTRICTIONS

[UCA 41-6a-702](#)

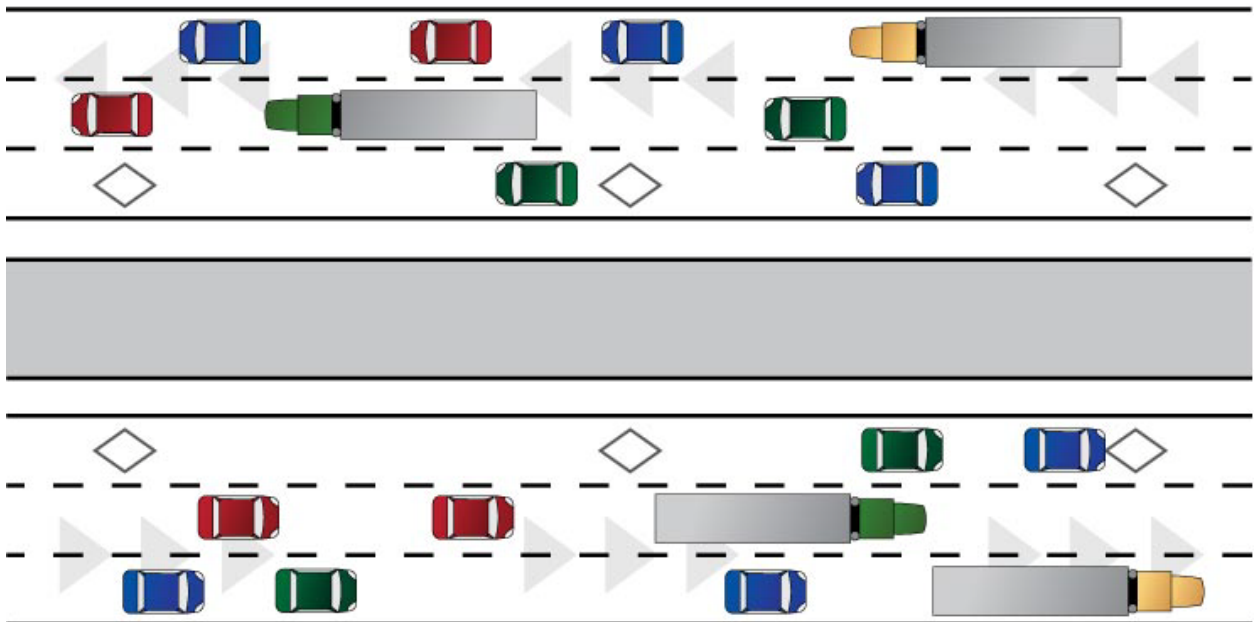
Left Lane Travel Restrictions

A person may not operate a vehicle in the left-most lane or High Occupancy Vehicle (HOV) lanes if:

- Vehicle is drawing a trailer or semi trailer regardless of size, or
- Vehicle or combination of vehicle has a gross vehicle weight of 12,001 or more pounds.
- Left most lane when three or more lanes exist in the direction of travel.

Right Lane Travel Restrictions

- Permitted non-divisible vehicles shall be driven in the right-most lane of traffic, except where the right lane must exit.
- When overtaking and passing another vehicle, permitted non-divisible vehicles shall be driven to the immediate left of the right-most lane returning to the right most lane as soon as feasible
- If right-most lane is designated as an "auxiliary" lane, vehicles shall operate in the center lane until the auxiliary" lane ends. Auxiliary" lane ends.



WEATHER TRAVEL RESTRICTIONS

[49 CFR 392.14](#)

Extreme caution in the operation of a commercial motor vehicle shall be exercised when hazardous conditions are present, such as those caused by snow, ice, sleet, fog, mist, rain, dust, or smoke, adversely affect visibility or traction. Speed shall be reduced when such conditions exist. If conditions become sufficiently dangerous, the operation of the commercial motor vehicle shall be discontinued and shall not be resumed until the commercial motor vehicle can be safely operated. Whenever compliance with the foregoing provisions of this rule increases hazard to passengers, the commercial motor vehicle may be operated to the nearest point at which the safety of passengers is assured.

No carrier shall operate a permitted vehicle or vehicles in excess of 81 feet cargo or cargo carrying length, when the following conditions exist:

- Wind in excess of 45 mph
- any accumulation of snow and ice on the roadway; or
- Visibility less than 1,000 feet

(Refer to R909-2-11)

When any of the above conditions exist the carrier may choose one of the following options:

- Cease operations until weather conditions improve
- Driver may drop a trailer at a safe location

Note: Leaving a trailer on the roadway including exit or entrance ramps is not permitted

CURFEW CONGESTION AREA TIMES AND RESTRICTIONS

General Restrictions

Unless otherwise authorized, travel is prohibited for loads or vehicles in excess of 10 feet wide, 105 feet overall length, and 14 feet in height, Monday thru Friday between 6 a.m. and 9 a.m. and between 3:30 p.m. and 6 p.m. mountain time on the following highways:

- all highways south of Perry Willard Interchange, I-15, Exit #357;
- all highways in Weber, Davis, and Salt Lake Counties;
- all highways in Utah County north of I-15, Exit #261;
- SR 68, North of mile post 16 in Utah County;
- I-80 East side of Salt Lake County mile post 139 to mile post 101 on the West side of Salt Lake County; and
- I-84 west of mile post 91.

The division may authorize exceptions to the curfew congestion restrictions based on mitigating circumstances.

(Refer to R909-2-12)

See map section in Chapter 43.

HOLIDAY TRAVEL

Travel is prohibited for loads in excess of 10' wide, 105' OAL, and 14' high during the following holidays:

- Christmas Day
- New Year's Day
- Memorial Day
- Independence Day
- Labor Day
- Thanksgiving Day

Travel is restricted on the above holidays as listed below:

- Monday Holiday Observance:

When the holiday is observed on a Monday, travel is prohibited from 2:00 p.m. on Friday until daylight on the Tuesday following the recognized holiday.

- Tuesday, Wednesday and Thursday Holiday Observance:

When the holiday is observed on a Tuesday, Wednesday, or Thursday, travel is prohibited from 2:00 p.m. on the day before the holiday until daylight the day after the holiday.

- Friday Holiday Observance:

When the holiday is observed on a Friday, travel is prohibited from 2:00 p.m. on Thursday until daylight on Monday following the recognized holiday.

Exception: The Motor Carrier Division may authorize exceptions to the above noted restrictions. This authorization may be granted by Division Administration or the Super Load Coordination Team based on mitigating circumstances.

(Refer to R909-2-13)

Note: Daylight is defined as 1/2 hour before sunrise and 1/2 hour after sunset.

SNOW PLOW OPERATIONS

Snow plows with wide blades may operate without oversize permits, when they are in compliance with the following:

- Blades in excess of 8 feet 6 inches must be equipped with a yellow, rotating beacon warning light.
- Snow plows with up to 12 feet wide blades may operate without oversize permits, when they are in compliance with:
 - lights which provide adequate illumination when the blade is in either the up, or down position;
 - signaling lights shall not be obscured; and
 - blades must be angled so that the minimum width is exposed to oncoming traffic during periods of travel between jobs.

NIGHT TIME RESTRICTIONS

Loads exceeding the following dimensions are restricted to daylight hours except as provided in R909-2-15:

- 14 feet high;
- 10 feet wide;
- 105 feet in length; or
- overhang in excess of 10 feet.

(Refer to R909-2-14.)

NIGHT TIME TRAVEL PROVISIONS

The movement of oversize loads at night will be allowed under the following conditions:

- loads may not exceed 12 feet wide on secondary highways, 14 feet wide on interstates, or 14 feet high on all roadways;
- loads exceeding 10 feet wide, 105 feet overall length, or 10 feet front or rear overhang are required to have one certified pilot escort on interstate highways and two on all secondary highways;
 - Exception. A tow truck towing vehicles with a total length of 120 feet or 10 feet wide may travel during hours of darkness and does not require a pilot escort.
- loads exceeding 92 feet overall length are required to have proper lighting every 25 feet, with amber lights to the front. Federal regulations require lighting front center and rear of load on all oversize loads regardless of length
- Lights on rear overhang of four feet for night time travel
- night time travel authorization does not supersede adverse weather conditions

The division may authorize exceptions to the night time travel provisions based on mitigating circumstances.

(Refer to R909-2-15)

CURFEW RESTRICTIONS

Unless otherwise authorized, travel is prohibited for loads or vehicles in excess of 10 feet wide, 105 feet overall length, and 14 feet in height, Monday thru Friday between 6 a.m. and 9 a.m. and between 3:30 p.m. and 6 p.m. mountain time on the following highways:

- all highways south of Perry Willard Interchange, I-15, Exit #357;
- all highways in Weber, Davis, and Salt Lake Counties;
- all highways in Utah County north of I-15, Exit #261;
- SR 68, North of mile post 16 in Utah County;
- I-80 East side of Salt Lake County mile post 139 to mile post 101 on the West side of Salt Lake County; and
- I-84 west of mile post 91.

The division may authorize exceptions to the curfew congestion restrictions based on mitigating circumstances.

Pilot escort provisions when it is required and general requirements

One pilot vehicle is required for vehicles or loads, which exceed the following dimensional conditions:

- 12 feet in width on secondary highways for non-interstate, and 14 feet in width on divided highways for interstates;
- 105 feet in length on secondary highways and 120 feet in length on divided highways; and
- overhangs in excess of 20 feet shall have a pilot escort vehicle positioned to the front for front overhangs and to the rear for rear overhangs.

Two pilot escort vehicles are required for vehicles or loads which exceed the following dimensional conditions:

- 14 feet in width on secondary highways;
- 16 feet in width on divided highways;
 - Mobile and manufactured homes with eaves greater than 12 inches shall be measured for overall width including eaves and pilot escort vehicles assigned as specified; or
- 120 feet in length on secondary highways;
- 16 feet in height on all highways; or
- when otherwise required by the division

Note: From US-89 to I-84 to I-15 and SR-201 are considered divided highway/interstate for Pilot Escort purposes

(Refer to R909-2-18)

Oversize Non-Divisible Load Provisions Requiring Police Escorts.

Police escorts are required for vehicles with loads which exceed:

- 17 feet wide or 17 feet 6 inches high on secondary highways; or
- 20 feet wide or 17 feet 6 inches high on all highways; or
- All loads in excess of 175 feet in length must have a minimum of one police escort;
- All loads in excess of 200 feet in length will require a minimum of two police escorts.

The division may require police escorts based on extenuating circumstances.

(Refer to R909-2-19.)

Before you proceed, please check secondary highway routes in this chapter.

Laura to update permit provision sheet to include loads over 8'6 to 10' needs to have all extremities lighted.

Secondary Highways with Additional Restrictions

Remove line on left hand side.

Secondary Highways with Additional Restrictions.

- (1) US 6 between the Nevada State line (RP 0) to the junction of US-50 (RP 107.5)
 - (a) Vehicles or loads exceeding 16' in width require 3 police escorts.
- (2) US 6 Eastbound from RP 239 to RP 242.
 - (a) Vehicles or loads in excess of 17' 3" high are prohibited.
- (3) State Route 9 from Hurricane (RP 10) to LaVerkin (RP 13).
 - (a) Vehicles/loads exceeding 12' in width require two certified pilot/escort vehicles.
- (4) State Route 9 from Hurricane RP 10 to LaVerkin RP 13.
 - (a) Vehicles/loads exceeding 1' in width require two certified pilot/escort vehicles.
- (5) State Route 9 from the junction of State route 17 eastbound to Zion National Park and from State Route 89 west to Zion National Park.
 - (a) Vehicles/loads exceeding 8'6" in width requires one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 10' in width require two certified pilot/escort vehicles.
 - (c) Vehicles/loads exceeding 14' in width require, in addition to certified pilot/escort vehicles, two police escorts.
 - (d) Commercial vehicles, regardless of dimensions, are prohibited through Zion National Park.
- (6) State Route 12 between the junctions of State Route 89 and State Route 24 (near Torrey, Utah).
 - (a) Vehicles/loads exceeding 10' in width require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 12' in width require two certified pilot/escort vehicles.
 - (c) Vehicles/loads exceeding 14' in width require two police escorts, in addition to certified pilot/escort vehicles..
- (7) State Route 14 between State Route 130 (RP 0) and State Route 89 (RP 41).
 - (a) Vehicles/loads exceeding 8'6" in width require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 10' in width require two certified pilot/escort vehicles.
 - (c) Vehicles/loads exceeding 12' in width are prohibited.
- (8) State Route 17 between Interstate 15 (RP 0) and LaVerkin (RP 6.07).
 - (a) Vehicles/loads exceeding 10' in width require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 12' in width require two certified pilot/escort vehicles.
- (9) State Route 18 between the junctions of Interstate 15 (RP-0) to the junction of State Route 56 (RP-51).
 - (a) Vehicles or loads exceeding 16' in width require three police escorts.

- (10) State Route 20 between Interstate 15 (RP 0) and State Route 89 (RP 20.4) Bear Valley.
- (a) Vehicles or loads exceeding 10' in width and 75' in length require one pilot/escort vehicle.
 - (b) Vehicles or loads exceeding 16' in width require three police escorts.
- (11) State Route 21 between the Nevada State Line (RP 0) and the junction of State Route 257 (RP).
- (a) Vehicle or loads require three police escorts at 16' wide.
- (12) State Route 24 between State Route 12 (Torrey) and State Routes 24 & 95 (Hanksville).
- (a) Vehicles/loads exceeding 10' in width require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 12' in width require two certified pilot/escort vehicles.
- (13) State Route 29 between Orangeville and Joe's Valley Reservoir.
- (a) Vehicles/loads exceeding 10' in width and 80' in length require two certified pilot/escort vehicles.
- (14) State Route 31 between RP 0 and RP 8.5.
- (a) Vehicles/loads exceeding 10' in width and/or 80' in length require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 12' in width are prohibited.
- (15) State Route 39 between State Route 203 (Harrison Blvd) at RP 9 and State Route 158 (Pineview Reservoir).
- (a) Vehicles/loads exceeding 10' in width or exceeding 77' in length are prohibited.
- (16) State Route 39 between State Route 158(Pineview Reservoir) and State Route 16 (Woodruff at RP 67).
- (a) Vehicles/loads exceeding 10' in width and/or 80' in length require two certified pilot/escort vehicles.
- (17) State Route 43 and State Route 44 between Wyoming (RP 0) and State Route 191 (RP 28).
- (a) Vehicles/loads exceeding 95' in length require two certified pilot/escort vehicles.
- (18) State Route 46 between the Colorado State Line and RP 18.
- (a) Vehicles/loads exceeding 10' in width and/or 80' in length require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 12' in width and/or 110' in length are prohibited.
- (19) State Route 50 between the Junction of US-6 (RP 0) to the junction of Interstate 15 (RP 119.7) Then from the junction of Interstate 15 (RP 130) and the junction of Interstate 70 (RP 149.1).
- (a) Vehicles or loads exceeding 16' in width require three police escorts.
- (20) State Route 56 between the Nevada state line (RP-0) to the junction of Interstate 15 (RP 61.5)
- (a) Vehicles or loads exceeding 16' in width require three police escorts.
- (21) State Route 59 between RP 19 and RP 23 (Hurricane Hill).
- (a) Vehicles/loads exceeding 12' in width and/or 85' in length require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 14' in width and/or 95' in length require two certified pilot/escort vehicles.

- (c) Vehicles/loads exceeding 14'6" in width require two police escorts.
- (d) Vehicles or loads exceeding 16' in width require three police escorts from (RP 0) to (RP 20).
- (22) State Route 65 between Interstate 80 and Interstate 84 (Henefer to Mt. Green).
 - (a) Vehicles/loads exceeding 80,000 pounds GVW are prohibited.
- (23) State Route 66 between State 65 and Interstate 84.
 - (a) Vehicles/loads exceeding 80,000 points GVW are prohibited.
- (24) State Route 67 (Legacy Parkway) between the junction of Interstate 15/US 89 and the junction of interstate 215.
 - (a) All vehicles with five (5) or more axles/80,000 lbs and greater are prohibited except as allowed by Utah State Code 72-3-113.
- (25) State Route 89 (Logan Canyon) between RP 461 and RP 498 (Bear Lake).
 - (a) Vehicles/loads exceeding 10' in width and/or 80' in length require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 12' in width are prohibited.
- (20) State Route 89 Kanab and Interstate 70.
 - (a) Vehicles/loads exceeding 10' in width and 75' in length require one certified pilot/escort vehicle.
- (26) State Route 92 between Highway 189 (Provo Canyon) and the Sundance Ski Resort.
 - (a) All oversize loads require two pilot/escort vehicles and two police escort vehicles.
- (27) State Route 128 between Interstate 70 and State Route 191 (RP 0 - RP 42).
 - (a) All oversize vehicles/loads exceeding 55,000 pounds GVW are prohibited.
- (28) State Route 130 between the junction of Interstate 15 (RP 6) and the junction of State Route 21 (RP 43).
 - (a) Vehicles or loads exceeding 16' in width require three police escorts.
- (29) State Route 143 between RP 3 and RP 20 (Panguitch to Brian Head).
 - (a) Vehicles/loads exceeding 8'6" width require one certified pilot/escort vehicle.
 - (b) Vehicles/loads exceeding 10' in width require two certified pilot/escort vehicles.
 - (c) Vehicles/loads exceeding 12' in width and 65' in length are prohibited.
- (30) State Route 148 from State Route 143 to State Route 14.
 - (a) Vehicles or loads exceeding 24,000 GVW prohibited through Cedar Breaks National Monument.
- (31) State Route 150 between Utah/Wyoming Border and State Route 32 in Kamas, Utah.
 - (a) Vehicles/loads exceeding 80,000 pounds GVW are prohibited.
- (32) State Route 153 between RP 9 and RP 20 (Elk Meadows to Beaver).
 - (a) Vehicles/loads exceeding 8'6" width require one certified pilot/escort vehicle.

(b) Vehicles/loads exceeding 10' in width and 65' in length are prohibited.

(33) State Route 162 Between Montezuma Creek and Aneth.

(a) Vehicles/loads exceeding 95' in length and/or 12' wide require two certified pilot/escort vehicles.

(34) State Route 189 (Provo Canyon) between RP 7 (SR-52) and RP 21 (Wallsburg Jct).

(a) Oversize loads in excess of 10' wide are prohibited.

(b) Loads in excess of 10' width may be allowed on a case by case basis with a letter of authorization from UDOT Motor Carrier Division.

(35) State Route 190 (Big Cottonwood Canyon) between Interstate 215 at Knudsen's Corner and the Salt Lake/Wasatch County Line.

(a) Vehicles/loads exceeding 9' in width and/or 65' in length require one certified pilot/escort vehicle.

(b) Vehicles/loads exceeding 10' in width and/or 80' in length require two certified pilot/escort vehicles.

(c) Vehicles/loads exceeding 12' in width are prohibited unless a letter of authorization has been obtained from UDOT Motor Carrier Division.

(36) State Route 191 (Indian Canyon) between State Routes 6 and US 40.

(a) Vehicles or loads exceeding 95' in length require two certified pilot/escort vehicles.

(b) Vehicles or loads exceeding 15' in width require two police escorts in addition to certified pilot/escort vehicles.

(37) State Route 191 between Vernal, Utah and the Wyoming State Line.

(a) Vehicles/loads exceeding 95' in length require two certified pilot/escort vehicles.

(b) Vehicles or loads exceeding 20,000 pounds on single axles and/or 34,000 pounds on tandem axles are prohibited across Flaming Gorge Reservoir Dam.

(38) State Route 191 between LaSal Junction (RP 103) and Potash Road (RP 127).

(a) Vehicles/loads exceeding 15' in width require two police escorts.

(39) State Route 210 (Little Cottonwood Canyon) between State Route 190 and Alta, Utah.

(a) Vehicles/loads exceeding 9' in width and/or 65' in length require one certified pilot/escort vehicle.

(b) Vehicles/loads exceeding 10' in width and/or 80' in length require two certified pilot/escort vehicles.

(c) Vehicles/loads exceeding 12' in width are prohibited unless written approval from UDOT Motor Carrier Division has been obtained.

(40) State Route 211 between State Route 191 and Canyon Lands (North of Monticello).

(a) Vehicles/loads exceeding 10' in width and/or 80' in length require one certified pilot/escort vehicle.

(b) Vehicles/loads exceeding 12' in width are prohibited unless written approval from UDOT Motor Carrier Division has been obtained.

(41) State Route 257 between the junction of State Route 21 (RP-0) and the junction of US-50/6 (RP 69.2).

(a) Vehicles or loads require three police escorts at 16' in width.

(42) State Route 261 between RP 7 and RP 10 (Moki Dugway) (SR 163 to SR 95).

(a) Oversize/Overweight vehicles and/or loads prohibited.

(43) State Route 264 between State Route 31 and State Route 96 (Electric Lake to East Fairview).

(a) Vehicles/loads exceeding 10' width and/or 80' in length require one certified pilot/escort vehicle.

(b) Vehicles/loads exceeding 12' in width and/or 110' in length are prohibited unless written approval from UDOT Motor Carrier Division has been obtained.

(44) Emigration Canyon between the Wasatch Drive/Sunnyside Ave. junctions and State Route 65.

(a) Vehicles/loads exceeding 9' in width and/or 65' in length require one certified pilot/escort vehicle.

(b) Vehicles or loads exceeding 10' in width and/or 80' in length require two certified pilot/escort vehicles.

(c) Vehicles/loads exceeding 12' in width are prohibited unless written approval from UDOT Motor Carrier Division has been obtained.

(45) Exceptions.

(a) The department may grant written authorization for vehicles engaged in public works projects with an origin or destination within the restricted portion of the highways listed.

(b) Longer Combination Vehicles (LCV's) are exempt from additional pilot/escort vehicle requirements.

(c) The department may grant temporary waivers to highway prohibitions provided additional police and certified pilot/escort vehicles are required. Other safety measures such as road closures, utility vehicle escorts, time of day limitations, may be required before a waiver is granted.

Chapter 24

National Defense Highway System

Eisenhower's support of the Federal-Aid Highway Act of 1956 can be directly attributed to his experiences in 1919 as a participant in the U.S. Army's first [Transcontinental Motor Convoy](#) across the United States on the historic [Lincoln Highway](#), which was the first road across America. The highly publicized 1919 convoy was intended, in part, to dramatize the need for better main highways and continued federal aid. The convoy left the Ellipse south of the [White House](#) in [Washington D.C.](#) on July 7, 1919, and headed for [Gettysburg, Pennsylvania](#). From there, it followed the Lincoln Highway to [San Francisco](#). Bridges cracked and were rebuilt, vehicles became stuck in mud, and equipment broke, but the convoy was greeted warmly by communities across the country. The convoy reached San Francisco on September 6, 1919.

The convoy was memorable enough for a young Army officer, Lt. Col. Dwight David Eisenhower, to include a chapter about the trip, titled "Through Darkest America With Truck and Tank," in his book *At Ease: Stories I Tell to Friends* (Doubleday and Company, Inc., 1967). "The trip had been difficult, tiring, and fun," he said. That experience on the Lincoln Highway, plus his observations of the German [autobahn](#) network during [World War II](#), convinced him to support construction of the Interstate System when he became President. "The old convoy had started me thinking about good, two-lane highways, but Germany had made me see the wisdom of broader ribbons across the land." His "Grand Plan" for highways, announced in 1954, led to the 1956 legislative breakthrough that created the Highway Trust Fund to accelerate construction of the Interstate System.

Eisenhower debated for the highways for the purpose of national defense. In the event of a ground invasion by a foreign power, the [U.S. Army](#) would need good highways to be able to transport troops across the country efficiently. Following completion of the highways the cross-country journey that took the convoy two months in 1919 was cut down to two weeks.

The Dwight D. Eisenhower National System of Interstate and Defense Highways consists of limited access facilities of the highest importance to the nation and are built to uniform geometric standards. They connect, as directly as practicable, the principal metropolitan areas, cities and industrial centers and provide important routes to, through and around urban areas. They serve national defense purposes and connect at border points with Canada and Mexico along routes of continental importance.

Exemption:

In 72-7-401(3)(a) it states: Except when operating on the national system of interstate and defense highways, a motor vehicle carrying livestock as defined in Section 4-1-8, or a motor vehicle carrying raw grain if the grain is being transported by the farmer from his farm to market prior to bagging, weighing, or processing, may exceed by up to 2,000 pounds the tandem axle weight limitations specified under Section 72-7-404 without obtaining an overweight permit under Section 72-7-406.

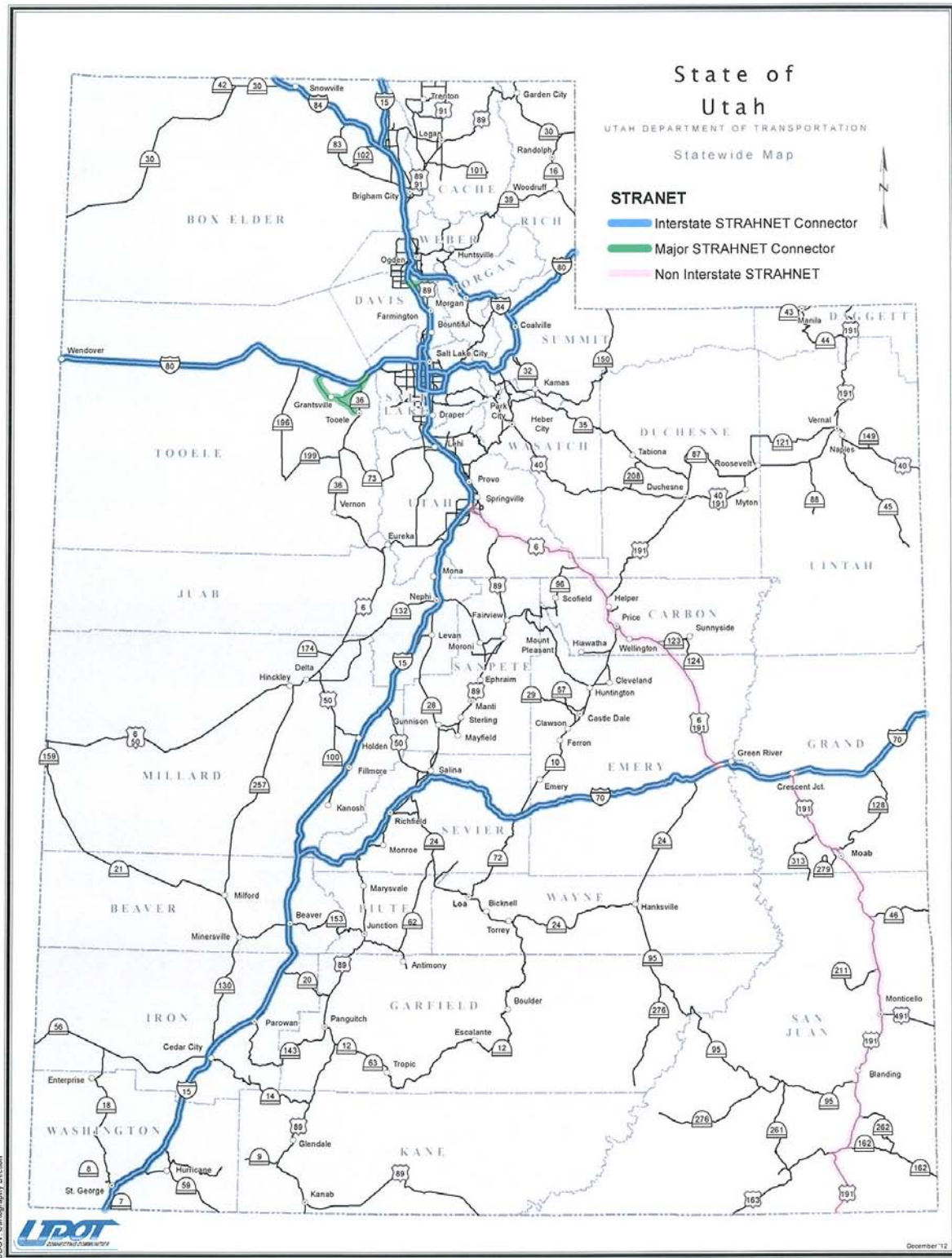
National Highways System map for Utah:

http://www.fhwa.dot.gov/planning/national_highway_system/nhs_maps/utah/index.cfm

(No shading 0193P)

ROUTE	BEGIN MP	END MP	DESCRIPTION	STRANET
0006P	173.424	300.359	From I-15 to I-70	Non Interstate STRAHNET
0015P	0.000	400.592	From Arizona/Utah State Line to Utah/Idaho State Line	Interstate STRAHNET Connector
0036P	62.897	65.914	From SR-138 to I-80 (Tooele)	Major STRAHNET Connector
0070P	0.000	231.673	From I-15 to Utah/Colorado State Line	Interstate STRAHNET Connector
0080P	0.000	119.591	From Nevada/Utah State Line to I-15	Interstate STRAHNET Connector
0080P	122.028	196.680	From I-15 to Utah/Wyoming State Line	Interstate STRAHNET Connector
0084P	0.000	42.716	From Idaho/Utah State Line to I-15 in Tremonton	Interstate STRAHNET Connector
0084P	81.043	119.773	From I-15 in Riverdale to I-80	Interstate STRAHNET Connector
0112P	0.000	5.874	From SR-138 to 1000 North (Tooele)	Major STRAHNET Connector
0138P	15.346	20.444	From I-15 to SR-36 (CHECK ALIGNMENT)	Major STRAHNET Connector
0138P	0.000	12.209	From I-15 to SR-36 (CHECK ALIGNMENT)	Major STRAHNET Connector
0191P	0.000	157.193	From Arizona/Utah State Line to I-70	Non Interstate STRAHNET
0193P*	0.521	5.669	From I-15 to US-89 (Hill Field Road)	Major STRAHNET Connector
0215P	0.000	28.982	From I-80 to I-15	Interstate STRAHNET Connector
2665P	0.000	0.966	From SR-112 to Railroad	Major STRAHNET Connector
2700P	0.000	3.304	From SR-112 to SR-138 (Sheep Lane) (Tooele)	Major STRAHNET Connector

Note: SR 193 FHWA displays this route from I-15 to Hill Field Road. It was my understanding that Kelly Lund was looking into this discrepancy (Lane)



Chapter 25

Oversize Non-Divisible Load Provisions

PERMIT FEE SCHEDULE

OVERSIZE NON-DIVISIBLE LOADS

72-7-402 and 72-7-406

Single Trip	\$30.00
Semi-Annual (180 Days)	\$75.00
Annual (365 Days)	\$90.00

Permitted oversize non-divisible must comply with the following conditions:

- All vehicles and/or loads shall be reduced to the minimum practical dimensions
- Semi-annual and annual permits may be issued for dimensions up to, but not exceeding:
 - 14 feet in height
 - 14 feet 6 inches in width
 - 105 feet in length
 - 10+ front and rear overhang
- Bulldozer blades, loader buckets or similar equipment exceeding 16 feet in width shall be removed for transport. This equipment may be hauled on the same load with the machinery after removal

Note: Exceptions may be granted by the Motor Carrier Division for an issuance of an annual oversize permit for a vehicle or combination of vehicles that exceeds 14 feet six inches wide, 14 feet high, or 105 feet long if the department determines the permit is needed to accommodate highway transportation needs for multiple trips on specific routes. For authorization contact the Division at 801-965-4892.

DEPARTMENT AUTHORITY

The Department may, at its discretion, authorize oversize permits exceeding the provisions of [UCA 72-7-406](#) for non-divisible loads over approved routes.

Note: When operating under an oversize non-divisible permit and carrying a divisible load, the load may not exceed legal divisible dimensions (14' high, 8'6" wide, 53' trailer). **For instance when transporting a divisible load, such as lumber, on a 10' wide trailer, the lumber is a divisible load and may not exceed 8'6".**

Because of additional processing requirements, Western Regional and/or loads exceeding 15' 6" H, 17' W, or over 300,000 lbs. will only be processed during M-F from 6 a.m. to 5 p.m. MST.

TRANSPORTING OF A VEHICLE AND/OR LOADS EXCEEDING:

- 17 feet in width or greater on two-lane routes
- 20 feet in width on interstates, or
- 17 feet 6 inches in height on all public highways may be allowed under the following terms and conditions:

Loads requiring regional authorization need to allow at least 48 hours in advance of the movement to provide sufficient time to obtain Regional clearance and arrange employee/vehicle support. It is the permittee's responsibility to obtain utility company authorizations/clearances and certified pilot escorts. The permit is not valid until all clearances are received.

The permittee shall notify the Motor Carrier Division by submitting a permit application online of the dimensions of the oversize vehicle and/or load and the proposed route to be used. The Motor Carrier Division will notify the Utah Department of Transportation (UDOT) region or district permit officials affected by the proposed route, and will obtain their authorization for the move. If the UDOT Permit Officer requires a department employee and **vehicle to accompany the move, the permittee must pay the cost**

The cost to the permittee shall be \$40.00, or the full cost of a department employee and vehicle, whichever is greater. Employee time shall be charged at actual rate plus expense and overtime. The permittee shall also pay any charges to move utility lines, traffic control devices or roadway appurtenances and for any damages caused by his operations.

The permit may also require a certified police escort. Costs and scheduling shall be the responsibility of the permittee. The permittee shall contact the appropriate local Law Enforcement Agency or the Utah Highway Patrol (UHP) at **(801) 965-4518** to make arrangements for a certified police escort; a minimum of 48 hours advance notice is required.

All loads in excess of 175 feet in length **MUST** have a minimum of one police escort; all loads in excess of 200 feet in length will require a minimum of two police escorts.

When a vehicle or load requires a permit that has to be adjudicated the following process is followed;

After the company submits a request for a permit, for vehicle or load that exceeds 17 ft in width or 17'6" in height, through the Motor Carrier Online System the permit is placed in a permitting Que. A member of the Superload Coordination Team will then process the application.

A Superload Team member will verify the route requested is safe for the load to travel, looking for any structures that might be too low for high loads and any construction sites it may impact. Loads exceeding 19' high may be required to submit a route survey.

When the route has been verified, a request will be sent to the corresponding region(s) the load will be traveling in. This request is sent to the Right of Way Permitting office in each region. The Right of Way office will check to ensure there are not any non-UDOT projects, such as utilities or other projects, that are going to be effected.

The Region(s) will send an e-mail back to the Superload Team indicating if the requested route is clear or indicate the location of a project that this load would interfere with. If there are no problems the permit will be issued by the Motor Carrier Specialist. If there

are problems, then a reroute will be done to route around the project(s) then the permit will be issued.

LIGHTING, SIGNING, AND FLAG REQUIREMENTS FOR OVERSIZE LOADS

- Warning lights required when headlights are necessary 49 CFR 393.11.
- Front overhang in excess of 3 feet shall be marked with a steady, amber marker light and red flag. Flag shall be a minimum of 18 inches square, that is red or florescent orange in color.
- Rear overhang exceeding four (4) feet shall be marked with red clearance lights for allowed night travel. Vehicles with front or rear overhang exceeding 20 feet from the front or rear bumper of a vehicle, or from the center of the closest axle in the absence of a bumper, shall display an American Association of Motor Vehicle Administrators (AAMVA) approved warning light which rotates, oscillates, or flashes through 360 degrees, mounted at the rearmost or foremost part of the overhang at a minimum height of 4 feet above ground.
- Tow vehicle headlights shall be operated on low beam, day or night, as an additional warning to traffic.
- Night time travel, when authorized by the Motor Carrier Division, may be permitted with marker lights indicating extreme width:
 - Amber lights front and center
 - Red lights to the rear and flags
 - **Flags front and rear**
 - **Night time travel provision**

NIGHT TIME TRAVEL PROVISIONS

The movement of oversize loads at night will be allowed under the following conditions:

- loads may not exceed 12 feet wide on secondary highways, 14 feet wide on interstates, or 14 feet high on all roadways;
- loads exceeding 10 feet wide, 105 feet overall length, or 10 feet front or rear overhang are required to have one certified pilot escort on interstate highways and two on all secondary highways;
 - Exception. A tow truck towing vehicles with a total length of 120 feet or 10 feet wide may travel during hours of darkness and does not require a pilot escort.
- loads exceeding 92 feet overall length are required to have proper lighting every 25 feet, with amber lights to the front. Federal regulations require lighting front center and rear of load on all oversize loads regardless of length
- Lights on rear overhang of four feet for night time travel
- night time travel authorization does not supersede adverse weather conditions

The division may authorize exceptions to the night time travel provisions based on mitigating circumstances.

(Refer to R909-2-15)

Oversize non-divisible load lighting:

- warning lights required when headlights are necessary;
- front overhang in excess of three feet shall be marked with a steady, amber marker light and red flag;
- rear overhang exceeding four feet shall be marked with red clearance lights for night travel;

- vehicles with front or rear overhang exceeding 20 feet from the front or rear bumper of a vehicle, or from the center of the closest axle in the absence of a bumper, a rotating or flashing beacon visible from a minimum of 500 feet, and shall be displayed at a minimum height of four feet above ground;
- tow vehicle headlights shall be operated on low beam, day or night, as an additional warning to traffic; and
- night time travel, when authorized by the division may be permitted with marker lights indicating extreme width using amber lights front and center, and red lights to the rear.

Oversize non-divisible load sign requirements. Non-divisible oversize loads exceeding 10 feet in width, 14 feet in height and 105 feet in length shall display an "OVERSIZE LOAD" sign, to warn the motoring public that extra large vehicles are in operation. Signs must:

- be 7 feet by 18 inches;
- have a yellow background with 10 inch high black letters that are painted with 1 5/8 inches wide stroke to read: "OVERSIZE LOAD";
- be impervious to moisture;
- have front signs mounted on front bumper or on top of vehicle cab with letters presented toward the front of the vehicle;
- have rear signs positioned at the rear most part of the
- Vehicle or load as feasible, ensuring in all cases that the load does not obstruct the view of the sign;
- if possible, have the bottom edge of the sign be positioned not more than 5 feet above the road surface;
- be mounted with adequate supporting anchorage, constructed, maintained, and displayed so that they are clearly legible at all times;
- be covered, removed or placed face down when the vehicle is not engaged in an oversize movement; and

Oversize non-divisible load flag requirements. Red or orange fluorescent flags must be affixed on all extremities when:

- vehicle or load exceeds 10 feet in width;
- loads on a vehicle exceeding three feet to the front or four feet to the rear of the bed or body while in operation;
- flags shall be completely clean and not torn, faded, or worn out and shall be fastened so as to wave freely; and minimum of 18 inches square
- over dimensional flagging is not required on LCVs.

(Refer to R909-2-20.)

SIGN REQUIREMENTS

Non-divisible oversize loads exceeding 10 feet in width, 14 feet in height and 105 feet in length shall display an "OVERSIZE LOAD" sign. The purpose for these signs is to warn the motoring public that extra large vehicles are in operation.

DIMENSIONS

- Signs must be 7 feet x 18 inches.
- Signs must have a yellow background with 10 inch high black letters; painted with 1 5/8 inches wide stroke to read: "OVERSIZE LOAD"

- Sign material is to be impervious to moisture.
- Front signs may be mounted on front bumper or on top of vehicle cab with letters presented toward the front of the vehicle.
- Rear signs shall be positioned at the rear most part of the vehicle or load as feasible and presented to the rear, ensuring in all cases that the load does not obstruct the view of the sign.
- If possible, the bottom edge of the sign should be positioned not more than 5 feet above the road surface.
- Signs should be mounted with adequate supporting anchorage, constructed, maintained, and displayed so that they are clearly legible at all times.
- When the vehicle is not engaged in an oversize movement, signs shall be covered, removed, or placed face down.
- Signs shall be completely clean and not torn, faded, or worn out.

Note: This requirement often allows for judgment to be exercised by both law enforcement personnel and the motor carrier industry. The most important consideration is whether the sign can be seen and understood by the oncoming motoring public. As a result it is not necessary the sign is positioned "exactly" 5 feet above the road surface, but that it meets the safety intent of the requirement.

FLAGGING REQUIREMENTS

Red/florescent orange flags must be affixed on all extremities when:

- Vehicle and/or load exceed 10 feet in width.
- Load overhangs the side(s) of the vehicle(s).
- Loads on a vehicle exceeding 4 feet to the rear of the bed or body while in operation.
- Loads on a vehicle exceeding 3 feet beyond the front of the power unit
- Flags shall be completely clean and not torn, faded, or worn out and shall be fastened so as to wave freely
- and a minimum of 18 inches square.

Note: Amber lights are not required when the overhang is positioned between vehicles in combination.

CONVOYS

The movement of more than one permitted vehicle is allowed provided prior authorization is obtained from the Motor Carrier Division with the following conditions:

- Number of permitted vehicles in the convoy shall not exceed two.
- Loads may not exceed 12' wide or 150' overall length.
- Distance between vehicles shall not be less than 500 feet or more than 700 feet.
- Distance between convoys shall be a minimum of one mile.
- All convoys shall have a certified pilot/escort in the front and rear with proper signs.
- Police escorts or UDOT personnel may be required.
- Convoys must meet all lighting requirements required in 49 CFR 393.11 and in the lighting section for nighttime travel.
- Convoys are restricted to freeway and interstate systems.
- Approval for convoys may be obtained by contacting the Motor Carrier Division or any Port of Entry.

(Refer to R909-2-21)

Note: The Motor Carrier Division on a case-by-case basis may grant exceptions to the above.

Chapter 26

Oversize Divisible Load Provisions

PERMIT FEE SCHEDULE

Single Trip	\$30.00
Semi-Annual (180 Days)	\$75.00
Annual (365 Days)	\$90.00

OVERSIZE DIVISIBLE PERMIT PROVISIONS

An oversize permit must be obtained for moving a combination of vehicles or loads exceeding the legal limits under the following conditions:

- The height of the combination and/or load does not exceed 14 feet.
- The width of the combination and/or load does not exceed 8 feet 6 inches.
- **In multiple trailer combinations, a lighter trailer may not be placed in front of a heavier trailer when the weight difference is greater than 4,000 pounds (R 909-2-16.)**

TRAILERS IN EXCESS OF 48 FEET TO 57 FEET IN LENGTH

Semi-trailers exceeding 48 feet, and up to 53 feet in length are not required to purchase oversize permits when operating on or within one mile of state designated routes and US highways.

Vehicles operating more than one mile from state designated routes and US highways will require an oversize permit available on a single trip, semiannual or annual basis.

Trailers exceeding 53 feet but not to exceed 57 feet shall acquire a single trip, semiannual or annual permit.

- Trailers in excess of 53 feet must have LCV authority to purchase semi-annual and annual permits.

(Refer to R909-2-22 Trailers in excess of 48 to 57 Feet in Length.)

The following restrictions will apply to trailers exceeding 53 feet in length on all highways in Utah:

- Trailers in excess of 53 feet and hauling divisible loads will be required to meet these requirements. If hauling a non-divisible load, the laws and regulations for non-divisible loads apply.
- Dual or super single tires (14 inches or greater) are required on all trailer axles.
- Rear under ride protection is required. **Title 49 Part 393.86**
- Gross vehicle weight will be determined by Bridge Table B Extended, in Chapter 21 - Bridge Table.
- Stretch trailers transporting divisible loads may not exceed 53'. Stretch trailers may obtain LCV authority to 57' length. Stretch trailers transporting divisible loads may not exceed legal width or overhangs as per **Chapter 19.**

LONGER COMBINATION VEHICLES (LCVS) – DEFINED

Motor Carriers operating LCVs in excess of 81 feet cargo/cargo carrying length must apply and be approved to operate on designated routes on Utah's Interstate System See Map Section.

Authorized motor carriers may operate interstate LCV's with a cargo or cargo carrying length as follows:

- a tractor trailer or tractor trailer combination in excess of 81 feet not to exceed 95 feet cargo or cargo carrying length
- A tractor and 2 or 3 trailer(s) combination in excess of 81 feet not to exceed 95 feet cargo/cargo carrying length; or
- a truck and two-trailer combination in excess of 92 feet not to exceed 95 feet in length, 14 feet in height, or 8 feet 6 inches in width.

LCV conditions for operation:

- in combinations, a longer trailer shall precede the shorter trailer ;
- non-divisible dimensions with a width greater than 8 feet 6 inches or height greater than 14 feet, may not be transported on LCV's; and
- acceptable travel conditions exist in accordance with hazardous conditions for loads in excess of 81 feet cargo or cargo carrying length.

A truck and single trailer exceeding legal length may be permitted up to 88 feet, and requires LCV authority exceeding 88 feet up to 92 feet.

A dromedary unit when exceeding legal length may be permitted up to 88 feet.

LCV's and double trailers exceeding 81 feet cargo carrying length may not operate on secondary highways other than those pre-approved by the division.

(Refer to R909-2-23. Longer Combination Vehicles.)

Application for LCV authority may be done by clicking on the Longer Combination on our online permitting system or call the Motor Carrier Division at (801) 965-4892 or (866) 215-5399.

Rocky Mountain Doubles

A tractor and two trailers, consisting of a long and a short trailer, and a cargo/cargo carrying length of 95 feet or less, when coupled together, including front and rear overhangs.

Turnpike Doubles

A tractor and two trailers of equal length including the cargo/cargo carrying length as above.

Double Trailers

Legal-61', Permitted-81", LCV Authorized-95', cargo/cargo carrying length.

Triple Trailers

A tractor and three trailers of equal length, and a cargo/cargo carrying length of 95 feet or less, when coupled together including front and rear overhangs.

Truck Trailer Combinations Include Overall Length

- Truck Trailer
 - Legal: 65'
 - Permitted: 88'
 - LCV Authorized: 92'
 - Interstate: See LCV authorization and application procedure above.
- Dromedary Unit
 - Legal: 65' overall length
 - Permitted: 88'

Note: A truck tractor equipped with a dromedary unit pulling a semi trailer where the dromedary unit is transporting Class 1 explosives or any munitions-related security material as specified by the Department of Defense is subject to a 75-foot minimum overall length limit on the National Network and reasonable access routes.

- Truck and Two Trailer Combination
 - Legal: 65'
 - Permitted: 92'
 - LCV Authorized: 95'
 - See LCV authorization and application procedure above

Motor Carriers operating longer combination vehicles or LCV's must apply and be approved to operate on designated routes on Utah's interstate system.

Authorized motor carriers may operate interstate LCV's with a cargo or cargo carrying length as follows:

- a tractor trailer or tractor trailer combination in
excess of 81 feet not to exceed 95 feet cargo or cargo carrying length; or
- a truck and two-trailer combination in excess of 92 feet not to exceed 95 feet in length, 14 feet in height, or 8 feet 6 inches in width.

LCV conditions for operation:

- in combinations, a longer trailer shall precede the shorter trailer ;

- non-divisible dimensions with a width greater than 8 feet 6 inches or height greater than 14 feet, may not be transported on LCV's; and
- acceptable travel conditions exist in accordance with hazardous conditions for loads in excess of 81 feet cargo or cargo carrying length.

A truck and single trailer exceeding legal length may be permitted up to 88 feet, and requires LCV authority exceeding 88 feet up to 92 feet.

A dromedary unit when exceeding legal length may be permitted up to 88 feet.

LCV's and double trailers exceeding 81 feet cargo carrying length may not operate on secondary highways other than those pre-approved by the division.

Refer to R909-2-23. Longer Combination Vehicles.

A combination of truck, truck tractor, semi-trailer and trailer(s), which exceed legal dimensions/weight and operate on highways by permit for transporting divisible loads.

LCV Conditions for Operation

- In combinations, a longer trailer shall precede the shorter trailer,
- Non-divisible dimensions (i.e., width greater than 8 feet 6 inches and height greater than 14 feet) may not be transported on LCVs.

Acceptable travel conditions exist in accordance with 43CFR 392.14 for loads in excess of 81' cargo/cargo carrying in length. Loads will not be permitted to travel when the following conditions exist:

- Wind in excess of 45 mph
- any accumulation of snow and ice on the roadway; or
- Visibility less than 1,000 feet

When any of the above conditions exist the carrier may choose one of the following options:

- Cease operations until weather conditions improve
- Driver may drop a trailer at a safe location

Exemptions

- Flagging Requirements: Over dimensional flagging is not required on LCVs. However, companies may choose to display flags as per their company policy.

Signing Requirements: Oversize load signs are not required on LCVs. However, companies may choose to display oversize signs as per company policy

Chapter 27

Overweight Non-Divisible Load Provisions

PERMIT FEE SCHEDULE

Combination Oversize/Overweight Non-Divisible Load Permits up to 125,000 GVW are available when both size and weight restrictions are met:	
Single Trip	\$60.00
Semi-Annual (180 Days)	--
80,001-84,000 pounds	\$180.00
84,001-112,000 pounds	\$320.00
112,001-125,000 pounds	\$420.00
Annual (365 Days)	--
80,001-84,000 pounds	\$240.00
84,001-112,000 pounds	\$480.00
112,001-125,000 pounds	\$540.00
Combination Overweight / Oversize Load Permits Exceeding 125,000 Pounds GVW	
Minimum	\$65.00
Maximum	\$540.00

Note: Overweight/Oversize permit fees for vehicles with a GVW in excess of 125,000 pounds are determined by the following table. Actual weights and miles shall be rounded to the next higher increment. [UCA 72-7-406\(7\)\(c\)\(i\)\(ii\)](#)

Fee Table for Non-Divisible Loads Exceeding 125,000 lbs. gross weight

Miles	50	100	150	200	250	300	350	400	450	500	550	600	650
Pounds													
150,000	\$80	\$80	\$130	\$170	\$210	\$250	\$290	\$340	\$380	\$420	\$460	\$500	\$540
175,000	\$80	\$110	\$170	\$230	\$290	\$340	\$400	\$460	\$510	\$540			
200,000	\$80	\$140	\$220	\$290	\$360	\$430	\$500	\$540					
225,000	\$90	\$170	\$260	\$350	\$440	\$520	\$540						
250,000	\$100	\$200	\$310	\$410	\$510	\$540							
275,000	\$120	\$230	\$350	\$470	\$540								
300,000	\$130	\$260	\$400	\$530									
325,000	\$150	\$290	\$440	\$540									
350,000	\$160	\$320	\$490										
375,000	\$180	\$350	\$530										
400,000	\$190	\$380	\$540										
425,000	\$210	\$410											
450,000	\$220	\$420											
475,000	\$240	\$470											
500,000	\$250	\$500											
525,000	\$270	\$530											
550,000	\$280	\$540											
575,000	\$300												
600,000	\$310												
625,000	\$330												
650,000	\$340												

675,000	\$360
700,000	\$370
725,000	\$390
750,000	\$400
775,000	\$420
800,000	\$430
825,000	\$450
850,000	\$460
875,000	\$480
900,000	\$490
925,000	\$510
950,000	\$520
950,000+	\$540

Permitted vehicles must comply with the following conditions:

- All vehicles and/or loads shall be reduced to the minimum practical dimensions.
- A permit for non-divisible loads will indicate the axle and gross weight authorized.
- **Exception. An overweight Non- Divisible load may operate with a divisible overweight permit provided the axle, gross and bridge limitations do not exceed those specified on the permit.**

(Refer to R909-2-24)

Note: If you are exceeding legal size you will need a separate permit for a Non-Divisible load

- **Classification of Divisible vs. Non-Divisible will be determined by the nature of the load and the vehicle configuration.**

OVERWEIGHT NON-DIVISIBLE LOADS

An overweight non-divisible load permit may be issued for moving a combination of vehicles and loads exceeding the legal limits under the following conditions:

- The vehicle or combination of vehicles is properly registered for 78,001 to 80,000 pounds or equal to or greater than the gross vehicle weight.
- Movement of overweight non-divisible loads shall meet requirements specified under rail crossings UCA 41-6a-1205.
- Permits may be obtained at three ports of entry locations upon entry, I-15 (St. George Port of Entry) and West bound I-80 (Echo Port of Entry) and East bound I 80 at (Wendover Port of Entry). or through the Motor Carrier Division. Contact information can be found in the Utah Agency Directory Section or by contacting the Motor Carrier Division at (801) 965-4892 or (866) 215-5399.

Before moving a vehicle or load, which exceeds legal dimensions as outlined in (Chapter 19) Legal Size Vehicle Dimensions a permit must be obtained and carried in the vehicle for inspection by law enforcement personnel.

The appropriate permit for oversize vehicles and loads must be obtained prior to operating within the State of Utah, except when entering on I-15 (St. George Port of Entry) and West bound I-80 (Echo Port of Entry) and East bound I 80 at (Wendover Port of Entry). The permit may be in paper or electronic format.

Permits may be purchased online at www.udot.utah.gov/public/mcs prior to entry. Permits may be purchased by major credit card and are issued on a single trip, semiannual or annual basis.

SINGLE, SEMI ANNUAL, ANNUAL TRIP PERMITS FOR OVERWEIGHT NON-DIVISIBLE LOADS

Vehicles with a gross vehicle weight of less than 125,000 may be permitted on a single trip, semiannual, or annual basis as described below:

Single Axle	29,500 lbs
Tandem Axle	50,000 lbs
Tridem Axle	61,750 lbs
Trunnion Axle	60,000 lbs
Gross Weight	125,000 lbs

The above weights are for general reference. Actual weight must comply with Bridge Table allowances

OVERWEIGHT NON-DIVISIBLE LOADS EXCEEDING 125,000 LBS

Loads exceeding 125,000 pounds gross, or axle weights in R909-2-24, may only purchase single trip permits.

Axle, bridge, and gross weight allowances will be determined based on the non-divisible bridge table formula $\sim 1.47 \times 500 (LN/N-1 + 12N + 36)$ or in accordance with the bridge table.

9 feet wide axles are allowed 7.5% more weight.

10 feet wide axles are allowed 15% more weight than 8 feet wide axles.

When using an axle equipped with eight tires, rather than four, add 10% to the weight authorized for an 8 foot wide axle group.

All tires shall be in compliance with the manufacturers tire load rating as indicated on the tire side wall.

All STE operations must have an STE profile sheet when the axle limitations specified in Table 3 or bridge table are exceeded.

(Refer to R909-2-26)

Add hyperlink to bridge table

Note: Please refer to Chapter 35: Special Mobile Equipment or Chapter 36: Special Truck Equipment for more info on these subjects and applicable weights.

CHAPTER 28

Overweight Divisible Load Provisions

COMBINATION OVERSIZE/OVERWEIGHT DIVISIBLE LOAD PERMITS ARE AVAILABLE WHEN BOTH SIZE AND WEIGHT RESTRICTIONS ARE MET:	
Single Trip	\$60.00
Semi-Annual (180 Days)	--
80,001-84,000 pounds	\$180.00
84,001-112,000 pounds	\$320.00
112,001-129,000 pounds	\$420.00
Annual (365 Days)	--
80,001-84,000 pounds	\$240.00
84,001-112,000 pounds	\$480.00
112,001-129,000 pounds	\$540.00

TIRE WIDTH

In circumstances where weight limitations are based on tire width, the manufacturer's size, as indicated on the sidewall will be used to determine maximum tire width.

Example. Tire sidewall reads: 265 R 22. 265 is the metric width. To convert, multiply 265 by .04 which gives you 10.6. Your tire would be 10.6 inches wide.

OVERWEIGHT DIVISIBLE LOADS

An overweight divisible load permit may be issued for moving a combination of vehicles and loads exceeding the legal limits under the following conditions:

- The vehicle or combination of vehicles is properly registered for 78,001 to 80,000 pounds.
- The width of the vehicle does not exceed 8 feet 6 inches wide or 14 feet high.
- Configurations equipped with tires 11 inches wide or less will be allowed 450 pounds per inch of tire width.
- Configurations equipped with tires 11 inches wide or greater will be allowed 500 pounds per inch of tire width.
- All axles weighing more than 10,000 pounds are required to have at least four tires per axle.

- A tridem axle group that is designed for equalized weight distribution, equipped with single tires less than 14 inches in width, will be allowed 30,000 pds.
- A tandem axle group that is designed for equalized weight distribution, equipped with single tires less than 14 inches in width, will be allowed 20,000 pds. All axles in a group must be duals or super singles to be allowed maximum weight.
- Dual or super single tires, that are 14 inches or greater, are required on all trailer axles.
- This does not apply to steering axles, self-steering variable load steering (VLS)/retractable axles, or wide base single tires (14 inch or greater as indicated by the manufacturer's sidewall rating).
- The following options are available **for all axles in a group**:
 - Dual tires on all axles
 - Super wide single tires (14 inches wide or greater)
 - Not to exceed 10, 000 pounds per axle
 - The axle, groups of axles, and GVW do not exceed the Utah Bridge Table.
 - The combination unit will conform to the Utah Bridge Table B Extended and the legal axle and gross vehicle weight limits.

An overweight divisible load permit may be issued for moving a combination of vehicles and loads exceeding the legal limits under the following conditions:

- **The vehicle or combination of vehicles is properly registered for 78,001 to 80,000 pounds;**
- **The width of the vehicle does not exceed 8 feet 6 inches wide or 14 feet high;**
- **All axles weighing more than 10,000 pounds are required to have at least four tires per axle except for steering axles, self-steering variable load suspension or retractable axles, or wide base single tires, that are 14 inches or greater as indicated by the manufacturer's sidewall rating**

Overweight divisible load options are:

- **dual tires on all axles;**
- **super wide single tires that are 14 inches wide or greater;**
- **not to exceed 10,000 pounds per axle;**
- **the axle, groups of axles, and GVW does not exceed the bridge formula $W = 500\{LN/(N-1) + 12N+36\}$; and**
- **all axles in the group must be duals or super singles to be allowed maximum authorized weight.**

The combination unit will conform to the bridge formula and the legal axle and gross vehicle weight limits.

A divisible load permit may not be used to transport a non-divisible load.

- **Exception. An overweight non-divisible load may operate with a divisible overweight permit provided the axle, gross and bridge limitations do not exceed those specified on the permit.**

(Refer to R909-2-24. Overweight Divisible Load Provisions)

VEHICLES TRANSPORTING LOGS FROM FOREST TO SAWMILL

[UCA 72-7-401\(2\)\(e\)](#)

Vehicles transporting logs or poles from forest to sawmill are exempt from size, axle, weight, and load limitations:

- When the vehicle is not operated on an Interstate Highway or the Defense Highway System: US491; US 191 (from US491 to I-70); US 6, and all roads leading into or out of a military facility
- When the gross vehicle weight does not exceed 80,000 pounds; and
- When the vehicle or combination of vehicles are in compliance with [UCA 72-7-404\(1\)](#)

SECTION 5

Mobile Homes, Houses and Buildings

Leave space for picture/from trucking guide

Chapter 29

Mobile and Manufactured Homes

LEGAL DIMENSIONS:

The legal dimensions of mobile and manufactured homes when no oversize permit is required are:

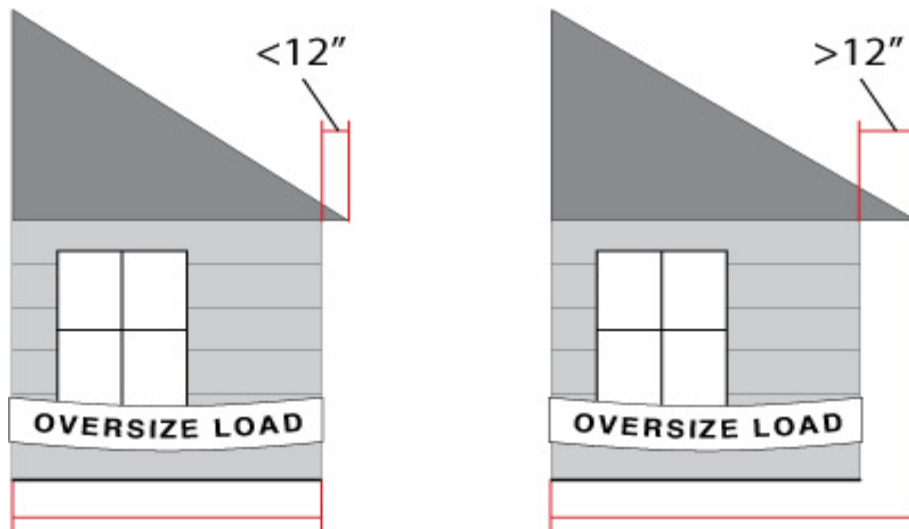
- Width: 8'6"
 - Safety appurtenances such as binder chains, clearance lights, rub rails, and load securing devices, or other devices such as door knobs, etc, may extend up to 3 inches beyond the prescribed width on either side
- Height: 14'
- Length: 45 feet. Mobile/Manufactured home only
- Overall Length: 65' overall length of Mobile/Manufactured home and tow vehicle

MEASURING MOBILE/MANUFACTURED HOMES

When the legal dimensions are exceeded, an oversize permit is required. Any mobile/manufactured home with any eave in excess of 12 inches and included in the overall width measurement.

- Width is measured horizontally (wall to wall) or actual structure that would include any protuberance on the mobile/manufactured home. Safety appurtenances such as binder chains, clearance lights, rub rails, and load securing devices, **or other devices such as door knobs, etc** may extend up to 3 inches beyond the prescribed width on either side. See note when eaves will be included in width measurement.
- Height is measured vertically from level road surface to the highest point of the mobile/manufactured home when hitched to tow vehicle and ready for the road or loaded on semi-trailer ready for the road.
- Length for a Mobile/Manufactured home only is measured horizontally from front of the trailer hitch to the rearmost protuberance on the mobile/manufactured home, or semi-trailer lowboy.
- Overall Length: 65' combination of Mobile/Manufactured home and tow vehicle
Trailer-tow combination or truck-trailer and semi-trailer lowboy, measured horizontally from the front bumper of the tow vehicle to the rearmost protuberance on the mobile/manufactured home, or semi-trailer lowboy.

Except as specified above, these Mobile/Manufactured homes shall be transported in compliance with this chapter.



Example for measuring eaves

Permits

Mobile/Manufactured homes, moved on their own running gear, which exceed the legal dimensions, may be moved on any highways under authority of an overweight/oversize permit.

For loads originating within Utah, a copy of the Tax Commission Movable Structure Tax Clearance/Moving Permit (TC-138) shall be affixed to the rear end of the Mobile/Manufactured home so as to be visible to any enforcement officer or agent. Proof of obtaining a TC-138 permit must be submitted, or presented prior to, or at the time of application. The oversize load permit will not be issued without proof of a TC-138 permit.

Axle and Tire Requirements for Mobile/Manufactured homes up to 14 feet 6 inches wide.

Mobile/manufactured home units see Tables I and II below for axle and tire requirements.

TABLE I Axle and Tire Requirements

Width of Home	Length of Home	Number of 6,000 lb Rated Axles	Minimum Standards of Rated Axles Mobile/Manufactured Home Rated Tires
12 feet wide	To 60 feet	2 Axles	7 x 14.5 / 8 ply
	Greater than 60 feet to 80 feet	3 Axles	7 x 14.5 / 8 ply
14 feet wide	To 52 feet	2 Axles	7 x 14.5 / 8 ply
	To 72 feet	3 Axles	7 x 14.5 / 8 ply
	To 80 feet	4 Axles	7 x 14.5 / 8 ply

TABLE II Axle and Tire Requirements

Width of Home	Length of Home	Number of 6,000 lb Rated Axles	Minimum Standards of Rated Axles Mobile/Manufactured Home Rated Tires
12 feet wide	To 60 feet	2 Axles	8 x 14.5 / 8 ply
	Greater than 60 feet to 80 feet	3 Axles	8 x 14.5 / 8 ply
14 feet wide	To 52 feet	2 Axles	8 x 14.5 / 8 ply
	To 72 feet	3 Axles	8 x 14.5 / 8 ply

TABLE III Tow Vehicle Requirements

Width of Vehicle to be towed	Tire Width	GVWR	Weight	Rear Axle Rating
Over 8 feet to 10 feet	7.00 inches	NA	6,000 lbs	N/A
Over 10 feet to 12 feet	8.00 inches	35,000 GVW	8,000 lbs	15,000 lbs
Over 12 feet to 14 feet 6 inches	8.25 inches	35,000 GVW	9,000 lbs	15,000 lbs

DIAGONAL (BIAS) PLY, SPECIAL TIRES LIMITED TO USE ON MOBILE HOMES ONLY, TIRES MOUNTED ON 15% DROP CENTER RIMS, TIRE AND RIM ASSOCIATION STANDARD

Tire Size															
	30	35	40	45					70	75	80	85	90	95	100
7 – 14.5 MH	1140	1240	1350	1440					1870	1940	2020	2090	2160	2230	2300
8 – 14.5 MH	1380	1510	1630	1750					2270	2360	2450	2540	2620	2710	2790
9 – 14.5 MH	1600	1750	1890	2020					2620	2730	2830	2940	2940	3130	3230

Note: For restricted speed highway service not exceeding 50 mph, the above load ratings may be increased 9%.

Important: Always use approved tire and rim combinations for diameter and contours.

Application Instructions for 14'6" – 16' Mobile/Manufactured Homes

1. Make sure both pages are completely filled out.
 2. All axles must be braking axles.
 3. Axles must be rated for at least 6000 pounds.
 4. Take the number of tires (6 axles X 2 = 12) and multiply by the tire pressure (found in table above)
- Add the hitch weight and together they must be equal to or greater than the axle weight of the home.

Example:	Approx. axle weight for home 26,120 lbs Approx. hitch weight 6790 lbs Total number of axles 6 Number of braking axles 6 Tire size and inflation pressure 8 – 14.5 / 70 lbs Manufacturer's tire rating 2270 lbs	12 tires X 2270 lbs Tire Rating = 27,240 lbs + 6790 lbs Hitch Weight = 34030 lbs	
		34,030 lbs > 26,120 lbs	This would pass because the rating and hitch weight are greater than the axle weight of the home

* If they do not pass they would need to increase their tire pressure or the number of axles.

"Length", for the purposes of transportation only, means the distance from the extreme front of the mobile/manufactured home to the extreme rear, including the drawbar and coupling devices, but not including expandable features that do not project from the body during transportation.

Determination of the number of axles required by use of the above tables does not eliminate the requirement for each axle to be capable of withstanding the actual imposed dead load without exceeding the maximum allowable stresses on tire load rating.

Tire and axles shall meet Federal Highway Administration Motor Carrier Safety Regulations required for vehicles used in interstate commerce. Such trailers will be of structural capacity to safely accommodate the loading at all times and the tire loading shall not exceed that recommended by the tire manufacturer.

- Mobile/Manufactured homes exceeding 14 feet 6 inches up to 16 feet in wall-to-wall width, transported on their own running gear, may be permitted on a single trip basis with approval from the Headquarters Port of Entry under the following conditions:
- All tires shall be in compliance with the manufacture's tire load rating as indicated on the tire sidewall.
- Axle/suspensions shall not exceed manufacture's capacity rating.
- All trailer axles shall be equipped with operational brakes.

Note: The Department will consider mobile homes in excess of 16 feet wall-to-wall width on a case-by-case basis.

Tow Vehicles

Tow vehicles shall comply with the following minimum requirements:

TABLE III Tow Vehicle Requirements

DIAGONAL (BIAS) PLY, SPECIAL TIRES LIMITED TO USE ON MOBILE HOMES ONLY,

TIRES MOUNTED ON 15% DROP CENTER RIMS, TIRE AND RIM ASSOCIATION STANDARD

Tire Size	TIRE LOAD LIMITS (LBS) AT VARIOUS COLD IMFLATION PRESSURES (PSI)														
	30	35	40	45	50	55	60	65	70	75	80	85	90	95	100
7 – 14.5 MH	1140	1240	1350	1440	1530	1620	1710	1790	1870	1940	2020	2090	2160	2230	2300
8 – 14.5 MH	1380	1510	1630	1750	1860	1970	2070	2170	2270	2360	2450	2540	2620	2710	2790
9 – 14.5 MH	1600	1750	1890	2020	2150	2280	2400	2510	2620	2730	2830	2940	2940	3130	3230

Note: For restricted speed highway service not exceeding 50 mph, the above load ratings may be increased 9%.

Important: Always use approved tire and rim combinations for diameter and contours.

- Conventional or cab forward configuration shall have a minimum wheelbase of 120 inches.
- Cab-over engine tow vehicles shall have a minimum wheelbase of 89 inches. Tow vehicles shall have a minimum four speed transmission.
- Power shall be sufficient to meet the requirements listed.
- Brakes must be on all wheels.
- Minimum of four rear tires.
- May use tires exceeding 14" in width (Super Single)
- Must have a hitch assembly and coupling device of a type designed to comply with Federal Motor Carrier Safety Regulations and be compatible with hitch assembly and coupling mechanism of manufactured home.
- A two-way radio is required and must be capable of transmitting and receiving voice messages over a minimum distance of one half mile. Citizens band radios with 4-watt rating are acceptable.
- The operator must be familiar with traffic laws and have sufficient experience and skill to operate a tow vehicle.

- Mirrors on each side of the tow vehicle shall be arranged so that the driver can see along the entire length of both sides of the towed unit.

Trailer Brakes

Administrative rule: R909-2-27(6)(a)(b) states;

(6) Trailer brake requirements:

(a) mobile manufactured homes in excess of 8 feet 6 inches wide, up to 12 feet wide and equipped with one axle, must have operational brakes; and

(b) a minimum of two axles equipped with operative brake assemblies is required on each mobile manufactured home unit in excess of 12 feet wide.

- The above rule would also apply to loads not exceeding 14 feet 6 inches wide.
- All axles must be operational braking axles for trailers in excess of 14'6" wide.
- The brake assembly requirement will not apply when the manufactured home is transported by a truck tractor and semi-trailer lowboy, which must be equipped with brakes.

Movement Requirements

In addition to permit provisions as specified, Mobile/Manufactured homes will observe the following additional requirements:

• Emergency Stops

When a mobile/manufactured home must stop because of emergency conditions, it shall be moved as far right as practicable away from highway traffic.

If any part of the combination is less than 3 feet from the right-hand edge of the nearest traffic lane, reflective triangles as outlined under [49 CFR 393.95\(h\)](#) shall be posted at 100 feet and 300 feet behind the vehicle to warn oncoming traffic.

When an emergency dictates night parking next to the highway, an amber flashing light (minimum diameter 4 inches) shall be placed on the corner of the trailer closest to the road so as to be clearly visible to approaching traffic.

The height of the light shall not be less than 3 feet above the road surface and not more than 8 feet above the road surface.

• Stop and Turn Signals

Rear mounted stop and turn signal lights shall be a minimum 6 inches in diameter with a type 35 red reflector lens.

The lens shall be mounted not more than 18 inches from the outer edge of the unit and not less than 15 inches or more than 8 feet above the road surface.

- **Safety Chains**

Two safety chains shall be used, one each on right and left sides of (but separate from) the coupling mechanism connecting the tow vehicle and the modular home while in transit.

Chains shall be 3/8 inch in diameter steel capable of passing a minimum brake test load of 16,200 pounds. Chains shall be strongly fastened at each end to connect the tow vehicle and manufactured home and assure that in the event of a coupling failure the manufactured home will track behind the tow vehicle. When the mobile/manufactured home is transported on a semi-trailer lowboy coupled to the tow vehicles with a fifth wheel and kingpin assembly the two safety chains are not required.

- **Paneling of the open sides of mobile/manufactured home**

A rigid material or 0.5 millimeter plastic sheathing backed by a rigid grillwork not exceeding squares of 4 feet to prevent billowing must fully enclose the open sides of the units in transit.

- **Adverse Weather Conditions FMCSA 392.14**

Extreme caution in the operation of a commercial motor vehicle shall be exercised when hazardous conditions, such as those caused by snow, ice, sleet, fog, mist, rain, dust, or smoke, adversely affect visibility or traction. Speed shall be reduced when such conditions exist. If conditions become sufficiently dangerous, the operation of the commercial motor vehicle shall be discontinued and shall not be resumed until the commercial motor vehicle can be safely operated. Whenever compliance with the foregoing provisions of this rule increases hazard to passengers, the commercial motor vehicle may be operated to the nearest point at which the safety of passengers is assured.

DISASTER MOVEMENT OF MOBILE/MANUFACTURED HOMES

The Resolution of the Utah Department of Transportation adopting the AASHTO National Policy entitled "Disaster Movement of Mobile Homes" is incorporated herein by reference. This National Policy sets forth criteria governing the movement of mobile/manufactured homes for Utah in Presidential declared disaster areas under the authority of [Public Law 93-288](#), Federal Disaster Relief Act.

Chapter 30

Moving Houses and Buildings

LEGAL DIMENSIONS - PERMIT NOT REQUIRED

Width: 8 Feet 6 Inches	Height: 14 Feet	Length: 65 Feet
------------------------	-----------------	-----------------

LEGAL DIMENSIONS - NO PERMIT REQUIRED

- Width: 8'6"
- Height: 14'
- Length: 45 feet
- Length: 65' combination of unit and tow vehicle

Note: Safety appurtenances such as binder chains, clearance lights, rub rails, and load securing devices, may extend up to 3 inches beyond the prescribed width on either side

PERMIT PROVISIONS

The transporting of a house or building exceeding the legal dimensions must be permitted under the following conditions:

- Houses and buildings up to 16 feet wide must be authorized by any of the Ports of Entry located throughout the State.
- Houses and buildings exceeding 16 feet wide must get UDOT Regional Authorization in addition to Regional Permits issued by the District.

To obtain permits, go to MCD Online System or call (801) 965-4892 with 48 hours notice.

- Houses, buildings, and structures not manufactured or built to be transported, will not require tail, brake, or signal lights mounted on the structures. Certified pilot and police escort vehicles provide sufficient warning of the intent to brake, turn or stop.

Movements of buildings in excess of 22 feet will be considered on a case-by-case basis, this includes movement on or along high volume traffic routes. To obtain authorization for these movements refer to the end of this section.

Note: Existing houses and buildings should be reduced to the smallest reasonable dimensions possible.

Section 6

Pilot/Escort Requirements and Training

Add picture

Chapter 31

Pilot/Escort Requirements

This section will provide necessary information for obtaining certification as a Utah Pilot/Escort with all vehicle requirements to meet the regulations for Pilot/Escort Vehicles.

OPERATING CONDITIONS REQUIRING PILOT/ESCORT VEHICLES

One pilot vehicle is required for vehicles/loads, which exceed the following dimensional conditions:

- 12 feet in width on secondary highways (non-interstate) and 14 feet in width on divided highways (interstates).
- 105 feet in length on secondary highways and 120 feet in length on divided highways.
- Overhangs in excess of 20 feet shall have pilot/escort vehicle positioned to the front for front overhangs and to the rear for rear overhangs.

Two pilot/escort vehicles are required for vehicles/loads which exceed the following dimensional conditions:

- 14 feet in width on secondary highways and 16 feet in width on divided highways, except for
 - Mobile and manufactured homes with eaves 12 inches or less on either roadside or curbside shall be measured for box width only and assigned escort vehicles as specified above in [R912-9-1](#).
 - Mobile and manufactured homes with eaves greater than 12 inches shall be measured for overall width including eaves and pilot/escort vehicles assigned as specified above [R912-9-2](#); or
- 120 feet in length on secondary highways.
- 16 feet in height on all highways. Lead escort will run a height indicator pole. Height pole shall be set 3 inches above the height of the load.
- When otherwise required by the Department.
- As specified for night time travel.

Police escorts are required for vehicles with loads which exceed:

- 17 feet wide or 17 feet 6 inches high on secondary highways; or
- 20 feet wide or 17 feet 6 inches high on all highways; or
- All loads in excess of 175 feet in length MUST have a minimum of one police escort; all loads in excess of 200 feet in length will require a minimum of two police escorts. Exception: Loads that are unladen (decked).
- When required by the Department.

CONVOY ALLOWANCES FOR PERMITTED VEHICLES

The movement of more than one permitted vehicle is allowed provided prior authorization is obtained from the Motor Carrier Division with the following conditions:

- Number of permitted vehicles in the convoy shall not exceed two.
- Loads may not exceed 12 feet wide or 150 feet overall length.
- Distance between vehicles shall not be less the 500 feet or more than 700 feet.
- Distance between convoys shall be a minimum of one mile.
- All convoys shall have a certified pilot/escort in the front and rear with proper signs.
- Police escorts or UDOT personnel may be required.

- Convoys must meet all lighting requirements required in [49 CFR 393.11](#) and in the lighting section for nighttime travel.
- Convoys are restricted to freeway and interstate systems.
- Nighttime travel is encouraged with Motor Carrier Division approval.
- Approval may be obtained by contacting the Motor Carrier Division or any Port of Entry **Note:** The Motor Carrier Division on a case-by-case basis may grant exceptions to the above. For further information please contact (801) 965-4892 or (866) 215-5399.

PRE-TRIP PLANNING AND COORDINATION REQUIREMENTS

- A coordination and planning meeting shall be held prior to load movement. The driver(s) carrying or pulling the oversize load(s), the pilot/escort vehicle driver(s), law enforcement officers (if assigned), department personnel (if involved), and public utilities company representatives (if involved) shall attend. When police escorts are present, a Utah Law Enforcement Check List and Reporting Criteria Form must be completed. This meeting shall include discussion and coordination on the conduct of the move, including at least the following topics:
 - The person designated as being in charge (usually a Department representative or a law enforcement officer).
 - Authorized routing and permit conditions. Ensure that all documentation is distributed to all appropriate individuals involved in the move.
 - Communication and signals coordination.
 - Verification/measurement of load dimensions. Compare with permitted dimensions
 - Copies of permit and routing documents shall be provided to all parties involved with the permitted load movement.

PILOT/ESCORT VEHICLE STANDARDS

- Pilot/escort vehicles may be either a passenger vehicle or a two-axle truck with a 95-inch minimum wheelbase and a maximum gross vehicle weight of 12,000 lbs and properly registered and licensed as required under Sections [UCA 41-1a-201](#) and [UCA 41-1a-401](#).
- Equipment shall not reduce visibility or mobility of pilot/escort vehicle while in operation.
- Trailers may not be towed at any time while in pilot/escort operations.
- Pilot/escort vehicles shall be equipped with a two-way radio capable of transmitting and receiving voice messages over a minimum distance of one-half mile. Radio communications must be compatible with accompanying pilot/escort vehicles, utility company vehicles, permitted vehicle operator and police escort, when necessary.
 - When operating with police escorts a CB radio is required.
- Pilot/escort vehicles may not carry a load.

PILOT/ESCORT VEHICLE SIGNING REQUIREMENTS

- Sign requirements on pilot/escort vehicles are as follows:
 - Pilot escort vehicles must display an "Oversize Load" sign, which must be mounted on the top of the pilot/escort vehicle.
 - Signs must be a minimum of five feet wide by 10-inch high visible surface space, with a solid yellow background and eight inch high by one inch wide black letters. Solid defined as: When being viewed from the front or rear at a 90-degree angle, no light can transmit through.
 - The sign for the front/pilot escort vehicle shall be displayed so as to be clearly legible and readable by oncoming traffic at all times.
 - The rear pilot/escort vehicle shall display its sign so as to be readable by traffic overtaking from the rear and clearly legible at all times.

PILOT/ESCORT VEHICLE LIGHTING REQUIREMENTS

- Two methods of lighting are authorized by the Department. Requirements are as follows:
 - Two AAMVA approved amber flashing lights mounted with one on each side of the required sign. These shall be a minimum of six inches in diameter with a capacity of 60 flashes per minute with warning lights illuminated at all times during operation, or
 - An AAMVA approved amber rotating, oscillating, or flashing beacon/light bar mounted on top of the pilot/escort vehicle. This beacon/light bar must be unobstructed and visible for 360 degrees with warning lights illuminated at all times during operation.
- Incandescent, strobe or diode (LED) lights may be used provided they meet the above criteria

PILOT/ESCORT VEHICLE EQUIPMENT REQUIREMENTS

- Pilot/escort vehicles shall be equipped with the following safety items:
 - Standard 18 inch or 24 inch red/white "STOP" and black/orange "SLOW" paddle signs. Construction zone flagging requires the 24-inch sign. For nighttime travel moves, signs must be reflective in accordance with MUTCD standards.
 - Nine reflective triangles or 18-inch reflective orange traffic cones (not to replace the following two items).
 - Eight red-burning flares, glow sticks or equivalent illumination device approved by the Department.
 - Three orange, 18-inch high cones.
 - Flashlight with a minimum 1 1/2 inch lens diameter, with extra batteries or charger (emergency type shake or crank will not be allowed).
 - 6 inch minimum length red or orange cone for use when directing traffic.
 - Orange hardhat and Class 2 safety vest for personnel involved in pilot/escort operations. Class 3 safety vests are required for nighttime moves
 - Hard hats may not be spray painted.
 - A height-measuring pole made of a nonconductive, non-destructive, flexible or frangible material, only required when escorting a load exceeding 16 feet in height.
 - Fire extinguisher
 - First aid kit must be clearly marked.
 - One spare "oversize load" sign, seven feet by 18 inches.
 - Serviceable spare tire, tire jack and lug wrench.
 - Handheld two way simplex radios or other compatible form of communication for operations outside pilot/escort vehicles.
- Vehicles shall not have unauthorized equipment on the vehicle such as those generally reserved for law enforcement personnel.

Note: Fire extinguisher with a missing gauge cannot be determined if charged.

POLICE ESCORT VEHICLE EQUIPMENT AND SAFETY REQUIREMENTS

- Police escort vehicles shall be equipped with the following safety items:
 - All officers must have a CB radio to communicate with the pilot and transport vehicles.
 - Officers shall complete a Utah Law Enforcement Check List and Reporting Criteria Form.
 - Officers shall verify that all pilot/escorts are in possession of current pilot/escort inspections, or they shall complete an inspection prior to load movement.
 - Police vehicles must be clearly marked with emergency lighting visible 360 degrees.
 - Officers shall be in uniform while conducting police escort moves.

INSURANCE

Driver shall possess a current certificate of insurance or endorsement which indicates that the operator, or the operator's employer, has in full force and effect not less than \$750,000 combined single limit coverage for bodily injury and/or property damage as a result of the operation of the escort vehicle, the escort vehicle operator, or both causing the bodily injury and /or property damage arising out of an act or omission by the pilot/escort vehicle operator of the escort duties required by the Rules. Such insurance or endorsement, as applicable, must be maintained at all times during the term of the pilot/escort certification. Pilot / escort vehicles shall have a minimum amount of \$750,000 liability. This is not a cumulative amount.

FLAGGING REQUIREMENTS

- During the movement of an over-dimensional load/vehicle, the pilot/escort driver, in the performance of the flagging duties required by these rules, may control and direct traffic to stop, slow or proceed in any situation(s) where it is deemed necessary to protect the motoring public from the hazards associated with the movement of the over-dimensional load/vehicle. The pilot/escort driver, acting as a flagger, may aid the over-dimensional load/vehicle in the safe movement along the highway designated on the over-dimensional load permit and shall:
 - (a) Assume the proper flagger position outside the pilot/escort vehicle, and as a minimum standard, have in use the necessary safety equipment as defined in 6E.1 of the MUTCD, and
 - (b) Use stop/slow paddles or a 24" red/orange square flag to indicate emergency situations, and other equipment as described in 6E.1 of the MUTCD; and
 - (c) Comply with the flagging procedures and requirements as set forth in the MUTCD at <http://mutcd.fhwa.dot.gov/> and the Utah Department of Transportation Flagger Training Handbook at <http://www.udot.utah.gov/main/f?p=100:pg:0::::T,v:1385>



State of Utah
Utah Department of Transportation
Motor Carrier Division
4501 South 2700 West, Box 141210
Salt Lake City, Utah 84114-1210

For Additional Information Contact:
Pilot Escort Coordinator
Phone: (801) 965-4892



Pilot / Escort Vehicle Inspection & Checklist

Company Name		Page 1 / 2		
Drivers Name		Driver's License Number	State	Expiration Date
Vehicle License Plate Number		Expiration of License Plate		
Company Address		City	State	Zip
Phone Number	Email Address	Date of Inspection		
Pilot / Escort Inspection Instructions				
<p>1. Items listed in Section One are mandatory for pilot/escort vehicle to operate in Utah. <u>Pilot/escort vehicles and/or operators missing any of these items will be ineligible to operate in Utah until item(s) are corrected.</u></p> <p>2. Items listed in Section Two are <u>required</u> however, if not present at the time of inspection pilot / escort vehicles may be passed with conditions and may be approved for a single trip only.</p> <p>3. Driver <u>must</u> carry driver certification cards and a copy of the most current inspection with them at all times..</p>				
Action Taken		Date		
<input type="checkbox"/>	Passed inspection	Agent Signature		
<input type="checkbox"/>	Passed inspection with conditions. <u>See comments. Authorized to complete current trip only.</u>	Badge No.		
<input type="checkbox"/>	Failed inspection. Ineligible to operate pilot/escort vehicle in Utah until items are corrected.	Port of Entry		
Drivers Signature		Date		
Comments: (include permit #, route, load description, and all violations.)				

Copy 1: Port of Entry File
Copy 2: Driver (Must be kept in vehicle and available for inspection)

MCD Form 003 Rev. 01/2012

Chapter 32

Pilot/Escort Training and Certifications

PILOT/ESCORT DRIVER REQUIREMENTS

Individuals who operate a pilot/escort vehicle must meet the following requirements:

- Must be a minimum of 18 years of age.
- Possess a valid drivers license for the state jurisdiction in which he/she resides.
- The following state certification is currently accepted in Utah: Colorado, Washington, Oklahoma, North Carolina, Florida and Arizona.
- Pilot/escort drivers will be issued a certification card by an authorized Qualified Certification Program as outlined in [R912-10](#), and shall have it in their possession at all times while in pilot/escort operations.
- Initial certification will be valid for four years from the date of issue. One additional four year certification may be obtained through a mail in or on-line re-certification process provided by a Qualified Pilot/Escort Training Entity/Institution.
- Pilot/escort drivers must provide a current (within 30 days) Motor Vehicle Record (MVR) certification to the Qualified Certification Program at the time of the course.
- Current certification for pilot/escort operators will be honored through expiration date. Prior to expiration of pilot/escort certification it will be the responsibility of the operator to attend classroom instruction provided by an authorized Pilot/Escort Qualified Certification Program. For more information contact:

RSA Network 801-838-8999

www.uspilotcar.com

- No passengers under 16 years of age are allowed in pilot/escort vehicles during movement of oversize loads.
- A pilot/escort driver may not perform as a tillerman while performing pilot escort operations.
- A pilot/escort driver must be flagger certified and shall have proof of certification in their possession at all times.

A pilot/escort driver must meet the requirements of [49 CFR 391.11](#) if using a vehicle for escort operations in excess of 10,000 lbs GVWR

DRIVER CERTIFICATION PROCESS

- Drivers domiciled in Utah must complete a pilot/escort certification course authorized by the Department. A list of authorized instructors may be obtained by contacting (801) 965-4892.
- Pilot/escort drivers domiciled outside of Utah may operate as a certified pilot/escort driver with another State's certification credential, provided the course meets the minimum requirements outlined in the Pilot/Escort Training Manual - Best Practices Guidelines as endorsed by the Specialized Carriers and Rigging Association, Federal Highway Administration, and the Commercial Vehicle Safety Alliance; and/or
- The Department may enter into a reciprocal agreement with other states provided they can demonstrate that course materials are comprehensive and meet minimum requirements outlined by the Department. For a current listing of these states, contact the Motor Carrier Division at (801)-965-4892.
- Pilot/escort driver certification expires four years from the date issued. It will be the responsibility of the driver to maintain certification.

SUSPENSIONS AND REVOCATIONS OF PILOT/ESCORT DRIVER CERTIFICATION

Pilot/escort drivers may have their certification denied, suspended, or revoked by the Department if it is determined that a disqualifying offense has occurred within the previous 4 years.

- Drivers convicted of serious traffic violations such as excessive speed, reckless driving and driving maneuvers reserved for emergency vehicles, driving under the influence of alcohol or controlled substances may have their certification suspended or revoked by the Department.
- The Department may suspend for first offenses up to one year. Subsequent offenses may result in permanent revocation of driver certification.

APPEAL PROCESS

When a driver is denied pilot/escort-driving privileges for reasons other than the conditions set forth in this chapter under Suspensions and Revocations of Pilot/Escort Driver Certification, the individual may file an appeal. The appeals shall be handled by a steering committee created by the Division. The steering committee shall have the powers granted to the Deputy Director in [R907-1-3](#) for appeals from other Motor Carrier Division administrative actions. This committee's decision, if adopted by the Director of the Motor Carrier Division, will be considered a final agency order under the Utah Administrative Act.

Section 7

Farmers, Implements of Husbandry and Agricultural Operations

Save space for picture

Chapter 33

Agricultural Operations and Implements of Husbandry

GENERAL

This section applies to those who are involved in Agricultural Operations.

The Federal Motor Carrier Safety Regulations (FMCSRs) apply to Agriculture when they use a commercial motor vehicle.

- **A commercial motor vehicle is a vehicle used on a highway to transport passengers or property when the vehicle:**
 - Has a gross vehicle weight rating (GVWR) or gross combination weight rating (GCWR) of 10,001 pounds or more, or
 - Is designed to transport 16 or more passengers including the driver, or
 - Is of any size and used in the transportation of hazardous materials which require placards as set forth in 49 CFR 171 - 178, and,
 - Is used in commerce.
- **A farmer is a person who cultivates land, or raises poultry, crops or livestock. Only certain agricultural exemptions apply. This section is intended to clarify such exemptions.**

Add Def. of Agricultural Operations

EXEMPTIONS

- **Utah is currently sharing reciprocity agreements for farm-plated vehicles registered in the State of Idaho hauling their own farm equipment or product.**
- **Securement/Tarping requirements do not apply to a vehicle or implement of husbandry carrying an agricultural product, if the agricultural product is:**
 - Being transported in a manner which is not a hazard or a potential hazard to the safe operation of the vehicle or to other highway users, and
 - Loaded in a manner that only allows minimal spillage.
- **Any implement of husbandry only incidentally operated on the highway by a farmer is not subject to license or registration.**

Per UCA 72-9-105(4) commercial vehicles operating Intrastate only, primarily used by a farmer for the production of agricultural product are exempt from the vehicle marking requirements. Needs to be moved this applies to farmers only

OBTAINING OVERSIZE AND/OR OVERWEIGHT PERMITS

Before moving a vehicle or load, which exceeds legal dimensions as outlined in the Legal Vehicle Dimensions a permit must be obtained and carried in the vehicle for inspection by law enforcement personnel.

The appropriate permit for oversize vehicles and loads, including special mobile equipment, must be obtained prior to operating within the State of Utah, except when entering on I-15 (St. George Port of Entry) and West bound I-80 (Echo Port of Entry) and East bound I 80 at (Wendover Port of Entry). The permit may be in paper or electronic format.

Permits may be purchased online at www.udot.utah.gov/public/mcs prior to entry. Permits may be purchased by major credit and card are issued on a single trip, semi-annual or annual basis.

The following conditions must be met to obtain a permit:

- Be in compliance with all financial responsibility obligations as specified under [49 CFR 387, R909-1](#) (Intrastate carriers).
- Vehicle(s) must be properly registered. [UCA 41-1a-101](#)
- Driver(s) are properly licensed with appropriate endorsements according to **Map 21 (in Chapter 34) and applicable FMCSR**
- Must be in compliance with the Federal Motor Carrier Safety Regulations (FMCSRs). [49 CFR Parts 382-399](#)
- Must be in compliance with the Hazardous Material Regulations (HMRs) [49 CFR Parts 171-180](#).
- Must be in compliance with all UCR requirements

TRANSFER / REPLACEMENT OF PERMITS

Permits may be transferred through the Online Permit System or Motor Carrier Division personnel may transfer permits from one vehicle to another for a \$35 fee and documentation providing evidence of ownership.

On-line services at Utahmc.com: **This website will be replaced Utahmc.com is going away**

- Annual and semi-annual permits may be transferred to another unit within the same company.
- Customer has sold or purchased a vehicle.

Changes that require assistance from the Motor Carrier Division @801-965-4892.

- Lease changes from one company to another requires written authorization from the previous company.
- Company reorganization or name change.
- A transfer permit will be issued the same expiration date as the original permit.

The following information must be provided when applying for a permit:

- Motor Carrier information:
 - USDOT number
 - Address
 - Telephone number
 - Fax number (if available)
 - Web address (if available)
 - Email address (if available)
 - Vehicle information
 - License number, state, expiration date

- Vehicle make, year, registered weight
- Unit number
- Complete Vehicle Identification Number (VIN)
- Specific load information
- Permit duration (Single, Semi-Annual, Annual)
- Requested routes
- Dimensions, length, width, height, front and rear overhang.

Double Bale of Agricultural Operations

Vehicle combinations may transport two rolls or bales of hay side by side when:

- Hay trucks. Vehicle combinations for hay truck operations may transport two rolls or bales of hay side by side when:
 - The two rolls or bales are 10 feet or less in combined width;
 - The load is being operated with a valid non-divisible oversize permit;
 - Oversize loads exceeding 8 feet 6 inches may not be transported on double trailers exceeding 61 feet cargo or cargo carrying length;
 - The load must meet all other divisible load requirements
 - Loads are properly secured. **R909-2-30**
 - Double trailers with a maximum length of 61 feet
 - Must meet Divisible axle weight limitation
 - Purchase Over/weight divisible permits where applicable

Chapter 34

Farmers

Implements of Husbandry moved by a farmer:

UCA 72-7-407

Implements of husbandry moved by a farmer, rancher or his employees in connection with an agricultural operation are exempt from permitting requirements provided the following conditions are met:

- Front and rear escort vehicles are required when the implement is 16 feet in width.
- One or more escort vehicles are required when the implement is traveling on a highway where special hazards exist.

Implements of husbandry moved by a farmer, rancher, or his employees in connection with an agricultural operation must comply with:

- every farm tractor and towed farm equipment towed or self propelled implements of husbandry, designed for operation at speeds not in excess of 25 miles per hour, shall at all time be equipped with a slow moving vehicle emblem mounted on the rear: and
- every farm tractor and every self-propelled implement of husbandry manufactured or assembled after January 1970 shall be equipped with vehicular hazard warning lights visible from a distance of not less than 1,000 feet to the front and rear in normal sunlight, which shall be displayed whenever any such vehicle is operated upon a highway.

(Refer to R909-2-30)

Note: Special hazards means extreme weather conditions and/or highway construction or maintenance projects
Oversize Non-Divisible Load Lighting, Signing and Flag Requirements.

Oversize non-divisible load lighting:

- warning lights required when headlights are necessary;
- front overhang in excess of three feet shall be marked with a steady, amber marker light and red flag;
- rear overhang exceeding four feet shall be marked with red clearance lights for night travel;
- vehicles with front or rear overhang exceeding 20 feet from the front or rear bumper of a vehicle, or from the center of the closest axle in the absence of a bumper, a rotating or flashing beacon visible from a minimum of 500 feet, and shall be displayed at a minimum height of four feet above ground;
- tow vehicle headlights shall be operated on low beam, day or night, as an additional warning to traffic; and
- night time travel, when authorized by the division may be permitted with marker lights indicating extreme width using amber lights front and center, and red lights to the rear.

Oversize non-divisible load sign requirements. Non-divisible oversize loads exceeding 10 feet in width, 14 feet in height and 105 feet in length shall display an "OVERSIZE LOAD" sign, to warn the motoring public that extra large vehicles are in operation. Signs must:

- be 7 feet by 18 inches;

- have a yellow background with 10 inch high black letters that are painted with 1 5/8 inches wide stroke to read: "OVERSIZE LOAD";
- be impervious to moisture;
- have front signs mounted on front bumper or on top of vehicle cab with letters presented toward the front of the vehicle;
- have rear signs positioned at the rear most part of the vehicle or load as feasible, ensuring in all cases that the load does not obstruct the view of the sign;
- if possible, have the bottom edge of the sign be positioned not more than 5 feet above the road surface;
- be mounted with adequate supporting anchorage, constructed, maintained, and displayed so that they are clearly legible at all times;
- be covered, removed or placed face down when the vehicle is not engaged in an oversize movement; and
- oversize loads signs are not required on LCVs.

Oversize non-divisible load flag requirements. Red or orange fluorescent flags must be affixed on all extremities when:

- vehicle or load exceeds 10 feet in width;
- loads on a vehicle exceeding three feet to the front or four feet to the rear of the bed or body while in operation;
- flags shall be completely clean and not torn, faded, or worn out and shall be fastened so as to wave freely; and
- over dimensional flagging is not required on LCVs **R909-2-20**.

SIGNING REQUIREMENTS

Every farm tractor and towed farm equipment, towed or self-propelled implements of husbandry, designed for operation at speeds not in excess of 25 miles per hours, shall at all times be equipped with a slow moving vehicle emblem mounted on the rear.

VEHICLES CARRYING LIVESTOCK OR GRAIN

Vehicles may carry up to 36,000 pounds on a tandem axle without obtaining overweight permit if:

- The load is livestock as defined in UCA 4-1-8(6), "Cattle, sheep, goats, swine, horses, mules, poultry, domesticated elk as defined in UCA 4-39-102, or any other domestic animal or domestic furbearer raised or kept for profit."
- The load is grain being transported by a farmer from a farm to market in a raw state prior to bagging, weighing or processing.
- The vehicle is not operated on an Interstate Highway or the Defense Highway System: US 491; US 191 (from US 491 to I-70); US 6, and all roads leading into or out of a military facility.
- The GVW does not exceed 80,000 pounds.
- While the defined legal and permitted weight limits apply to livestock as well as to other carriers, consideration shall be given to the unique nature of living cargo, which can move around changing axle weights. This is especially true where dividers are not built in livestock compartments.
- Every reasonable effort shall be made to minimize the delay of livestock carriers in the ports of entry, so as to avoid harm or injury to the animals.
- If off-loading is required, the animal's safety and that of personnel involved shall be given the highest priority.

AGRICULTURAL EXEMPTIONS / REGULATIONS - MAP 21

Covered Farm Vehicle*:

1. 26,000 Gross vehicle weight rating, gross combination weight rating, gross vehicle weight, gross combination weight or less:
 1. **Not subject to:**
 - i. Part 391 – Physical qualification
 - ii. Part 395 – Hours of service
 - iii. Part 396 – Inspection Repair & Maintenance
2. More than 26,000 Gross vehicle weight rating, gross combination weight rating, gross vehicle weight, gross combination weight; staying within a state or when traveling outside the state is within 150 air-mile radius of the farm or ranch, distribution or retail point.
 1. **Not subject to:**
 - i. Part 383 - CDL
 - ii. Part 382 – Drug/Alcohol Testing
 - iii. Part 391 – Driver qualifications in regards to physical qualifications
 - iv. Part 395 – Hours of Service
 - v. Part 396 – Inspection Repair & Maintenance.

Parts 387, 390, 392 and 393 are still applicable.

***Covered farm vehicle:** includes any commercial motor vehicle (CMV) (defined in 49 CFR 390.5, but excluding vehicles transporting hazardous materials in a quantity requiring placards), traveling in the State in which the vehicle is registered or another State, is not “for-hire” and:

(a) Is operated by—

(i) a farm owner or operator, or an employee or family member of the farm owner or operator; or

(ii) a ranch owner or operator; or an employee or family member of the ranch owner or operator.

(b) Is being used to transport to or from a farm or ranch—

(i) agricultural commodities;

(ii) livestock; or

(iii) machinery or supplies;

(c) Is equipped with a special license plate or other designation by the State in which the vehicle is registered to allow for identification of the vehicle as a farm vehicle by law enforcement personnel

Transportation Of Agricultural Commodities with A Non-Covered Farm Vehicle.

1. *Not subject to the following regulations when operating within 150 air-mile radius:*
 - vi. Part 382 – Drug/Alcohol Testing
 - vii. Part 383 – Commercial Driver’s License
 - viii. Part 391 – Driver Qualifications
 1. Non-articulated are exempt if within 150 air-mile radius
 2. Articulated are excepted from all but medical qualifications if within 150 air-mile radius
 - ix. Part 395 – Hours of Service

CDL STANDARDS/REQUIREMENTS

Covered Farm Vehicle*:

3. 26,000 Gross vehicle weight rating, gross combination weight rating, gross vehicle weight, gross combination weight or less:
 1. Not subject to:
 - i. Part 391 – Physical qualification
 - ii. Part 395 – Hours of service
 - iii. Part 396 – Inspection Repair & Maintenance
4. More than 26,000 Gross vehicle weight rating, gross combination weight rating, gross vehicle weight, gross combination weight; staying within a state or when traveling outside the state is within 150 air-mile radius of the farm or ranch, distribution or retail point.
 1. Not subject to:
 - i. Part 383 - CDL
 - ii. Part 382 – Drug/Alcohol Testing
 - iii. Part 391 – Driver qualifications in regards to physical qualifications
 - iv. Part 395 – Hours of Service
 - v. Part 396 – Inspection Repair & Maintenance.

Parts 387, 390, 392 and 393 are still applicable.

*Covered farm vehicle: includes any commercial motor vehicle (CMV) (defined in 49 CFR 390.5, but excluding vehicles transporting hazardous materials in a quantity requiring placards), traveling in the State in which the vehicle is registered or another State, is not “for-hire” and:

(a) Is operated by—

(i) a farm owner or operator, or an employee or family member of the farm owner or operator; or

(ii) a ranch owner or operator; or an employee or family member of the ranch owner or operator.

(b) Is being used to transport to or from a farm or ranch—

(i) agricultural commodities;

(ii) livestock; or

(iii) machinery or supplies;

(c) Is equipped with a special license plate or other designation by the State in which the vehicle is registered to allow for identification of the vehicle as a farm vehicle by law enforcement personnel

Transportation of Agricultural Commodities with a Non-Covered Farm Vehicle.

2. Not subject to the following regulations when operating within 150 air-mile radius:
 - vi. Part 382 – Drug/Alcohol Testing
 - vii. Part 383 – Commercial Driver’s License
 - viii. Part 391 – Driver Qualifications
 1. Non-articulated are exempt if within 150 air-mile radius
 2. Articulated are excepted from all but medical qualifications if within 150 air-mile radius
 - ix. Part 395 – Hours of Service

Question Beginning - October 1, 2012	HOS Exemption MAP-21 Section 32101 Inter/Intrastate	Covered Farm Vehicle MAP-21 Section 32934 Inter/Intrastate
Who can use the exemption	Anyone	Farmer, their employees or family.
What commodities are covered	Agricultural commodities, farm supplies, livestock, machinery	Agricultural commodities, farm supplies, livestock, machinery
Can they transport placarded HM	Yes	No
Are special plates or other identification required for vehicles	No	Yes
Does the size of vehicle affect exemption	No	Yes <26,001 – Exempt anywhere in US. >26,001 – Exempt anywhere in state of registration OR within 150 air-mile radius of farm or ranch
Are crop share arrangements included in exemption	Yes	Yes
Does the exemption include for-hire transportation	Yes	No
What Parts of the regulations are exempt	395 - Hours of Service	382 - Drug and Alcohol testing, 383 - CDL, 391 - (Subpart E) - Physical Qualifications & Exam. This includes articulated 395 - Hours of Service 396 – Inspection, Repair & Maintenance
Do mileage limits apply to the exemption	Yes 150 air-mile radius from the distribution point; wholesale or retail.	Depends on weight of vehicle <26,001 – Exempt anywhere in US., no mileage restriction >26,001 – Exempt anywhere in state of registration OR within 150 air-mile radius of farm or ranch with respect to which the vehicle is being operated.
Does Annual Inspections apply to the exemption	Yes	No
Does CVSA inspections apply	Yes	No
Does pre/post trip inspections apply	Yes	No

§ 173.5Agricultural operations.(a) For other than a Class 2 material, the transportation of an agricultural product over local roads between fields of the same farm is excepted from the requirements of this subchapter. A Class 2 material transported over local roads between fields of the same farm is excepted from subparts G and H of part 172 of this subchapter. In either instance, transportation of the hazardous material is subject to the following conditions:(1) It is transported by a farmer who is an intrastate private motor carrier; and(2) The movement of the agricultural product conforms to requirements of the State in which it is transported and is specifically authorized by a State statute or regulation in effect before October 1, 1998.(b) The transportation of an agricultural product to or from a farm, within 150 miles of the farm, is excepted from the requirements in subparts G and H of part 172 of this subchapter and from the specific packaging requirements of this subchapter when:(1) It is transported by a farmer who is an intrastate private motor carrier;(2) The total amount of agricultural product being transported on a single motor vehicle does not exceed:(i) 7,300 kg (16,094 lbs.) of ammonium nitrate fertilizer properly classed as Division 5.1, PG III, in a bulk packaging, or(ii) 1900 L (502 gallons) for liquids or gases, or 2,300 kg (5,070 lbs.) for solids, of any other agricultural product;(3) The movement and packaging of the agricultural product conform to the requirements of the State in which it is transported and are specifically authorized by a State statute or regulation in effect before October 1, 1998; and(4) Each person having any responsibility for transporting the agricultural product or preparing the agricultural product for shipment has been instructed in the applicable requirements of this subchapter.(c) Formulated liquid agricultural products in specification packagings of 220 L (58 gallons) capacity, or less, with closures manifolded to a closed mixing system and equipped with positive dry disconnect devices may be transported by a private motor carrier between a final distribution point and an ultimate point of application or for loading aboard an airplane for aerial application. (d) Moveable fuel storage tenders. A non-DOT specification cargo tank motor vehicle may be used to transport Liquefied petroleum gas, UN1075, including Propane, UN1978, as moveable fuel storage tender used exclusively for agricultural purposes when operated by a private carrier under the following conditions: (1) The cargo tank must have a minimum design pressure of 250 psig. (2) The cargo tank must meet the requirements of the HMR in effect at the time of its manufacture and must be marked accordingly. For questions regarding these requirements, contact PHMSA by either: (i) Telephone (800) 467-4922 or (202) 366-4488 (local); or (ii) By electronic mail (e-mail) to: infocntr@dot.gov. (3) The cargo tank must have a water capacity of 1,200 gallons or less. (4) The cargo tank must conform to applicable requirements in National Fire Protection Association (NFPA) 58, Liquefied Petroleum Gas Code (IBR, see § 171.7 of this subchapter). (5) The cargo tank must be securely mounted on a motor vehicle. (6) The cargo tank must be filled in accordance with § 173.315(b) for liquefied petroleum gas. (7) The cargo tank must be painted white, aluminum, or other light-reflecting color. (8) Transportation of the filled moveable fuel storage tender is limited to movements over local roads between fields using the shortest practical distance. (9) Transportation of the

moveable fuel storage tender between its point of use and a liquefied petroleum gas distribution facility is authorized only if the cargo tank contains no more than five percent of its water capacity. A movable fuel storage tender may only be filled at the consumer's premises or point of use. (e) Liquid soil pesticide fumigants. MC 306 and DOT 406 cargo tank motor vehicles and DOT 57 portable tanks may be used to transport liquid soil pesticide fumigants, Pesticides, liquid, toxic, flammable, n.o.s., flash point not less than 23 degrees C, 6.1, UN2903, PG II, exclusively for agricultural operations by a private motor carrier between a bulk loading facility and a farm (including between farms). However, transportation is not to exceed 150 miles between the loading facility and the farm, and not more than five days are permitted for intermediate stops for temporary storage. Additionally, transport is permitted only under the following conditions: (1) Cargo tanks. MC 306 and DOT 406 cargo tank motor vehicles must: (i) Meet qualification and maintenance requirements (including periodic testing and inspection) in accordance with Subpart E of Part 180 of this subchapter; (ii) Conform to the pressure relief system requirements specified in § 173.243(b)(1); (iii) For MC 306 cargo tanks, be equipped with stop-valves capable of being remotely closed by manual and mechanical means; and (iv) For DOT 406 cargo tanks, conform to the bottom outlet requirements specified in § 173.243(b)(2). (2) Portable tanks. DOT 57 portable tanks must— (i) Be constructed of stainless steel; and (ii) Meet qualification and maintenance requirements of Subpart G of Part 180 of this subchapter. (f) See § 173.315(m) pertaining to nurse tanks of anhydrous ammonia. (g) See § 173.6 pertaining to materials of trade. (h) See § 172.800(b) pertaining to security plans. [Amdt. 173-259, 62 FR 1215, Jan. 8, 1997, as amended by Amdt. 173-262, 62 FR 49566, Sept. 22, 1997; Amdt. 173-259, 63 FR 8142, Feb. 18, 1998; 65 FR 50460, Aug. 18, 2000; 70 FR 73165, Dec. 9, 2005; 73 FR 4717, Jan. 28, 2008; 76 FR 5491, Feb. 1, 2011]

SPECIAL FUELS

- **Diesel** - Dyed diesel fuel purchases for off-road uses are exempt from highway use tax. Tax paid for undyed diesel fuel for off-road agricultural purposes is refundable. No use tax is charged at the point of purchase for dyed diesel fuel and no permit is required. Refunds for taxes paid on purchased of undyed diesel fuel are claimed as a credit on the Utah income tax return. Farmers and ranchers should keep accurate records to qualify for such refunds.
- **Gasoline** - Tax paid by farmers and ranchers for gasoline used for off-highway agricultural use is refundable. Refunds are claimed as a credit on the Utah income tax return. For additional information contact: Utah State Tax Commission
Motor Carrier Services
210 North 1950 West
Salt Lake City, Utah 84114
Phone: (801) 297-6800

Note: For fuel tax auditing purposes, at the time of fueling, record vehicle mileage, the number of gallons, cost per gallon, and date of purchase.

INSPECTION, REPAIR AND MAINTENANCE

Farmers with commercial motor vehicles must have an inspection repair and maintenance program as stated in [49 CFR Part 396](#).

INSURANCE AND FINANCIAL RESPONSIBILITY

Farmers must have the minimum level of insurance coverage (\$750,000) as stated in [49 CFR Part 387](#).

- Implements of husbandry incidentally operated on the highway by a farmer are not subject to licensing or registration.

FARM VEHICLE MARKING REQUIREMENTS INTERSTATE OPERATIONS:

[49 CFR 390.21](#)

Farmers operating as a commercial motor vehicle must mark their vehicles as follows:

- Markings must display:
 - The legal name or a single trade name of the motor carrier.
 - The motor carrier identification number issued by the FMCSA, preceded by the letters "USDOT."
 - If the name of any person other than the operating carrier appears on the CMV, the name of the operating carrier must be followed by the information required above and preceded by the words "operated by."
- Markings must:
 - Appear on both sides of the vehicle;
 - Be in letters that contrast sharply in color;
 - Be readily legible, during daylight, from a distance of 50 feet; and
 - Be maintained to ensure readability.

INTRASTATE OPERATIONS:

[UCA 72-9-105](#)

Farmers operating as a commercial motor vehicle must mark their vehicles as follows:

- Markings must display:
 - The name of the motor carrier company; and
 - The location of domicile by city and state.
 - USDOT number
- Markings must:
 - Lettering shall be free from obstruction and legible from a distance of at least 50 feet.

Exception: A commercial vehicle primarily used by a farmer for the production of agricultural product is exempt from the provisions of this section. [UCA 72-9-105\(4\)](#)

HOURS OF SERVICE

Short Haul Operations - 100 Air-Mile Radius Driver [49 CFR Part 395.1\(e\)](#)

When a driver operates and stays within a 100 air-mile radius of their normal work reporting locations, a logbook does not have to be maintained if the following requirements are met:

- The driver operates within a 100 air-mile radius of the normal work reporting location;
- The driver, except a driver-salesperson, returns to the work reporting location and is released from work within 12 consecutive hours. (A driver-salesperson means any employee who is employed solely as such by a private carrier of property by commercial motor vehicle, who is engaged both in selling goods, services or the use of goods, and in delivering by commercial motor vehicle the goods sold or provided or upon which the services are performed, who does so entirely within a radius of 100 miles

of the point at which he/she reports for duty, who devotes not more than 50 percent of his/her hours on duty to driving time. The term selling goods for purposes of this section shall include in all cases solicitation or obtaining of reorders or new accounts, and may also include other selling or merchandising activities designed to retain the customer or to increase the sale of goods or services, in addition to solicitation or obtaining of reorders or new accounts. - Part 395.2 Definitions)

- A property-carrying commercial motor vehicle driver has at least 10 hours off duty separating each 12 hours on duty.
- A passenger-carrying commercial motor vehicle driver has at least eight consecutive hours off duty separating each 12 hours on duty.
- A property-carrying commercial motor vehicle driver does not exceed 11 hours maximum driving time following 10 consecutive hours off duty; or
- A passenger-carrying commercial motor vehicle driver does not exceed 10 hours maximum driving time following eight consecutive hours off duty; and
- The carrier that employs the driver maintains and retains for a period of six months accurate and true time records showing:
 - The time the driver reports for duty each day;
 - The total number of hours the driver is on duty each day;
 - The time the driver is released from duty each day; and
 - The total time for the preceding seven days in accordance with [49 CFR 395.8\(j\)\(2\)](#) for drivers used for the first time or intermittently.

NON-CDL - 150 AIR-MILE RADIUS DRIVER

[49 CFR 395.1\(e\)\(2\)](#)

When a driver operates and stays within a 150 air-mile radius of their normal work reporting locations, a logbook does not have to be maintained if the following requirements are met:

- The driver operates a property-carrying commercial motor vehicle for which a commercial driver's license is not required under [49 CFR 383](#);
- The driver operates within a 150 air-mile radius of the location where the driver reports to and is released from work, i.e., the normal work reporting location;
- The driver returns to the normal work reporting location at the end of each duty tour;
- The driver has at least 10 consecutive hours off duty separating each on-duty period
- The driver does not drive more than 11 hours following at least 10 consecutive hours off duty;
- The driver does not drive:
 - After the 14th hour after coming on duty on five days of any period of seven consecutive days; and
 - After the 16th hour after coming on duty on two days of any period of seven consecutive days;
- The driver does not drive:
 - After having been on duty for 60 hours in seven consecutive days if the employing motor carrier does not operate commercial motor vehicles every day of the week;
 - After having been on duty for 70 hours in eight consecutive days if the employing motor carrier operates commercial motor vehicles every day of the week;
- Any period of seven or eight consecutive days may end with the beginning of any off- duty period of 34 or more consecutive hours
- The motor carrier that employs the driver maintains and retains for a period of six months accurate and true time records showing:
 - The time the driver reports for duty each day;
 - The total number of hours the driver is on duty each day;
 - The time the driver is released from duty each day; and
 - The total time for the preceding 7 days in accordance with [49 CFR 395.8\(j\)\(2\)](#) for drivers used for the first time or intermittently.

LIGHTING

Every farm tractor and every self-propelled implement of husbandry manufactured or assembled after January 1970 shall be equipped with vehicular hazard warning lights visible from a distance of not less than 1,000 feet to the front and rear in normal sunlight, which shall be displayed whenever any such vehicle is operated upon a highway.

Exception: *An exception applies to a farmer only operating motor vehicles with a GVWR of 10,000 pounds or less.*

Note: This exception does not apply to vehicles with any quantity of hazardous materials 1.1, 1.2, 1.3, 2.3 (Zone A), or 6.1 (PG I, Zone A) and when placards are required.

LOAD SECUREMENT AND TARPING REQUIREMENTS

Does NOT apply to a vehicle carrying an agricultural product, if the agricultural product is:

- Being transported in a manner which is not a hazard or potential hazard to the safe operation of the vehicle or to other highway users; and
- Loaded in a manner that only allows minimal spillage.

For additional information see the Load Securement and Tarping Requirement in Chapter 17.

Section 8

Special Equipment

Save space for picture

Chapter 35

Special Mobile Equipment

SPECIAL MOBILE EQUIPMENT (SME)

Special Mobile Equipment refers to:

- Vehicles not designed or used primarily for the transportation of persons or property.
- Not designed to operate in traffic; and
- Only incidentally operated or moved over the highways.
 - "Incidental" means transportation that occurs occasionally or by chance, but does not exceed a distance of 20 miles. (Refer to R909-2-3 Definitions)
- Special Mobile Equipment includes:
 - Farm tractors
 - Off-road motorized construction or maintenance equipment including but not limited to backhoes, bulldozers, compactors, graders, loaders, road rollers, tractors, trenchers and ditch-digging apparatus
- An affidavit of "Special Mobile Equipment" must be obtained through the Motor Carrier Division and must be carried at all times in the authorized vehicle, otherwise the vehicle must be properly registered as an SME in its base state and documentation must be made available upon request. You can get and print the affidavit by clicking [here](#). **CORRECT to open affidavit for SME**
- SMEs shall be routed by the Motor Carrier Division.

SINGLE TRIP PERMIT

SMEs may be issued a single trip permit to operate under their own power, on approved routes other than interstate systems, as follows:

- Oversize and pilot/escort restrictions apply when in excess of legal dimensions. Pilot/escort vehicles are required as specified in Chapter 31- Pilot / Escort Requirements
- Single axles shall not exceed 40,000 pounds.
- When the weight of the vehicle exceeds 125,000 pounds GVW only a single trip may be issued. If axle weights exceed the limits in Chapter 20 Legal Weight limitations, Axle and Tire Loads obtain written approval before applying for a permit.

Note: For rates see Chapter 25 Oversize Non-Divisible Load Provisions

Note: For additional information regarding pilot/escort requirements refer to Chapter 31 Pilot/Escort Vehicle Requirements.

COMBINATION PERMIT

If an SME vehicle exceeds legal size, a combination oversize/overweight permit, listing the dimensions shall be required.



Motor Carrier Division
 4501 South 2700 West
 Box 148240
 Salt Lake City, Utah 84114
 801-965-4892

AFFIDAVIT OF SPECIAL MOBILE EQUIPMENT

Company Name:		
Street Address:		
City:	State:	Zip:
VIN:	Year:	Make:

Company Representative Signature:	Date:	Phone Number:
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Special Mobile Equipment Exempt From Registration Requirements	
<input type="checkbox"/> Backhoe	<input type="checkbox"/> Bulldozer
<input type="checkbox"/> Compactor	<input type="checkbox"/> Grader
<input type="checkbox"/> Loader	<input type="checkbox"/> Road Roller
<input type="checkbox"/> Scraper	<input type="checkbox"/> Farm Tractor
<input type="checkbox"/> Trencher	<input type="checkbox"/> Ditch Digging Apparatus
<input type="checkbox"/> Vehicles not designed to be moved or operated over the highway (specify) _____	
NOTE: Affidavit must be carried in the vehicle at all times.	

Approved: ☐ Yes ☐ No Authorized by: _____

Reason Denied: _____

Note: All registration and licensing requirements must be satisfied if vehicle does not qualify as Special Mobile Equipment. If vehicle does not qualify as Special Mobile Equipment, contact the Utah State Tax Commission, Motor Vehicle Department (801) 297-6800.

Chapter 36

Special Truck Equipment

Vehicles not classified as Special Mobile Equipment are eligible for a 50% registration fee reduction as required under [UCA 41-1a-1206](#). The following vehicle configurations are considered Special Truck Equipment:

- Concrete pumper trucks
- Cranes/trucks performing crane service with a crane lift capacity of five tons or more
- to be included but not limited to Well boring trucks

(Refer to R909-2-35)

SPECIAL TRUCK EQUIPMENT PERMIT PROVISIONS

An overweight/oversize permit shall be issued for moving Special Truck Equipment exceeding legal dimensions. Please refer to the Maximum Legal Size Vehicle Dimensions Chapter 19, Oversize Non-Divisible Size Permit Provisions Chapter 25, and Overweight Non-Divisible Permit Provisions Sections Chapter 26.

The STE must be registered for 78,001 to 80,000 pounds or the total gross weight of the vehicle to obtain overweight permits for increased axle weight or when exceeding 80,000 pounds gross.

A profile sheet can be obtained from any of the Ports of Entry.

- A Motor Carrier Agent or the carrier can fill out Profile sheet
- Profile sheet must be signed by a carrier official
- All information on form must be completed
- Contact Super load Team for approval at 801-964-4588
- An approved profile sheet shall be carried in the vehicle at all times

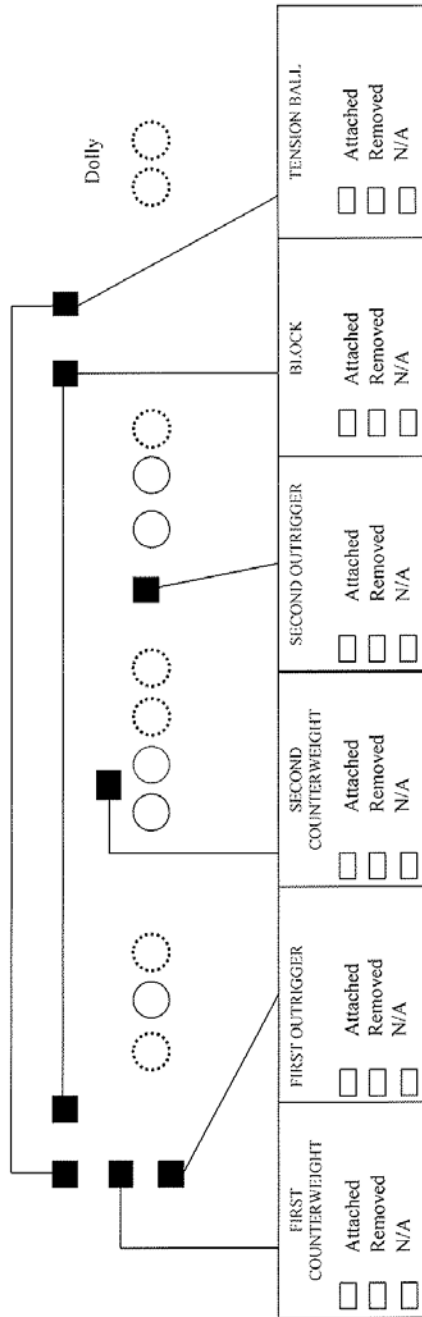
SEMI-ANNUAL / ANNUAL PERMIT

When legal limits are exceeded, a single, semi- annual or annual permit may be issued for gross weights up to 125,000 pounds. The authorized axle limitations will be specified on the permit.

Note: For permit fees see Chapter 27 Overweight Non-Divisible load Provisions.

Profile Sheet Must Be Carried In Vehicle and Permit Weights Must Match

Axle width is hub to hub not outside of tire to outside of tire



Company Name:	USDOT Number:	Today's Date:	
Company Address:	Unit Number & License Plate	Motor Carrier Specialist:	
Company Contact:	Year & Make	Permit Type:	Trip Permit Annual Permit:
Company Phone Number:	VIN:	Approved:	Y: N:

CHAPTER 37

Snow Plows, Parade Floats, Utility Poles and Recreational Vehicles

SNOW PLOW OPERATIONS

UCA 72-7-401

Blades in excess of 8 feet 6 inches must be equipped with a yellow, rotating beacon warning light.

Snow plows with up to 12 feet wide blades may operate without oversize permits, when they are in compliance with:

- lights which provide adequate illumination when the blade is in either the up, or down position;
- signaling lights shall not be obscured; and
- blades must be angled so that the minimum width is exposed to oncoming traffic during periods of travel between jobs

(Refer to R909-2-31)

PARADE FLOATS

Parade floats are not required to obtain an overweight/oversize permit. Rules for the movement of parade floats are as follows:

- all floats must have sufficient proof of insurance;
- all floats must carry the necessary safety equipment for the safe operation of the vehicle during movement;
- the float driver must have a clear 360 degree visibility;
- movement to and from parades should be made only during daylight hours unless the vehicle is adequately lighted and there is minimal congestion; and
- floats in excess of 14 feet in height, must be routed by the division.

(Refer to R909-2-32)

ADVISORY

- A follow-up vehicle is recommended for float moves to and from parade routes.
- Two-way communication is recommended between the float and the follow-up vehicle.

TRANSPORTATION OF UTILITY POLES

Vehicles measuring up to 120 feet in overall length, including overhangs, with single trip, semi-annual or annual permits, that are hauling utility poles shall comply with:

- oversize load restrictions;
- pilot escort requirements;
- travel restrictions; and
- signing and lighting requirements.

Permits are issued to the trailer transporting the poles using the trailer registration information.

- Upon company request, the permit may be issued to the truck or truck tractor.
- Utility poles exceeding 120 feet shall purchase a single trip, non-divisible oversize permit.

(Refer to R909-2-33)

TRANSPORTATION OF RECREATIONAL VEHICLES



- Recreational vehicles that are driven solely as family or personal conveyances for noncommercial purposes
- Qualified motor vehicle does not include recreational vehicles
- Recreational vehicles are exempt from IFTA fuel tax licensing and reporting.
- Exempt from registration
- Recreational Vehicles are legal at 65' Measured from bumper to bumper



Section 9

Tow Truck

Space for picture from Trucking Guide or get one

Chapter 38

Tow Truck Requirements

Tow Truck Operation Certifications

Any company who operates a vehicle as a tow truck as defined in 72-9-102(5) is required to obtain three certifications:

- Driver certification.
- Tow truck (vehicle) certification
- Company certification.

State law 72-9-602 requires that all three certifications must be obtained prior to operation and that these certifications are to be maintained.

Exception: This does not apply to a person who is towing a vehicle owned by that person in a noncommercial operation i.e. using a tow strap or other means to tow their own vehicle to home shop etc.

Note: The tow truck and tow truck motor carrier certifications are only valid for two years from date of issuance. Driver certifications are valid for the time specified by the certifying entity.

Tow Truck Driver Certification

Tow truck Motor Carriers are required to ensure that the driver of a tow truck is properly:

- Obtain and maintain a certification from one of the accepted certification courses
- Has a current valid license for the type of vehicle operated as per Title 53, Chapter 3, Uniform Drivers License Act
- Complies with the requirements under Sections 41-6a-1406 and 72-9-603
- Comply with the Parts 380 – 396 of the Federal Motor Carrier Safety Regulations

The Department of Transportation accepts the tow truck driver certification from the following entities:

- Towing and Recovery Association of America (TRAA) Testing Program - <http://www.towserver.net/>
- Wreckmaster Certification Program - <http://www.wreckmaster.com/>
- AAA Certification Program - <http://www.aaa.com/>
- Utah Safety Council - <http://www.utahsafetycouncil.org/>

R909-19 Safety Regulations for Tow Truck Operations - Tow Truck Requirements for Equipment, Operation and Certification:

<http://www.rules.utah.gov/publicat/code/r909/r909-019.htm> The Utah Tow Truck Rules were updated in February 2012.

Tow Truck Certification Requirements

Inspections for tow truck certification may be obtained at one of the county or region inspection sites or by going to a Port of Entry. Click on the following link for a schedule of inspection sites and dates:

http://www.utahmc.com/udot_tow.php . **This website will be replaced Utahmc.com is going away**

Just click on the name of the site and complete the information. These inspection dates and locations are set up to assist companies in keeping their vehicle certifications current. If there is no scheduled appointment's for on a particular date and location the regional inspection will be canceled.

A tow truck will be certified when all of the required safety equipment, vehicle maintenance, data plates for bed, boom and wheel lift is on the vehicle, and functioning properly.

REQUIRED SAFETY EQUIPMENT

- A. **Amber rotating/oscillating lights** - used to alert public of emergency situations; permanent fixtures mounted on vehicle at the highest point; at least one 6" diameter light.
- B. **Horn** - Used as an audible warning device.
- C. **Portable lights** - turn signals, brake lights, and emergency hazards when the rear lights on the tow vehicle are blocked. May also be wireless.
 - o Only required if tow vehicle is equipped with a wheel lift.
- D. **Fire extinguisher (min. 10BC)** - used to put out small fires; must be readily available, secured, and charged.
- E. **Tie down chains or straps (min. 4)** - used to ensure load remains secured.
- F. **Door and steering wheel ties** - used to ensure doors remain closed while towing; also locks steering (and tires) in place.
- G. **Cones or triangles (min. 3)** - used for traffic control; required as emergency equipment on commercial motor vehicles.
 - o Not required on tow vehicles used exclusively for repossessing of vehicles
- H. **Scoop shovel** - used to clean up glass, etc. from roadway.
 - o Not required on tow vehicles used exclusively for repossessing of vehicles
- I. **Broom** - used to clean up glass, etc. from roadway.
 - o Not required on tow vehicles used exclusively for repossessing of vehicles
- J. **Safety chains (min. grade 7)** - used to lock towed vehicle to boom or bed; prevent excessive swaying and possible breaking away from boom or bed.
- K. **Oil absorbent** - used to assist in absorbing spilled diesel fuel, anti-freeze, battery acid, and etc. from roadway.
 - o Not required on tow vehicles used exclusively for repossessing of vehicles
- L. **Debris container (5 gal)** - used to contain glass, etc. swept from roadway.
 - o Not required on tow vehicles used exclusively for repossessing of vehicles
- M. **Chock blocks (2) or stiff legs** - used to stabilize the recover vehicle when on ice or other slippery surfaces during recovery operations.
- N. **Safety vest (class 2)** - used to highlight the tow truck driver to the public.

VEHICLE MAINTENANCE

- **Front of truck** - check head lamps, turn signals, four-way flashers, marker and ID lights; proper lamp color, mounting, and visibility; and wipers.
- **Side of truck** - check wheels, rims, and tires.
- **Rear of truck** - check tail lamps, stop lamps, turn signals, and 4-way flashers.
- **Cargo area** - ensure all equipment and other cargo is properly secured.
- **Annual State Safety Inspection**

VEHICLE LOAD CAPACITY

- **Truck data plate** - must be attached to vehicle (or official document available in vehicle); must state manufacture's Gross Vehicle Weight Rating (GVWR).
- **Boom or Bed data plate** - must be attached to boom or bed (or official document available in vehicle); must state boom's (or bed's) maximum load capacity.
- **Wheel Lift data plate** - must be attached to wheel lift, boom, or bed (or official document available in vehicle); must state wheel lift's maximum load capacity.

Note: Some vehicles may only have one data plate that gives the information on the bed, boom wheel lift instead of having individual plates on each area.

Upon completion of the inspection of the vehicle and the vehicle passed the inspection a sticker will be placed in the rear window of the vehicle and a copy of the vehicle inspection report will be given to the Tow Truck company.

Tow Truck Motor Carrier Certification

For a Motor Carrier to become certified to operate as a tow truck motor carrier, they must ensure all the drivers have and maintain a current certification, all the vehicles used to tow are currently certified and these certifications are maintained. A tow truck motor carrier must submit an online application listing all the drivers and vehicles used to tow along with certifying that they are in compliance with the following:

- Tow receipts have all the required information as per R909-19
- All fee's including storage, towing, administrative and fuel surcharge fees are posted in an obvious location as per R909-19
- The carrier has in force the proper amount of insurance liability
 - \$750,000 liability for carriers performing police generated tows only.
 - \$1,000,000 if performing all types of tows.
- Along with the insurance level mentioned above the motor carrier must have an MCS-90 environmental restoration on file at their principal place of business.
- Motor Carrier must have updated their MCS-150 form every two years per FMCSA Part 390.19.
- All vehicles are marked with the proper company name as per FMCSA Part 390.21.
- The Unified Carrier Registration fee is paid for all years a carrier is registered with FMCSA as interstate.
- Comply with all the requirements in R909-19, 41-6a-1406 and 72-9-603.

The application will automatically be submitted electronically by clicking on the “create printable pdf” button.

After the application is received a Motor Carrier Specialist will contact the tow truck motor carrier and schedule an appointment to conduct a compliance review as per 72-9-602. These compliance reviews must be done at least once every two years.

For Further Information Contact the Tow Truck Team at 801-965-4892.

Section 10

Compliance Safety and Accountability

Save space for picture

Chapter 39

CSA (Compliance, Safety, Accountability)

What is CSA?

“Compliance, Safety, Accountability.” CSA is a new FMCSA safety program to improve large truck and bus safety and ultimately prevent crashes, injuries, and fatalities related to commercial motor vehicles. It introduces a new enforcement and compliance model that allows FMCSA and its State Partners to contact more carriers earlier in order to address safety deficiencies before crashes occur. The program establishes a new nationwide system for making the roads safer for motor carriers and the public alike.

CSA monitors on-road safety performance data on a monthly basis using roadside performance data available in FMCSA's Motor Carrier Management Information System (MCMIS) collected during the previous 24 months. This data is then used in a Safety Measurement System (SMS) that gives a more comprehensive profile of carriers and drivers, better pinpoints the source(s) of safety problems, and more effectively identifies high crash-risk behavior in seven BASICs.

What are BASIC's?

The new Compliance Safety Monitoring System (CSMS) uses all safety-based inspection violations which are given a risk based violation weight and groups this data into seven Behavioral Analysis Safety Improvement Categories (BASICs):

1. Unsafe Driving
2. Fatigued Driving (Hours-of-Service)
3. Driver Fitness
4. Controlled Substances and Alcohol
5. Vehicle Maintenance
6. Hazardous Material
7. Crash History

CSMS produces a percentile ranking for each BASIC from 0 to 100 for motor carriers having sufficient roadside inspection or crash events to be evaluated. Higher percentile rankings reflect poor performance relative to other motor carriers with similar levels of exposure or inspection activity. For example, a Vehicle BASIC of 80 indicates that approximately 80% of the motor carrier population with a similar number of inspections had better vehicle inspection results.

Each BASIC's overall status will display a ⚠️ symbol if either the on-road performance's percentile is over the established threshold or the investigation results show the discovery of a Serious Violation. This indicates that the BASIC is ⚠️ and the motor carrier may be prioritized for an intervention, which can include a warning letter, investigation, and identification for a roadside inspection.

What is an intervention?

There are three categories of interventions: early contact, investigation, and follow-on.

Early Contact

- [Warning Letter](#)— Correspondence sent to a carrier's place of business that specifically identifies a deficient Behavior Analysis and Safety Improvement Category (BASIC) and outlines possible consequences of continued safety problems. The warning letter provides instructions for accessing carrier safety data and measurement as well as a point of contact.

Investigation

- **Offsite Investigation** – A carrier is required to submit documents to FMCSA or a State Partner. These documents are used to evaluate the safety problems identified through the SMS and to determine their root causes. Types of documents requested may include third party documents such as toll receipts, border crossing records, or drug testing records. The goal is to identify issues responsible for poor safety performance. If the carrier does not submit requested documents they may be subject to an Onsite Investigation or to subpoena records (see below).
- **Onsite Focused Investigation** – The purpose of this intervention is to evaluate the safety problems identified through the SMS and their root causes. An Onsite Focused Investigation may be selected when exceeding the threshold in two or fewer BASICs. Onsite Focused Investigations target specific problem areas (for example, maintenance records), while Onsite Comprehensive Investigations address all aspects of the carrier's operation.
- **Onsite Comprehensive Investigation** – This intervention is similar to a Compliance Review and takes place at the carrier's place of business. It is used when the carrier exhibits broad and complex safety problems through BASICs continually exceeding the threshold, worsening BASICs (three or more), or a fatal crash or complaint.

Follow-on

- **Cooperative Safety Plan** – Implemented by the carrier, this safety improvement plan is voluntary. The carrier and FMCSA collaboratively create a plan, based on a standard template, to address the underlying problems resulting from the carrier's substandard safety performance.

- **Notice of Violation (NOV)** – The NOV is a formal notice of safety problems that requires a response from the carrier. It is used when the regulatory violations discovered are severe enough to warrant formal action but not a civil penalty (fine). It is also used in cases where the violation is immediately correctable and the level of, or desire for, cooperation is high. To avoid further intervention, including fines, the carrier must provide evidence of corrective action or initiate a successful challenge to the violation.
- **Notice of Claim (NOC)** – An NOC is issued in cases where the regulatory violations are severe enough to warrant assessment and issuance of civil penalties.
- **Operations Out-of-Service Order** – An order requiring the carrier to cease all motor vehicle operations.

For more information go to frequently asked questions at:

<http://cas.fmcsa.dot.gov/faqs.aspx?faqid=1502>

How can a carrier find their CSA scores?

Motor carriers can sign in via the [Federal Motor Carrier Safety Administration \(FMCSA\) Portal](#) or directly through the [SMS Website](#). From this SMS page, a carrier representative can log in with its U.S. DOT Number and PIN in order to access the carrier's non-public data. The carrier sign-in is at the bottom center of the screen. Once signed in, you will be guided back to the SMS homepage. After that, in the search box in the middle right section of the screen, you should type in the U.S. DOT # or MC # and hit search.

Motor carriers can request an FMCSA Portal account by clicking [here](#) and following the instructions to request an account. For additional assistance with an FMCSA Portal account, contact the Help Desk at 800-832-5660. To sign in via the SMS Website, you will need your U.S. DOT Number and PIN. Note that a Docket Number PIN will not enable you to see your SMS data. If they cannot locate their PIN or were never assigned one, complete the [PIN registration process](#). A notification letter with their PIN will be generated and mailed to the address that was submitted on the most recent MCS-150 form. This letter should be received within two weeks.

When motor carriers sign in, they will be able to see additional data:

- Hazardous Materials (HM) Compliance BASIC percentile rank
- Crash Indicator BASIC percentile rank
- Driver names and other privacy-related material from individual inspection results

Chapter 40

North American Standard Level 1 Inspection Procedure



When traveling through Utah, you may be asked to participate in a Level 1 Inspection in an effort to ensure the safety of the equipment being operated on Utah's highways. We have included the following information to educate drivers in the process.

INSPECTIONS KEEP EVERYONE SAFE

Conducting Level 1 Inspections on a regular basis safeguards commercial vehicle drivers and their equipment and cargo. They also work to both encourage and enforce industry-wide safety, while reducing duplication of inspections efforts and motor carrier operating delays. Overall, the inspections are important to keep the roadways safe for everyone who travels on them

NORTH AMERICAN STANDARD DRIVER/VEHICLE INSPECTION LEVELS

Level I: North American Standard Inspection

Level II: Walk-around Driver/Vehicle Inspection

Level III: Driver-only Inspection

Level IV: Special Inspections

Level V: Vehicle-only Inspection

Level VI: Enhanced NAS Inspection for Radioactive Shipments

CERTIFIED CVSA LEVEL I INSPECTORS WILL PERFORM THE FOLLOWING PROCEDURES:

Step 1: Choose the Inspection Site

- Inspectors will select a safe location, paved, level, away from traffic, and able to support the weight of the vehicle.
- They will avoid hills, curves, soft shoulders and construction sites.
- Vehicles must be visible to oncoming traffic.

Step 2: Approach Your Vehicle

- Inspectors will observe you, the driver.
- They will adhere to officer/inspector safety policies;
- And be alert for leaks and unsecured cargo.

Step 3: Greet and Prepare You

- Inspectors will identify themselves.
- They will ensure that you are capable of communicating sufficiently to understand and respond to official inquiries and directions.
- They will also: place chock blocks on the driver's side;
- Explain this inspection procedure;
- Ensure engine is off;
- Check seat belt usage and condition;
- Observe your overall condition for illness, fatigue or other signs of impairment;
- And check for illegal presence of alcohol, drugs, weapons or other contraband.

Step 4: Interview You

- They will ask you for the following: starting location, final destination, load description, time traveled, most recent stop, fueling location(s).
- They will talk to you about the trip.

Step 5: Collect Your Documents

- Medical Examiner's Certificate (if applicable).
- Skill Performance Evaluation (SPE) Certificate (if applicable).
- Driver's license, Commercial Driver License (CDL), record of duty status.
- Shipping papers.
- Periodic inspection certificates (CVIP).
- Supporting documents: bills of lading, receipts, other documents used to verify record of duty status

Step 6: Check for the Presence of Hazardous Materials/Transportation of Dangerous Goods

- Inspectors will check shipping papers, placards, any leaks or spills, unsecured cargo, markings and labels.

Step 7: Identify the Carrier

- Inspectors will identify carrier using the following: vehicle identification, vehicle registration, insurance, and driver interview.

Step 8: Examine Your Driver's License

- Expiration date
- Endorsements
- Status
- Class
- Restrictions

Step 9: Check Medical Examiner's Certificate and Skill Performance Evaluation (SPE) Certificate

- Check certificate date (valid for 24 months).
- Check corrective lens requirement.
- Check hearing aid requirement.
- Check physical limitations.

Note: In Canada and Mexico proper class indicates adequate medical.

Step 10: Check Record of Duty Status

- Hours of Service verification.
- If you claim to be exempt, they will check that you meet all criteria for said exemptions(s).
- Accuracy of record.

Step 11: Review Periodic Inspection Report

- Ensure vehicle has passed the required inspection and has the required documents and decals.

Step 12: Prepare You for Vehicle Inspection

- Inspectors will explain the vehicle inspection procedure.
- Advise you in the use of hand signals.
- Check chock blocks.
- Prepare the vehicle, vehicle transmission in neutral. Engine off, key must be in the "on" position, and release all brakes.
- Instruct you to remain at the controls.

Step 13: Inspect Front of Tractor

- Inspectors will check headlamps and turn signals (do not use four way flashers to check turn signals) for improper color, operation, mounting, and visibility.
- Check windshield wipers for improper operation (two wipers are required unless one can clean the driver's field of vision).

Step 14: Inspect Left Front Side of Tractor

- Check left front wheel, rim, hub, and tire.

Step 15: Inspect Left Saddle Tank Area

- Check left fuel tank area.
- Check exhaust system.

Step 16: Inspect Trailer Front

- Check air and electrical lines.

Step 17: Check Left Rear Tractor Area

- Check wheels, rims, hubs, and tires.
- Check lower fifth wheel.
- Check upper fifth wheel.
- Check sliding fifth wheel.
- Check lamps.

Caution: Never place yourself in between tires of tandem axles.

Step 18: Inspect Left Side of Trailer

- Check frame and body.
- Check condition of hoses.
- Check van and open-top trailer bodies.
- Check cargo securement.

Step 19: Inspect Left Rear Trailer Wheels

- Check wheels, rims, hubs, and tires.
- Check sliding tandem.

Step 20: Inspect Rear of Trailer

- Check tail, stop, turn signals, and lamps on projecting loads.
- Check cargo securement.

Step 21: Inspect Double, Triple and Full Trailers

- Check safety devices: full trailers/converter dollies.
- Check the safety devices (chains/wire rope) for sufficient number, missing components, improper repairs, and devices that are incapable of secure attachments. Inspect pintle hook, eye and drawbar for cracks, excessive movement, and improper repairs.

Step 22: Inspect Right Rear Trailer Wheels

- Check as in Step 19.

Step 23: Inspect Rear Side of Trailer

- Check as in Step 18.

Step 24: Inspect Rear of Tractor Area**Step 25: Inspect Right Saddle Tank Area**

- Check as in Step 15.

Step 26: Inspect Right Front Side of Tractor

- Check as in Step 14.

Step 27: Inspect Steering Axle

- Check steering system (both sides).
- Check front suspension (both sides).
- Check front axle.
- Check frame and frame assembly.
- Check front brakes (both sides).
- Check and mark push rods (both sides).

Note: Inspectors will inform you that they are going under the vehicle. They will enter the under carriage in view of you. (At front power unit, rear of power unit, and front of trailer axle(s).

Step 28: Inspect Axles 2 and/or 3 (Under Carriage of CMV)

- Suspension (both sides).
- Brake components (both sides).
- Mark all pushrods on "S" cam brakes (both sides).
- Exit under carriage in view of the driver.

Step 29: Inspect Axles 4 and/or 5

- Same as step 28.

Step 30: Check Brake Adjustment

- Ensure air pressure is 90-100 p.s.i.
- Have driver fully apply brakes and hold.
- Measure and record all push rod travel.
- Identify size and type of brake chambers.
- Ensure brake lining to drum contact.

Step 31: Inspect Tractor Protection System

(This procedure tests both the tractor protection valve and the emergency brakes.)

- Have driver release brakes and disconnect both brake lines.
- Full brake application.

Step 32: Test Low Air Pressure Warning Device

- Observe dash gauges while ignition is "on" and the driver is pumping the foot valve to approximately 55 p.s.i.

Step 33: Test Air Loss Rate

- Apply brakes while the engine is idling, the governor has cut in, and pressure is 80-90 p.s.i.
- Check as in Step 18.

Step 34: Check Steering Wheel Lash

- Prepare the driver and vehicle.
- Check for excessive movement.

Step 35: Check Fifth Wheel Movement

- Check tail, stop, turn signals, and lamps on projecting loads.

Caution: If conducted improperly, this method of checking for fifth-wheel movement can result in serious damage to the vehicle. Inspector must use caution and instruct the driver carefully.

Step 36: Complete the Inspection

- Inspectors will complete documentation.
- Conclude with you, the driver.
- Follow correct and current OOS procedures (if applicable).
- Issue CVSA decal (if applicable).

Safety Fitness Procedures

Title 49 CFR, Part 385

A motor carrier receives a safety rating when the safety specialist conducts an on-site review of the carrier's compliance with the Federal Motor Carrier Safety Regulations and the Hazardous Materials Regulations. The specialist reviews records, roadside vehicle inspection data and accidents to determine whether a motor carrier meets Section 385.5 Safety Fitness standard.

Safety Ratings:

- **Satisfactory** – A motor carrier has in place and functioning adequate safety management controls to meet the safety fitness standard prescribed in Section 385.5. Safety management controls are adequate if they are appropriate for the size and type of operation of the particular motor carrier.
- **Conditional** – A motor carrier does not have adequate safety management controls in place to ensure compliance with the safety fitness standard that could result in the occurrences listed in Section 385.5 (a) through (h).
- **Unsatisfactory** – A motor carrier does not have adequate safety management controls in place to insure compliance with the safety fitness standards that has resulted in occurrences listed in Section 385.5 (a) through (h). Motor carriers receiving an “unsatisfactory safety rating” may be subject to the provisions of Section 385.13.
- **Non-Rated** – CSA focused reviews that are satisfactory.

CSA Interventions:

- **Focused Reviews** – based on CSA BASIC's above the threshold in:
 1. Unsafe Driving
 2. Fatigued Driving (HOS)
 3. Driver Fitness
 4. Controlled Substances/Alcohol
 5. Vehicle Maintenance
 6. Hazardous Materials
 7. Crash Indicator
- **Comprehensive** – based on 4 or more of the above BASIC's
- **Accident Investigation**
- **Complaint**

Section 11

Resources

Leave room for picture

Chapter 41

Contact, Agency Directories and Information

Motor Carrier Division

4501 South 2700 West,
Box 148240
Salt Lake City, Utah
84114-8240

Hours of Operation:

Monday - Friday,
8:00 a.m. - 5:00 p.m.

- Permit Requests
- Construction Reports
- LCV Application Request
- Port Bypass Program Info
- Routing Assistance
- Unified Carrier Registration Info
- Western Regional Permits

Phone: (801) 965-4892 or (866) 215-5399
Fax: (801) 965-4847

Pilot/Escort verification
Phone: (801) 964-4588

DANIELS PORT OF ENTRY

3741 Highway 40 East,
East RFD
Heber City, Utah 84032
U.S. 40, Mile Post 22

Supervisor:

Tom Greer
Phone: (435) 654-1091
Fax: (435) 587-3427

ECHO PORT OF ENTRY

P.O. Box 340
I-80 Utah - Wyoming Border,
Mile Post 180
Henefer, UT 84033

Supervisor:

Brenda Hennefer
Phone: (435) 336-5286
Fax: (435) 336-5288

KANAB PORT OF ENTRY

2136 North 300 West, #31
Kanab, Utah 84741
US 89, Mile Post 67

Supervisor:

Jared Hammon
Phone: (435) 644-5871
Fax: (435) 644-5865

MONTICELLO PORT OF ENTRY

P.O. Box 1138
Monticello, Utah 84535
Highway 491, Mile Post 2

Supervisor:

Rick Clark
Phone: (435) 587-2662
Fax: (435) 587-3427

PEERLESS PORT OF ENTRY

P.O. Box 446
Helper, Utah 84526
US 6, Mile Post 231

Supervisor:

Shane Barr
Phone: (435) 472-3401
Fax: (435) 472-8280

PERRY PORT OF ENTRY

P.O. Box 878
Brigham City, Utah 84302

Supervisor:

Carrie Baker
Phone: (435) 734-9414
Fax: (435) 723-2327

ST. GEORGE PORT OF ENTRY

P.O. Box 571
I-15, Utah - Arizona State Line,
Mile Post 1
St. George, Utah 84771

Supervisor:

JoAnna Gunderson
Phone: (435) 673-9651
(801) 965-4219
(Salt Lake Line)
Fax: (435) 628-8322

WENDOVER PORT OF ENTRY

P.O. Box 249
I-80, Mile Post 3
Wendover, Utah 84083

Supervisor:

Kaylena Pizzo
Phone: (435) 665-2274
Fax: (435) 628-8322

Utility company & Utah coordinators for loads exceeding 17'6"High

Power

Rocky Mtn. Power Statewide contact - Tusi Simeona (801) 576-6216 (get work order)

fax: (801) 572-6382

Moon Lake Electric Paul Betts (435) 724-1532
(435) 722-5400

(Vernal- Uinta Basin)

Empire Electric Ken Tarr (970) 564-4453 or
(970) 799-2613

(Monticello and US 491)

Telephone

Century Link *(Bluffdale-Point of the Mountain North to Idaho)*

Kevin Bulow (801) 636-1160
pager: (801) 223-2278

Annette Hanks (801) 374-4165 or
(801) 636-0673

(Bluffdale-Point of the Mountain South to Arizona)

Amber Womack (435) 622-5229

Uintah Basin Communication

Todd Massey (435) 622-6302

(North Eastern Region-Vernal, Roosevelt, Etc)

Mark Austin (435) 609-0328

Emery Tel-Com

Mike Behling (435) 748-2223,
(435) 749-1002

Ryan Sharp (435) 749-2165

(Carbon, Emery, Grand and Wayne Counties)

Frontier Communications

Bill Hall (435) 257-8124

Randy French (435) 259-1414

(Box Elder, Millard, Juab, Kane, Garfield, Grand & San Juan Counties)

Cable T.V.**Electric LightWave**

Shauna Jones (801) 924-6674

(IntegraTelecom)

Donnlee Watson (801) 746-2135

(South Provo to Ogden-Wasatch Front only)

Comcast

Denise Braithwaite (801) 401-3072

MCI

Blair Penrod (801) 372-5258

(Statewide)

Precise Communications

Rich Higgins (435) 820-4100

Shane Briggs (435) 637-6813

(Price-Moab-Southeastern Utah)

XO Communications

Curt Hansen (801) 983-1712 or
(801) 983-2816

(Statewide)

Bresnan Communications

Dan Johnson

(435) 828-1724

*(Vernal Area-Uinta Basin)***Local City Power Companies**Company's responsibility to contact. uamps.com/new***Signal Operations*****UDOT Region 1**

Dale Lake

(801) 620-1606

UDOT Region 2

Mark Taylor

(801) 887-3714

UDOT Region 3

Grant Jackson

(801) 227-8040

UDOT Region 4

Lee Thompson

(435) 590-9976

Graig Ogden

(435) 896-1327 or
(435) 201-4465*(Cedar, Richfield & Price)***S.L. City Signals**

Cabbott Jennings

(801) 535-6994

S.L. County Signals

James Neil

(801) 554-5623

UDOT District Permit Coordinators**Region 1 (North)**

Tommy Vigil

(801) 791-4988

Region 2(SLC)

Nazee Treweek

(801) 887-8763

(Interstate & Secondary Highways)

Jerry McMillan

(801) 887-3643

(cell) 801-910-2052

Region 3 (Central)

Rux Rowland

(801) 222-3409

Austin Tripp

(801) 830-9535

(All Highways)

Region 4 (South)

(Cedar City District)

Teri Peterson (435) 865-5503
cell: (435) 590-1285

(Richfield District)

Teri Peterson (435) 865-5503
cell: (435) 890-1285

(Price District)

George Leighton cell: (435) 636-1403
(435) 650-1479

Dale Stapley (435) 636-1402
cell: (435) 650-1477

Super Load Coordination Team

Adam Anderson	Salt Lake City	(801) 964-4588	Aanderson@utah.gov
	Cell #	(801) 915-5767	
	Fax #	(801) 965-4265	

Byron Stuart	Peerless	(435) 472-0225	Bstuart@utah.gov
	Fax #	(435) 472-8280	

Brad Dean	St George	(435) 628-5705	bradleydean@utah.gov
	Fax #	(435) 628-5706	

Stephenie Johnson	Salt Lake City	(801) 965-4342	Sjohnson@utah.gov
	Fax #	(801) 965-4488	

Monday through Friday

UHP Police Escorts Contact Number - (801) 965-4518 (Statewide)

NOTICE

The permittee is required to arrange all utility employee/vehicle support and police escorts.

Permits will not be issued until documentary approval has been obtained, and sent to UDOT, at Fax number (801)965-4211 or requesting Super Load Specialist, from all public utilities and municipalities to pass under their overhead utility lines. It is the permittee's responsibility to obtain clearances from all utilities involved, and arrange for police escorts. Failure to do so shall be reason to deny or delay the permit. UDOT will contact District Permit Coordinators for authorization.

Internet Resource List

Code of Federal Regulations

www.ecfr.gov

Commercial Vehicle Safety Alliance

www.cvsa.org

Data Qs

<https://dataqs.fmcsa.dot.gov/login.asp>

Federal Highway Administration

www.fhwa.dot.gov

Federal Motor Carrier Safety Administration

www.fmcsa.dot.gov/rules-regulations/rules-regulations.htm

Federal Motor Carrier Safety Regulations

www.fmcsa.dot.gov/rules-regulations

Federal Register

<https://www.federalregister.gov/>

First Gov | The U.S. Government's Office Web Portal

<http://www.usa.gov/>

Hazardous Material Regulations

<http://www.phmsa.dot.gov/hasmat/regs>

International Fuel Tax Agreement

www.iftach.org

International Registration Plan

<http://www.irponline.org/>

National Highway Traffic Safety Administration

www.nhtsa.dot.gov

Transportation Security Administration

www.tsa.gov

Transportation Safety Institute

www.tsi.dot.gov

U.S. Code

<http://uscode.house.gov>

U.S. Department of Transportation

www.dot.gov

USDOT Office of Hazardous Materials Safety

<http://www.phmsa.dot.gov/hasmat/regs>

USDOT Registration

<http://www.fmcsa.dot.gov/>

Utah Department of Transportation

<http://www.dot.utah.gov/>

Utah Drivers License Division

<http://publicsafety.utah.gov/>

Utah Tow Truck Regulations

<http://www.rules.utah.gov/publicat/code/r909/r909-019.htm>

Impound Vehicle Service (IVS System)

<https://secure.utah.gov/ivs-client/index.html>

Division of Administrative Rules

<http://www.rules.utah.gov>

Utah Trucking Regulations

www.utah.gov/government/utahlaws.html

www.rules-utah.gov

Important Motor Carrier Websites

<http://www.fmcsa.dot.gov/> is the home website for the Federal Motor Carrier Safety Administration. This page highlights links in question-and-answer format on specific subject matters most frequently requested by the public. The variety of topics can assist you in finding information you need to know about a particular Federal Motor Carrier Safety Administration program or activity. Please look for the question(s) you are interested in and go to the appropriate website. Also included are several links to the Motor Carrier Division of UDOT.

1. What is the Federal Motor Carrier Safety Administration's official Website address?
www.fmcsa.dot.gov
2. What must I do to start a trucking business? (**Frequently Asked Questions**)
www.fmcsa.dot.gov/about/other/faq/faqs.asp
3. How do I obtain a copy of my company profile or challenge data shown on my profile?
<https://portal.fmcsa.dot.gov>
4. How can I change the name and address of my business online?
<http://www.fmcsa.dot.gov/registration-licensing/index.htm>
5. Where can I find carrier safety ratings, inspections, and accident summary data?
<http://www.saferys.org/companySnapshot.aspx>
6. Where can I file a complaint against a motor carrier company?
<http://www.utahmc.com> This website will be replaced Utahmc.com is going away
7. Where can I set up an account to monitor my CSA information?
<https://portal.fmcsa.dot.gov/login>
8. Where can I find information about the transportation of Hazardous Materials?
<http://phmsa.dot.gov> and <http://www.fmcsa.dot.gov/safetyprogs/hm.htm>
9. What safety and security programs are available through the FMCSA?
<http://www.fmcsa.dot.gov/safety-security/safety-security.htm>
10. Where can I find information concerning UCR? <https://www.ucr.in.gov/>
11. How do I contact the Motor Carrier Division of UDOT to purchase permits on line, ask questions or access the Motor Carrier Guide? <http://utahmc.com/> This website will be replaced Utahmc.com is going away

or <https://www.udot.utah.gov/public/mcs/f?p=155:1:3139151838513051::NO::>
12. Where are the locations and phone numbers for the 10 Ports of Entry in Utah?
http://utahmc.com/udot_info.php This website will be replaced Utahmc.com is going away
13. How do I get started? (**Educational packet developed by UDOT**)
<http://www.udot.utah.gov/main/f?p=100:pg:0:::1:T,V:188>
14. Driver Education/Training Site.
<http://www.fmcsa.dot.gov/about/outreach/education/driverTips/index.htm>
15. The Pre-Employment Screening Program (PSP) – assists motor carriers in investigating crash history and roadside safety performance of prospective drivers.
<http://www.psp.fmcsa.dot.gov/Pages/default.aspx>

Driver and Company Safety Resources



Truck Smart

This program teaches auto drivers how to drive safely around big-rigs. Visit this page for a driver education teacher's toolkit and current ad campaign information.

Visit Site >> (Connects to link: <http://utahmc.com/trucksmart/>) **This website will be replaced Utahmc.com is going away**



Drive to Stay Alive

This educational campaign reminds CMV drivers to slow down, buckle up and stay alert. Visit this page for CMV safe driving tips and techniques and other campaign materials.

Visit Site >> (Connects to link: <http://utahmc.com/DTSA>) **This website will be replaced Utahmc.com is going away**



Commercial Vehicle Safety Alliance

This site has many safety programs including Operation Safe Driver and Teens and Trucks.

Visit Site >> (Connects to link: <http://www.cvsa.org/programs/index.php>) **This website will be replaced Utahmc.com is going away**

Chapter 42

Definitions

Administrators (AAMVA) - A tax-exempt, nonprofit organization striving to develop model programs in motor vehicle administration, police traffic services and highway safety. The association serves as an information clearinghouse for these same disciplines, and acts as the international spokesman for these interests.

Agricultural Products - Any raw product, which is derived from agriculture, including silage, hay, straw, grain, manure, and other similar product. American Association of Motor Vehicle

Alcohol- is the intoxication agent in beverage alcohol, ethyl alcohol, or low molecular weight alcohol including methyl and isopropyl alcohol.

Alcohol use- is the consumption of any beverage, mixture or preparation (including medication) that contains alcohol.

American Association of State Highway Transportation Officials (AASHTO) - A non-profit, nonpartisan association representing highway and transportation departments in the 50 states, the District of Columbia and Puerto Rico.

Appurtenance - Consists of items such as binders, chains, clearance lights, rub rails, and load securing devices.

Appurtenance- as defined in CFR 23-658 and Section 72-7-402.

Articulated vehicle- consists of two or more vehicles that are connected by a joint that can pivot.

Authorized Personnel - Means a Certified Pilot/Escort Driver as described in MUTCD 6C.02, and also classified as a "Flagger" as set forth in Chapter 6E of the MUTCD.

BMC 82 - Public liability surety bond filed with the FMCSA as required under 49 CFR 387.303(b)(1).

BMC 91 - Motor Carrier Automobile Bodily Injury and Property Damage Liability Certificate of Insurance form as required under 49 CFR 387.303(b)(1) or (2).

Bill of Lading - Written transportation contract between shipper and carrier (or its agents). Identifies freight, recipient, place of delivery, and terms of agreement.

Bridge formula is a bridge protection formula used by federal and state governments to regulate the amount of weight that can be put on each of a vehicle's axles, or the number of axles, and the distance between the axles or group of axles must be to legally carry a given weight.

Bulk packaging- means a packaging other than a vessel or barge, including a transport vehicle or freight container, in which hazardous materials are loaded with no intermediate form of containment and which has:

1. A maximum capacity greater than 119 gallons as a receptacle for a liquid:
2. A maximum net mass greater than 882 pounds and a maximum capacity greater than 119 gallons as a receptacle for a solid:
3. A water capacity greater than 1000 pounds as a receptacle for a gas as defined in 173.115.

CDL - Commercial Driver's License.

CDLIS - Commercial Driver's License Information System.

CFR - Code of Federal Regulations

CSA-means the Compliance, Safety, Accountability program administered by the Federal Motor Carrier safety Administration, where they work together with state partners and industry to further reduce commercial motor vehicle crashes, fatalities, and injuries on our nation's highways.

Cargo/Cargo Carrying Length - Means the total length of a combination of trailers and/or load measured from the foremost of the first trailer and/ or load to the rearmost of the last trailer and/or load including all coupling devices.

Chocks - A wedge or block for steadying a body and holding it motionless.

Commercial vehicle- as defined in CFR 390.5 and Section 72-9-102.

Commercial Motor Vehicle Federal Definition 49 CFR 390.5 - Means any self-propelled or towed motor vehicle used on a highway in interstate commerce to transport passengers or property when the vehicle--

- Has a gross vehicle weight rating or gross combination weight rating, or gross vehicle weight or gross combination weight, of 4,536 kg (10,001 pounds) or more, whichever is greater; or
- Is designed or used to transport more than eight passengers (including the driver) for compensation; or
- Is designed or used to transport more than 15 passengers, including the driver, and is not used to transport passengers for compensation; or
- Is used in transporting material found by the Secretary of Transportation to be hazardous under 49 U.S.C. 5103 and transported in a quantity requiring placarding under regulations prescribed by the Secretary under 49 CFR, subtitle B, chapter I, and subchapter C.

Commercial Vehicle Utah Definition UCA 72-9-102 - Means a motor vehicle, vehicle, trailer, or semi trailer used or maintained for business, compensation, or profit to transport passengers or property on a highway if the commercial vehicle:

- Has a manufacturer's gross vehicle weight rating or gross combination weight rating of 10,001 or more pounds;
- Is designed to transport more than 15 passengers, including the driver; or
- Is used in the transportation of hazardous materials and is required to be placarded in accordance with 49 C.F.R. Part 172

The following vehicles are not considered a commercial vehicle for purposes of this chapter:

- Equipment owned and operated by the United States Department of Defense when driven by any active duty military personnel and members of the reserves and national guard on active duty including personnel on full-time national guard duty, personnel on part-time training, and national guard military technicians and civilians who are required to wear military uniforms and are subject to the code of military justice;
- Firefighting and emergency vehicles, operated by emergency personnel, not including commercial tow trucks; and
- Recreational vehicles that are driven solely as family or personal conveyances for noncommercial purposes

Consumer commodities -are materials that are packaged and distributed in a form intended for, or suitable for sale through retail sales. In order to determine if a particular hazardous material may

qualify as a consumer commodity, refer to the section number in Part 173 identified in column 8 of the 172.101 Table for that material.

CVISN - Commercial Vehicle Information System and Network

CVSA - Commercial Vehicle Safety Alliance Defense Highway - US 491, US 191 (from US 491 to I-70), US 6, and all roads leading into or out of a US military facility.

Daylight-means one-half hour before sunrise and one-half hour after sunset.

Department - Means the Utah Department of Transportation.

Divisible Load - A load that can reasonably be dismantled or disassembled and does not meet the definition of non-divisible as defined in this section.

Division - Means the Motor Carrier Division.

Drawbar - Connection between two vehicles, measured from box to box and/or frame-to-frame, or actual drawbar, one of which is towing or drawing the other on a highway.

Driving time- is all time spent at the driving controls of a commercial motor vehicle in operation.

Dromedary Unit - A truck-tractor capable of carrying a load independent of a trailer. A dromedary is a box, deck, or plate mounted behind the cab and forward of the fifth wheel on the frame of the power unit.

Dromedary unit-is a truck-tractor capable of carrying a load independent of a trailer. Units manufactured prior to December 1, 1982 are exempt as a truck-trailer.

Dunnage - All loose materials used to support and protect cargo.

Dunnage Bags - An inflatable bag intended to fill otherwise empty space between articles of cargo, or between article of cargo and the wall of the vehicle.

Exclusive Devices - All commercial vehicle length and width devices and appurtenances exempt from measurement in 23 CFR 658 of the Federal Register. (Final Rule effective April 29, 2002) UCA 72-7-402.

Fixed axle- means an axle that is not steerable, self steering or retractable.

Flagger- is a person that is trained to direct traffic using signs or flags to aid the over-dimensional load or vehicles in the safe movement along the highway as designated on the over-dimensional load permit.

FMCSA - Federal Motor Carrier Safety Administration

FMCSRs (Federal Motor Carrier Safety Regulations) - Contains the rules governing operations of trucks and buses in interstate commerce. Utah has adopted the FMCSR for intrastate transportation.

For-Hire Motor Carrier - means a person engaged in the transportation of goods or passengers for compensation.

Freight Bill - Shipping document describing the freight, classification, rates charged, total amount Definitions for transportation, and any other charges made under a tariff.

Full trailer- a vehicle without motive power designed for carrying property and for being drawn by a motor vehicle and constructed so that no part of its weight rests upon the towing vehicle.

GRAMA - Government Records Access Management Act

Gross Combination Weight Rating (GCWR) - Means the value specified by the manufacturer as the loaded weight of a combination (articulated) motor vehicle. In the absence of a value specified by the manufacturer, GCWR will be determined by adding the GVWR of the power unit and the total weight of the towed unit and any load thereon.

Gross Vehicle Weight Rating (GVWR) - Means the value specified by the manufacturer as the loaded weight of a single motor vehicle.

Hazardous Materials - Substance or material capable of posing unreasonable risk to health, safety, and property when transported in commerce, as determined by the U.S. Secretary of Transportation.

HMR - Hazardous Materials Regulations

High-risk motor carrier- is a carrier that is:

☐ above the threshold in the Crash or Fatigue or Unsafe BASIC that is greater than or equal to 85%, plus one other BASIC at or above the "all other" motor carrier threshold; or

☐ a motor carrier with any four or more BASIC's at or above the "all other" motor carrier threshold.

Highway - Any public road, street, alley, lane, court, place, viaduct, tunnel, culvert, bridge, or structure laid out or erected for public use, or dedicated or abandoned to the public, or made public in an action for the partition of real property, including the entire area within the right-of-way. UCA 72-1-102(7)

IFTA - International Fuel Tax Agreement

Incidental- means transportation that occurs occasionally or by chance, but does not exceed a distance of 20 miles.

Implement of Husbandry - Every vehicle designed or adapted or used exclusively for an agricultural operation and only incidentally operated or moved upon the highways.

Interstate Commerce - Means trade, traffic, or transportation in the United States:

- Between a place in a state and a place outside of such state (including a place outside of the United States);
- Between two places in a state through another state or a place outside of the United States; or
- Between two places in a state as part of trade, traffic, or transportation originating or terminating outside the state or the United States.

Interstate System means - Any highway designated as an interstate or freeway. For the purpose of this guide I-15, I-215, I-80, I-70, US 89 between I-84 and I-15 and SR 201 between I-15 and I-80 will be considered interstate.

Intrastate Commerce - Means any trade, traffic, or transportation in any state, which is not described in the term "interstate commerce."

IRP - International Registration Plan

ISS - Inspection Selection System

Laden- means carrying a load.

Large packaging- means-a packaging that—(1) Consists of an outer packaging that contains articles or inner packaging; (2) Is designated for mechanical handling; (3) Exceeds 400 kg net mass or 450 liters (118.9 gallons) capacity; (4) Has a volume of not more than 3 cubic meters (m³) (see § 178.801(i) of this subchapter); and (5) Conforms to the requirements for the construction, testing and marking of Large Packaging as specified in subparts P and Q of part 178 of this subchapter.

Logbook - (Record of duty status) Written record completed by a commercial vehicle driver in a graph-grid format. Entries indicate daily number of hours worked, driven, off-duty, and vehicle(s) driven.

Longer Combination Vehicle or an (LCV) is a - A combination of truck, truck tractor, semi-trailer and trailer(s), which exceeds legal dimensions/ weight and operates on highways by permit for transporting divisible loads.

Longer Combination Vehicle (LCV) Authority - An authorization given to a specific company to exceed standard permitted length allowances for vehicle configuration on pre-approved routes.

MCD - Motor Carrier Division

MCS 90 - Endorsement for motor carrier policies of insurance for public liability.

MCSAP - Motor Carrier Safety Assistance Program

MCSIP - Motor Carrier Safety Improvement Program

Manufactured/Home - A transportable factory built housing unit constructed on or after June 15, 1976, in one or more sections, and designed to be used as a dwelling with or without a permanent foundation when connected to the required utilities, and includes the plumbing, heating, air-conditioning, and electrical systems.

Manufactured mobile home- means a transportable factory built housing unit built prior to June 15, 1976, in accordance with a state mobile home code, which existed prior to the Federal Manufactured Housing and Safety Standards Act.

Materials of Trade transportation (see 49 CFR 171.8) by highway may be accepted from many of the requirements of the HM regulations when transported in accordance with the procedures contained in 49 CFR 173.6.

Medical Certificate - Certificate showing that driver has passed USDOT - prescribed physical exam. Also know as a "medical card."

Medical waiver - Waiver from medical requirements for drivers who cannot meet minimum driver qualification standards under 49 CFR Part 391.

Motor Carrier - A person engaged in or transacting the business of transporting passengers, freight, merchandise, or other property by a commercial vehicle on a highway within this state and includes a tow truck business.

Motor carrier-as defined in Section 72-9-102.

MVR - Motor Vehicle Record

Non-Divisible - Any load or vehicle exceeding applicable length: width, or height or weight limits which, if separated into smaller loads or vehicles would: (a) Compromise the intended use of the load and/or vehicle, (b) Destroy the value of the load or vehicle, or (c) Require more than eight work hours to dismantle using appropriate equipment.

Multi-trip- means two or more daily or a minimum of 10 weekly trips in the proximity of a port-of-entry

MUTCD - Means Manual on Uniform Traffic Control Devices

NHTSA - National Highway Traffic Safety Administration

Non-divisible- any load or vehicle exceeding applicable length, width, or height or weight limits which, if separated into smaller loads or vehicles would:

- A. compromise the intended use of the load or vehicle;
- B. destroy the value of the load or vehicle; or
- C. require more than eight work hours to dismantle using appropriate equipment.

ORM-D materials are materials such as a consumer commodity, which although is subject to the regulations presents a limited hazard during transportation due to its form, quantity, and packaging. Each ORM-D material and category of ORM-D material is listed in the 49 CFR 172.101 Table and 173.144.

Off duty- means the driver has been relieved of all responsibilities and is not ready for work.

On duty time- is all time a driver spends performing work, or being ready to work, until being relieved by the carrier of all responsibility. "On duty" time also includes any compensated work performed by the driver for a non-motor carrier entity. On duty time does not include time spent resting in or on a parked vehicle; (also applies to passenger-carrying drivers); any time spent resting in a sleeper berth; or up to 2 hours riding in the passenger seat of a property-carrying vehicle moving on the highway immediately before or after 8 consecutive hours in sleeper-berth.

Out-of-Service - Condition where a motor vehicle, because of mechanical condition or loading, is considered imminently hazardous and likely to cause an accident or breakdown; or where a driver violation renders a commercial vehicle operator unqualified to drive.

Placard - Diamond-shaped sign required on a four sides of motor vehicle hauling hazardous materials that shows hazard classification of material transported.

Pole Trailer - Every vehicle without motive power designed to be drawn by another vehicle and attached to the towing vehicle by means of a reach, or pole, or by being boomed or otherwise secured to the towing vehicle, and is ordinarily used for transporting long or irregular shaped loads such as poles, pipes, or structural members generally capable of sustaining themselves as beams between the supporting connections.

Port-of-entry by-pass permit-allows a motor carrier a temporary permit that would allow by-pass of a designated port of entry.

PRISM - Performance & Registration Information Systems Management

Private Motor Carrier - A person who provides transportation of property or passengers, by commercial motor vehicle, and is not a for hire motor carrier.

Quad Axle Group - A group of four consecutive fixed axles.

Record of duty status- is the tracking of work hours by means of a graph, grid, log or an automatic on-board recording device.

Recreational Vehicle - Vehicles that are driven solely as family or personal conveyances for noncommercial purposes.

Retractable Axle - An axle which can be mechanically raised and lowered by the driver of the vehicle, but which may not have its weight-bearing capacity mechanically regulated.

Rocky Mountain doubles- a tractor and two trailers, consisting of a long and a short trailer.

Saddle mount- means a truck or tractor towing other vehicles with the front axle of each towed vehicle mounted on top of the frame of the proceeding vehicle or vehicles.

Saddlemount - A device, designed and constructed as to be readily demountable, used in driveaway-towaway operations to perform the functions of a conventional fifth wheel:

- **Upper-half** - Upper-half of a "saddlemount" means that part of the device which is securely attached to the towed vehicle and maintains a fixed position relative thereto, but does not include the "kingpin;"
- **Lower half** - Lower half of a "saddlemount" means that part of the device, which is securely attached to the towing vehicle and maintains a fixed position relative thereto but does not include the "king-pin;"
- **Kingpin** - A device which is used to connect the "upper-half" to the "lower-half" of the 5th wheel plate in such a manner as to permit relative movement in a horizontal plane between the towed and towing vehicles.

SAFER - Safety and Fitness Electronic Records

SAFETYNET - Networked PC Data Collection and Delivery System

SEA - Safety Evaluation Area

Secondary Highway - All other routes not designated as interstate or freeway. Two-lane, two-way highways are synonymous with secondary highways.

Semi Trailer - Every vehicle without motive power designed for carrying persons or property and for being drawn by a motor vehicle and constructed so that some part of its weight and its load rest or is carried by another vehicle.

Shipping Paper - USDOT's shipping order, bill of lading, or other document used in connection with the movement of freight.

Shoring Bar - A device placed transversely between the walls of a vehicle and cargo to prevent cargo from tipping and shifting.

Special Event - Means the movement of an over-dimensional load/vehicle.

Special hazards- means extreme weather conditions and/or highway construction or maintenance projects

Special mobile equipment- or an SME means a vehicle or vehicles exempt from registration that is not designed or used primarily for the transportation of persons or property; is not designed to operate in traffic; and is only incidentally operated or moved over the highways.

Special truck equipment- or an STE means a vehicle by nature of design that cannot meet the non-divisible weight allowances such as cement pump trucks, well boring trucks, or cranes with a lift capacity of five or more tons.

Split Axle - In split-axle designs, the wheel on each side is attached to a separate shaft. Modern passenger cars have split drive axles. In some designs, this allows independent suspension of the left and right wheels, and therefore a smoother ride. Even when the suspension is not independent, split axles permit the use of a differential, allowing the left and right drive wheels to be driven at different speeds as the automobile turns, improving traction and extending tire life.

Spread axle- is two single axles that exceed 96 inches apart.

Stinger Steered - A semi-trailer combination wherein the fifth wheel is located on a drop frame behind and below the rearmost axle of the power unit.

Super Load - A vehicle and/or load in excess of 17 feet in width on secondary highways, 20 feet in width on Interstate systems, 17 feet 6 inches in height or in excess of 125,000 pounds gross vehicle weight.

Tandem Axle - Two or more axles spaced not less than 40 inches nor more than 96 inches apart and having at least one common point of weight suspension.

Time record- is a time card or other record showing the time the driver reports for duty each day, total hours the driver is on duty each day, time the driver is released from duty each day, and total time for preceding days if the driver is used for first time or intermittently.

Tridem Axle - Any three consecutive axles whose extreme centers are not more than 144 inches apart, and are individually attached to or articulated from, or both, a common attachment to the vehicle including a connecting mechanism designed to equalize the load between axles.

Trailer (Full) - Vehicle without motive power designed for carrying property and for being drawn by a motor vehicle and constructed so that no part of its weight rest upon the towing vehicle.

Triple trailer- means a tractor and three trailers of approximately equal length.

Truck- means any self-propelled motor vehicle, except a truck tractor, designed or used for the transportation of property, laden or un-laden.

Truck Tractor - A motor vehicle designed and used primarily for drawing other vehicles and not constructed to carry a load other than a part of the weight of the vehicle and load that is drawn.

Trunnion Axle - An axle configuration with two individual axles mounted in the same transverse plane, with four tires on each axle.

Trunnion Axle Group - Two or more consecutive trunnion axles that are attached to the vehicle by a weight equalizing suspension system and whose consecutive centers are more than 40 inches, but not more than 96 inches apart.

Turnpike doubles- means a tractor and two trailers of equal length.

UCR means - Unified Carrier Registration

UDOT - Utah Department of Transportation

Un-laden- means a vehicle is not carrying a load

USDOT - United States Department of Transportation

Variable load suspension axle- or VLS is an axle that can be adjusted mechanically to various weight bearing capacities and can also be mechanically raised and lowered

Vehicle - Every device in, upon, or by which any person or property is or may be transported or drawn upon a highway, except devices used exclusively upon rails or tracks.

Vintage Vehicle - Means a motor vehicle that is 40 years old or older, from the current year, primarily a collector's item, and used for participation in club activities, exhibitions, tours, parades, occasional transportation, and similar uses, but that is not used for general daily transportation.

Western Association of State Highway Transportation Officials, Committee on Highway Transport (WASHTO, COHT) - An association of transportation officials organized to promote uniform laws, regulations and practices among member jurisdictions and other jurisdictions for the efficient movement of goods and services while ensuring the safety of all highway users and preserving the highway infrastructure.

Chapter 43

Maps

New Maps need to be entered from files from Debbie

[Restricted Travel Zone for Oversized Loads](#)

[Secondary Highways with Restrictions](#)

[Designated Utah Routes for WRP](#)

[Port of Entry Locations](#)

[Approved High Load Routes](#)

[Designated routes for LCV](#)

[Designated State routes for WRP](#)

[Strat Map for Defensive Highway](#)

Chapter 44

Truck Parking

The current location will have to be moved utahmc.com is going away

http://www.utahmc.com/top_maps.php



Chapter 45

Chain Law

Snow Tire and Chain Requirements

- Locations shall be designated by the Department of Transportation's Region Director after coordinating with the local Utah Highway Patrol office. The designations by the Region Director shall be established through a Traffic Engineering Order (TEO) from the Division of Traffic and Safety to the Region Director's office wherein the designated highway is located.
- The Utah Department of Transportation's Division of Traffic and Safety shall maintain and annually publish a listing of those highways so designated for distribution to:
 - Utah Department of Transportation Region Offices;
 - Utah Highway Patrol;
 - county offices; and
 - local law enforcement officials.
- When any designated highway is so restricted no vehicle shall be allowed or permitted the use of the highway, during the period between October 1 and April 30, or when conditions warrant due to adverse, or hazardous weather or roadway conditions, as determined by the Utah Department of Transportation, unless:
 - said vehicle is equipped with either:
 - steel link chains or have chains in possession;
 - mounted snow tires; (tires with an M/S designation with or without studs);
 - elastomeric tire chains, designed for use with radial tires; or
 - four-wheel drive vehicles with a minimum of two mounted snow tires.
- Radial tires without snow tread do not meet the requirements.
- An operator of a commercial vehicle with four or more drive wheels, other than a bus, shall affix tire chains to at least four of the drive wheel tires.
- An operator of a bus or recreational vehicle shall affix tire chains to at least two of the drive wheel tires.

(Refer to **R920-6-3**)